

2020 “State of the Streets”

Final Report

Prepared for:

**Village of Lansing, Illinois &
Chicago Metropolitan Agency for Planning**

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ACRONYMS

Acronym	Definition
AC	Asphalt concrete pavement
APC	Asphalt concrete overlay on Portland cement concrete pavement
ASTM	American Society for Testing and Materials
BR	Brick pavement
CIP	Capital Improvement Plan
CMAP	Chicago Metropolitan Agency for Planning
CSU	Colorado State University
FT	Foot
G&AI	Gorronzona and Associates, Inc.
GIS	Geographic information system
GR	Gravel pavement
IRI	International Roughness Index
K	Thousand
L&T	Longitudinal and transverse cracking
LCD	Last construction date
M	Million
M&R	Maintenance and rehabilitation
P	Primary rank pavement
PAVER	PAVER Pavement Management System
PCC	Portland cement concrete pavement
PCI	Pavement Condition Index
PMP	Pavement management program
PMS	Pavement management system
S	Secondary rank pavement section
SF	Square feet
SU	Sample unit
SY	Square yard
T	Tertiary rank pavement section

1 EXECUTIVE SUMMARY

1.1 History

In October of 2020, the Chicago Metropolitan Agency for Planning (CMAP) retained the services of Gorrondona and Associates, Inc. (G&AI) to implement a pavement management system for the Village of Lansing that will enable the Village to manage its roadway network in a more proactive, cost-effective, and sustainable way. To accomplish this objective, G&AI: 1) assessed the condition of the Village’s roadways, 2) implemented and customized a pavement management system for the Village, and 3) developed near- and long-term pavement maintenance and rehabilitation (M&R) recommendations for the Village’s roadways.

During the fall of 2019 and the spring of 2020, G&AI’s state-of-the-art PathRunner pavement condition data collection system (shown in Figure 1) was deployed to capture continuous, high-resolution pavement cracking, rutting, and roughness data of the Village’s roads. Collected data were entered into the PAVER Pavement Management System (PAVER), and baseline pavement condition scores were determined for each roadway.

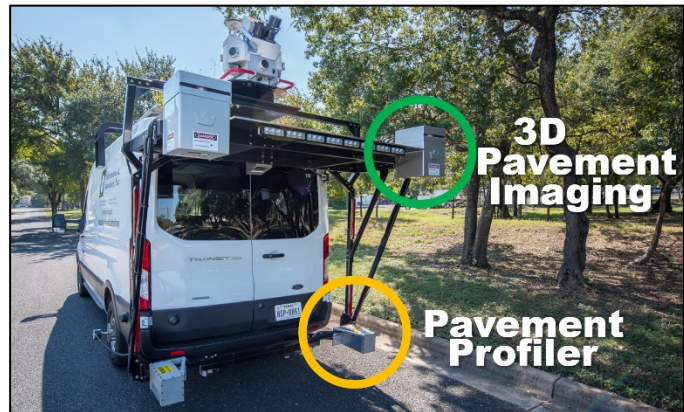


Figure 1. PathRunner pavement condition data collection system.

In July of 2020, preliminary results of the condition survey were presented to the Village. G&AI has since worked with the Village to collect additional pavement M&R records and M&R unit cost data with which to calibrate PAVER so that it is specific to the Village.

The collected pavement condition data along with both the historical M&R data and unit prices provided by the Village were used to develop network-level M&R recommendations presented herein for the Village’s consideration.

1.2 PAVER Pavement Management System

PAVER stores two primary “measures” of pavement condition. The most obvious measure of pavement condition is the **International Roughness Index (IRI)**, which describes the rideability (i.e., smoothness) of the roadway as experienced by the driver.

The second measure of pavement condition is the **Pavement Condition Index (PCI)**, which provides an indication of both the structural integrity and surface operational condition of the roadway. PAVER uses PCI values to determine the most cost-effective level of M&R likely needed. PAVER prioritizes funding for life-extending, lower-cost preventive maintenance activities (e.g., crack sealing, slurry seals, and localized patching) above more costly funding of last resort major M&R activities, such as resurfacing and reconstruction. This prioritization in the PAVER algorithm seeks a proactive and cost-effective approach to pavement management with the avoidance of – unless necessary – more costly reactive practices.

In addition to routinely collected IRI and PCI data, PAVER stores pavement inventory information, historical M&R records, and M&R unit cost data. The system uses this information to predict future

pavement conditions and identify network-level deterioration trends and M&R needs over time. It will also allow the Village to evaluate if present M&R methods are performing as expected.

1.3 Purpose and scope

The purpose of this project is to implement a comprehensive pavement management system for the Village’s roadways. The scope of this project includes all roadways managed by the Village, which total approximately 92.9 centerline miles. This pavement management system will serve as a primary tool to assist the Village in more efficiently allocating its pavement M&R funding.

To this end, G&AI:

1. Developed an inventory of the Village’s roadways in PAVER. The PAVER inventory contains pavement surface type, functional classification, M&R unit costs, and historical M&R data. *Note: Inventory development is a one-time effort that can be used by the Village if PAVER is retained, only requiring updates to address changes to the Village’s roadway network and changes in M&R unit costs.*
2. Performed a pavement condition survey of the Village’s roadways. This survey was used to determine PCI and IRI values for analysis purposes and will serve as an initial baseline of roadway conditions.
3. Used the condition survey with the developed PAVER inventory to determine the impact of different funding levels on the Village’s roadways and identify potential network-level pavement M&R needs.

1.4 Results

Pavement Condition Index (PCI) and **International Roughness Index (IRI)** values were determined for each roadway. PCI values provide an indication of both the structural integrity and surface operational condition of a pavement. PCI values range from 0 (a failed pavement) to 100 (a pavement in excellent condition). Table 1 shows the categories chosen to represent the Village’s PCI assessment criteria, which includes typical pavement distresses and levels of M&R needed within each category.

Table 1. Village’s pavement condition categories.

Category	Typical Distresses and Typical Level of M&R Needed	PCI Range
Good	Longitudinal and transverse cracking and weathering of surface Preventive maintenance: <i>Crack sealing and surface treatments</i>	86-100
Satisfactory	More extensive longitudinal and transverse cracking and weathering of surface Preventive maintenance: <i>Crack sealing and surface treatments</i>	71-85
Fair	Extensive longitudinal and transverse cracking, early stage alligator (fatigue) cracking, early stage rutting, and weathering of surface Global preventive maintenance and localized repairs: <i>Localized surface and/or full-depth patching, surface treatments, and thin overlays</i>	56-70
Poor	More extensive and severe longitudinal and transverse cracking, alligator (fatigue) cracking, rutting, and weathering of surface Major rehabilitation: <i>Localized full-depth patching, mill and overlays, and traditional overlays</i>	41-55
Very Poor	More extensive and more severe longitudinal and transverse cracking, alligator (fatigue) cracking, rutting, weathering of surface, potholes Major rehabilitation: <i>Full-depth patching, mill and overlays, traditional overlays, and reconstruction</i>	26-40
Serious	Extensive and severe failure of pavement surface Major rehabilitation: <i>Reconstruction</i>	11-25
Failed	Complete failure of pavement surface Major rehabilitation: <i>Reconstruction</i>	0-10

At the time of G&AI’s inspection, the Village’s pavements were found to have an average PCI of 60, indicating that the Village’s roadways are in overall “fair” condition.

IRI values measure the roughness (vertical displacement over a fixed interval reported in inches per mile) of a roadway pavement:

- IRI values less than 200 inches/mile indicate “smooth” pavement.
- IRI values between 200 and 400 inches/mile indicate a “marginally rough” pavement.
- IRI values greater than 400 inches/mile indicate “rough” pavement.

The Village’s roadways were found to have an average IRI value of 300 inches/mile, which indicates overall “marginally smooth” pavement.

Following this executive summary, Map 1 shows PCI categories for each roadway. Roadways that were planned for resurfacing or reconstruction in 2020 (i.e., after the field inspection was performed) were assigned an assumed PCI value of 100. All other PCI values shown on Map 1 reflect the conditions of the

roadways at the time of the field inspection. Map 2 shows IRI categories for each roadway at the time of inspection. IRI values reflect a physical measurement of roughness. Consequently, IRI values were not adjusted for roadways that were planned for resurfacing or reconstruction in 2020.

The causes of pavement deterioration as quantified by the PCI may be divided into three general categories:

- Vehicle load related.
- Climate/durability related.
- Other (construction defects and material issues).

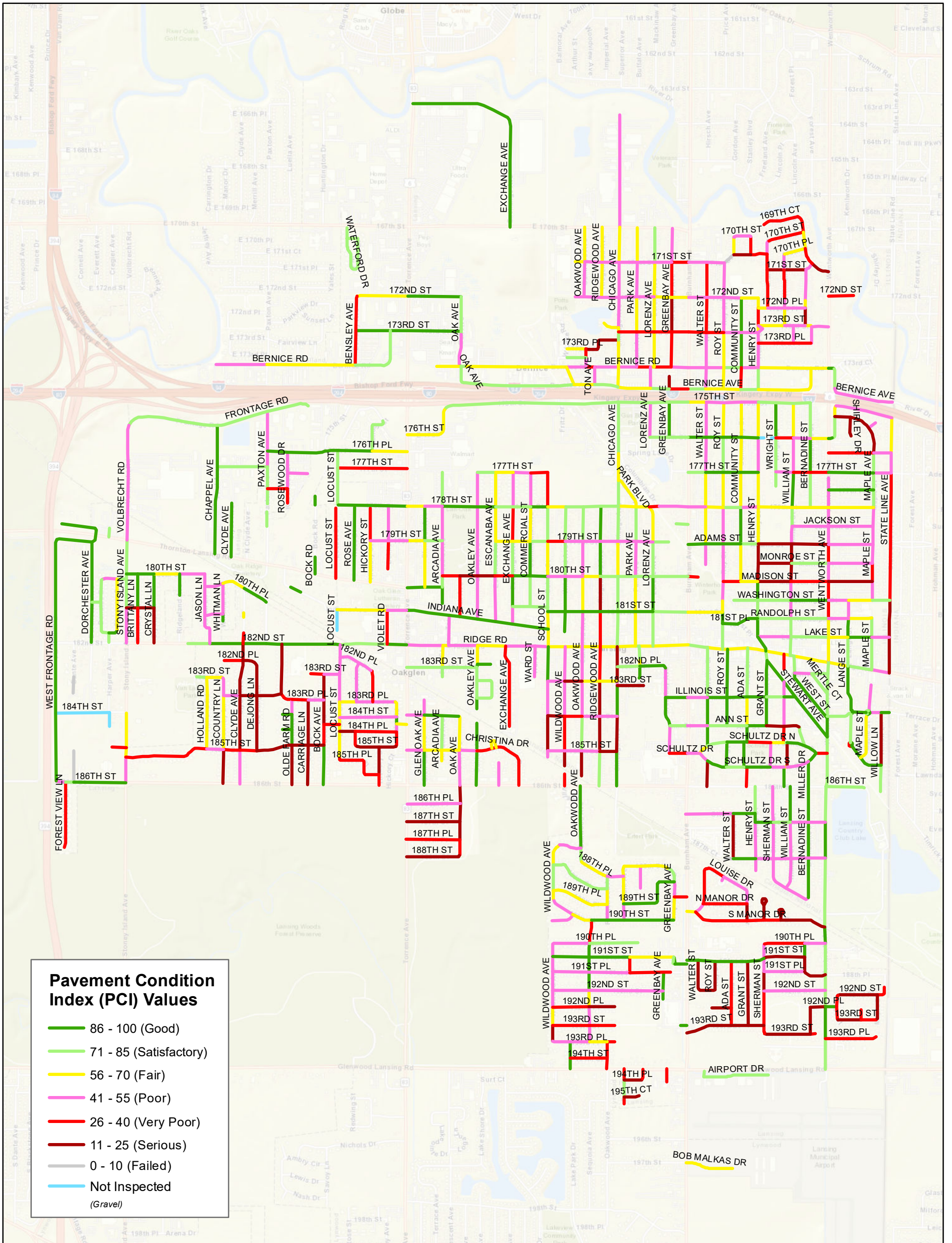
The deterioration observed on the Village’s pavements at the time of inspection was caused by a mixture of vehicle load- and climate-related distresses. Vehicle load-related distresses, including alligator cracking and rutting, were pronounced on many of the Village’s roadways and contributed most to lower PCI values. Significant climate-related distresses, including block cracking and weathering, were also observed on the Village’s roadways.

1.5 Recommendations

For the Village to get the most return on their investment from PAVER, the system must be considered a living entity. The Village should:

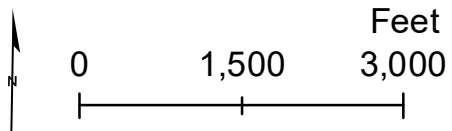
1. Implement pavement preservation techniques to cost-effectively extend the life of its roadways.
2. Determine when resurfacing is no longer a cost-effective option and reconstruction is needed.
3. Annually update M&R activities performed on Village roadways in the PAVER database.
4. Annually update M&R unit costs (or whenever economic conditions cause changes in unit prices).
5. Commit future funding to the routine collection of pavement condition data (all roadways should be inspected on a two- to three-year cycle).
6. Use collected pavement condition data to assess the performance of the roadways and applied M&R activities.

With such attention, PAVER will become a repository of accurate, up-to-date data and the primary tool that the Village uses for more cost-effectively programming M&R funding.



Pavement Condition Index (PCI) Values

Green	86 - 100 (Good)
Light Green	71 - 85 (Satisfactory)
Yellow	56 - 70 (Fair)
Pink	41 - 55 (Poor)
Red	26 - 40 (Very Poor)
Dark Red	11 - 25 (Serious)
Grey	0 - 10 (Failed)
Blue	Not Inspected (Gravel)

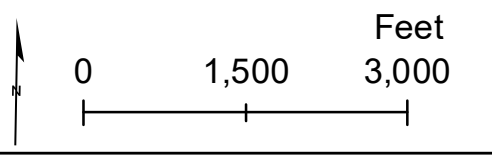
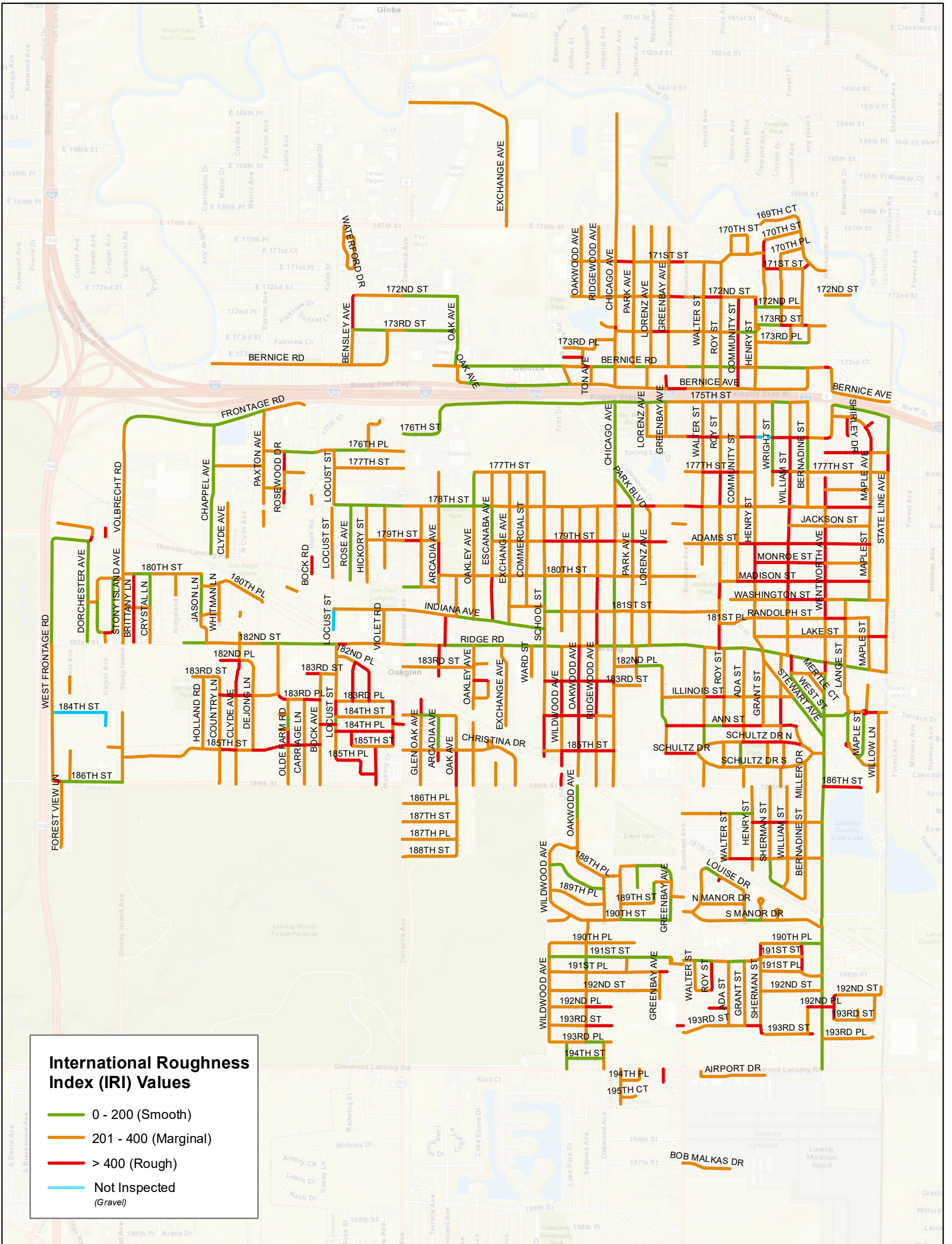


Map 1:
Pavement Condition Index (PCI) Values

Lansing, Illinois

Pavement Management Program





Map 2:
International Roughness
Index (IRI) Values

Lansing, Illinois

Pavement Management Program



2 INTRODUCTION

2.1 Foreword

This section of the report expands on the Executive Summary and provides the reader with information pertaining to the creation and implementation of this pavement management system for the Village.

At the core of a modern pavement management system is a geocentric database that contains pavement inventory and condition information. Combined with up-to-date M&R unit cost data, calibrated deterioration models, and owner-specific M&R practices, this information is used by analysis tools in the pavement management system to predict future pavement conditions, develop multi-year M&R plans, and forecast anticipated funding needs.

This section provides a conceptual overview of pavement management and follows with the benefits and costs of implementing a pavement management system. Implementation of the Village’s pavement management system is detailed in Sections 3, 4 and 5. This section closes with an overview of effective preventive maintenance strategies that should be considered by the Village.

2.2 Background, scope, and objectives

The Chicago Metropolitan Agency for Planning (CMAP) retained the services of Gorrondona and Associates, Inc. (G&AI) to assess the existing condition of the roadways maintained by the Village. The primary objectives of this project are to implement a comprehensive and Village-wide pavement management system, perform a network-level pavement condition survey, and identify future pavement M&R needs.

The project will provide the Village with a better understanding of the current condition of its roadways and network-level recommendations for future M&R based on the results of the pavement condition survey. Moving forward, the pavement management system will continue to serve as a repository for pavement condition data, historical M&R records, and pavement condition deterioration trends.

PAVER was implemented for the Village, and a state-of-the-art PathRunner pavement condition data collection system was deployed to capture continuous, high-resolution pavement cracking, rutting, and roughness data of the Village’s roadways.

G&AI has since developed the PAVER inventory database and worked with the Village to collect additional pavement M&R records and M&R unit cost data with which to calibrate the PAVER database so that it is Village specific. These M&R records and M&R unit costs, along with the collected pavement condition data, have been used to identify present network-level M&R needs.

2.3 Project tasks

To successfully accomplish the objectives of this project, G&AI performed the following tasks, which are covered in greater detail in Sections 3, 4, and 5 of this report, respectively:

1. Pavement management system implementation
G&AI developed an inventory of the Village’s roadway pavements and implemented PAVER.
2. Pavement condition survey
G&AI performed a network-level pavement condition survey on the roadway pavements using a state-of-the-art pavement imaging and profiling data collection system. The pavement condition survey was performed in the fall of 2019 and spring of 2020.
3. M&R analyses
G&AI reviewed the collected condition data and determined the impact of several funding scenarios on the Village’s roadways and identified potential pavement M&R needs using PAVER.

The 3D pavement imaging and profiling technology used to assess the condition of the Village’s roadway pavements is the most comprehensive available. This technology has evolved rapidly over the past several years, and it is now used across the United States by more than half of the state DOTs. Unlike the inherently subjective windshield pavement condition surveys of years past, high resolution cracking, rutting, and roughness condition data were captured continuously for each of the Village’s roadways surveyed.

The collected data were then analyzed using a hybrid methodology that incorporates both automated crack detection and classification along with manual quality control. This approach yields a complete set of pavement condition data that may be used for both network-level (high-level budgeting) multi-year M&R planning as well as project-level (estimating M&R quantities) analyses. The collected data were then entered into and analyzed using PAVER. Continuously developed by the US Army Corps of Engineers, PAVER is a sophisticated, non-proprietary system widely used by municipal agencies across the United States and around the world.

2.4 Conceptual overview of pavement management

The use of a pavement management system is intended to provide municipal agencies with a systematic process for cost-effectively managing their pavement network, which may include roadways, parking lots, and alleys. The American Public Works Association (APWA) defines pavement management in the following way:

Pavement management is a systematic method for routinely collecting, storing, and retrieving the kind of decision-making information needed to make maximum use of limited maintenance (and construction) dollars.

Combined with local knowledge and practical judgment, the recommendations from a pavement management system may be used to help make better pavement M&R decisions.

At the core of a pavement management system is the method for assessing pavement condition. The most widely used method for assessing pavement condition is the Pavement Condition Index (PCI), which is industry standard practice and defined in ASTM D6433. The PCI method outlines a process for more objectively assessing the condition of a pavement based on visual observations and measurements that take place during a field inspection. These observations and measurements are then distilled into a PCI

value that ranges between 0 and 100. A PCI value of 0 indicates a failed pavement, and a PCI value of 100 indicates a pavement in good condition.

PCI values help determine the level of M&R needed to cost-effectively maintain or rehabilitate the pavement. These values may also be used to prioritize roadway improvements for the purpose of developing strategic capital improvements programs. When a pavement is in good condition, preventive maintenance can be applied to extend the life of the pavement. However, once a pavement falls below critical condition, preventive maintenance may no longer be cost effective, and more significant and perhaps more costly rehabilitation strategies should be considered.

The “Critical PCI” value for a pavement is the PCI value below which cost-effective preventive maintenance is no longer a viable option, and more significant rehabilitation and sometimes reconstruction may be necessary. As shown in Figure 2, the primary objective of pavement management is to preserve pavements in good condition above the Critical PCI with less costly preventive M&R rather than allow them to deteriorate below the Critical PCI, resulting in the need for more costly major M&R (rehabilitation or reconstruction).

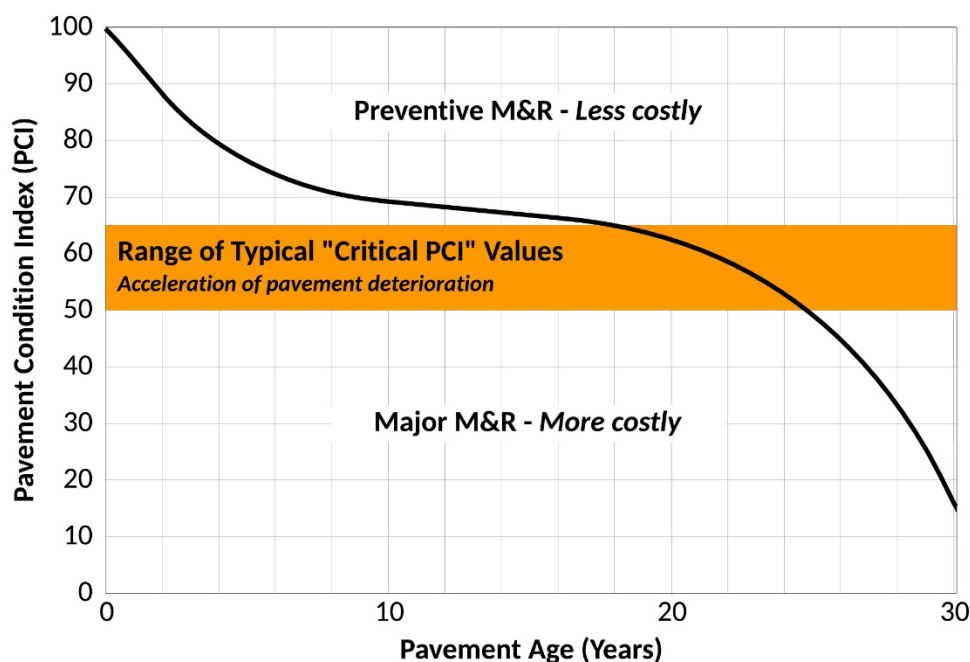


Figure 2. Example of the correct timing of preventive and major M&R relative to the Critical PCI.

The Critical PCI value is determined based on the repeated measurement of pavement condition over time as well as agency-specific M&R policies. Critical PCI values typically range between 50 and 65 (as shown in Figure 2) because the acceleration of pavement deterioration, and subsequent need for more costly M&R, typically occurs then. Setting a higher Critical PCI value simply results in pavements being recommended for major M&R earlier. Some agencies set higher Critical PCI values for their arterial roadways than for their local roadways to ensure that the roadways most heavily traveled (and often at higher speeds) are maintained to a higher standard.

PAVER’s default Critical PCI value of 55 has been used for the Village’s roadways. The Village may change this value as more condition data and historical M&R data are captured and the deterioration rates

of the Village’s roadways are better understood. Typically, two to three PCI inspections are needed to converge on acceptable Critical PCI values. The Village may choose to set Critical PCI values for each functional classification of roadway based on desired policy goals.

When the appropriate preventive maintenance treatments (e.g., crack sealing, seal coats, and patching) are undertaken at the correct times during a pavement’s service life, these relatively inexpensive preventive M&R treatments can extend the service life of the pavement, as shown in Figure 3.

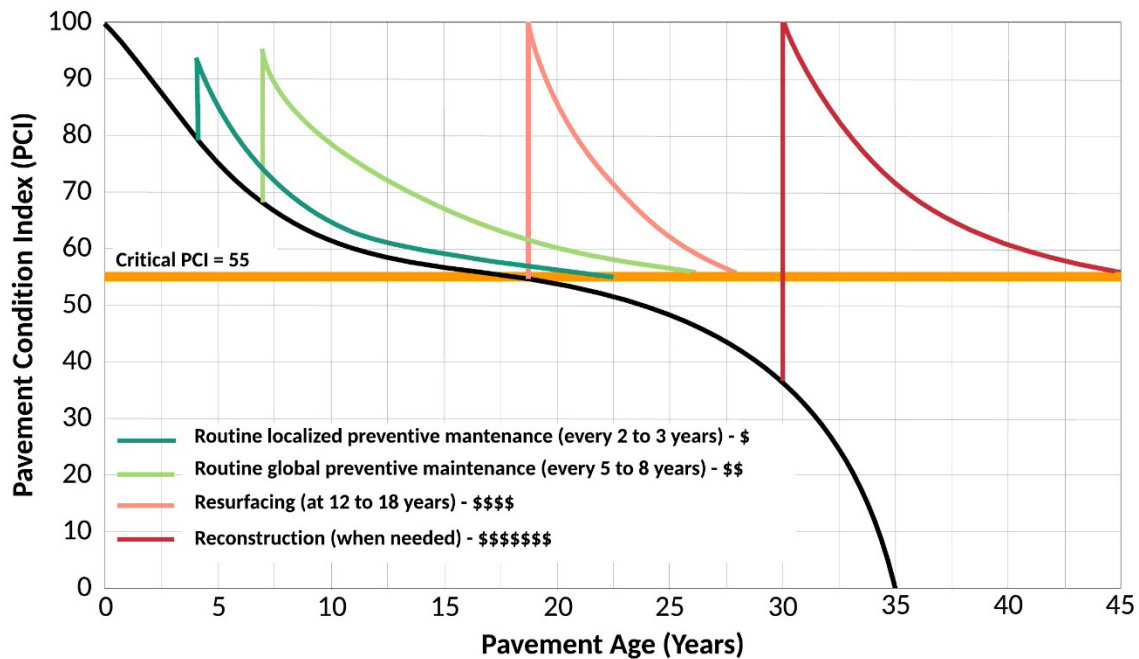


Figure 3. Example of the increasing prices and decreasing benefits of M&R.

It is important to note that the IRI, which provides a useful measure of pavement smoothness, does not correlate well to the level of M&R needed to correct smoothness issues. Consequently, IRI values are not considered when forecasting future M&R needs. Instead, IRI values are used in pavement management systems to identify pavements requiring a special inspection, or they may be used in conjunction with PCI values when prioritizing M&R projects.

As pavement management concepts have gained traction, computer-based pavement management systems have been developed to assist agencies in more optimally managing their pavements. Pavement management systems currently rely on a detailed pavement inventory, routine pavement condition assessments, pavement performance modeling, and sophisticated analysis tools that can forecast future pavement condition and estimate future M&R needs and costs.

2.5 Benefits and costs of implementing a pavement management system

Pavement management systems provide:

- A centralized location for storing pavement condition and inventory data, including construction, maintenance, and rehabilitation records.
- Decision-making support tools for:
 - ✓ Evaluating maintenance and rehabilitation alternatives.
 - ✓ Analyzing the consequences of alternative funding levels on pavement conditions.

- ✓ Improved scheduling and coordination of pavement M&R projects and other infrastructure projects.
- Analysis tools for evaluating the effectiveness of historical methods of rehabilitation.
- Reporting tools for distilling complex data and justifying funding needs to elected officials.

The benefits of implementing and maintaining a pavement management system improve over time as more data are entered into the system. The costs associated with maintaining a pavement management system include:

- Pavement inventory data collection and routine updates (typically performed annually following the end of the paving season).
- Routine pavement condition data collection (arterials and collectors are typically surveyed every other year and local roadways are surveyed on a three-year cycle).
- Evaluating pavement performance and developing M&R plans (typically performed annually following the end of the paving season – or following a condition survey – to determine candidate roadways for the next paving season).
- Software acquisition, installation, system maintenance, and updates.
- Staff training, as needed.

To ensure the success of a pavement management system, agencies should develop a plan for staffing, maintaining, and funding the system appropriately.

2.6 Incorporating pavement preservation strategies

The implementation of a pavement management system has the added benefit of assisting agencies in determining which pavements may be candidates for preventive maintenance. The use of preventive maintenance early in the life of a pavement, before any significant deterioration, has been demonstrated to be a cost-effective way to extend a pavement’s service life.

In the Federal Highway Administration (FHWA) publication, Pavement Preservation, A Road Map to the Future, preventive maintenance is defined as:

“...the planned strategy of cost-effective treatments to an existing roadway system and its appurtenances that preserves the system, retards future deterioration, and maintains or improves the functional condition of the system (without significantly increasing the structural capacity).”

The FHWA adds that preventive maintenance:

“...is typically applied to pavements in good condition having significant remaining service life. As a major component of pavement preservation, preventive maintenance is a strategy of extending the service life by applying cost-effective treatments to the surface or near-surface of structurally sound pavements.”

The following preventive maintenance treatments have been demonstrated to be effective when applied at the right time during a pavement’s service life:

- Crack sealing, crack filling, and joint sealing of flexible and rigid pavements
- Patching and edge repairs
- Chip seals, fog seals, and slurry seals
- Micro-surfacing
- Thin “functional” and “maintenance” overlay projects

Too frequently these activities are incorrectly applied as “stop-gap” or “cosmetic” treatments for pavements in poor condition rather than as true preservation activities. Preventive maintenance strategies should be applied to pavements that are in relatively good condition, and the activities should be planned and applied systematically following either the resurfacing or reconstruction of a pavement. The following FHWA website provides additional information for pavement preservation:
<https://www.fhwa.dot.gov/pavement/preservation/>.

2.7 Summary

This section provided the reader with background information pertaining to the creation and implementation of the non-proprietary PAVER system for the Village. The section provided a conceptual overview of pavement management and discussed:

1. The benefits the Village will see from the implementation of the pavement management system.
2. The costs expected to be incurred with the maintenance of the system.
3. The additional functionality beyond the obvious support the system can provide by objectively assisting the Village in optimizing the allocation of its M&R funding.

Implementation of the Village’s pavement management system is detailed in Sections 3, 4, and 5. This section closed with an overview of effective preventive maintenance strategies that should be considered by the Village moving forward.

3 PAVEMENT MANAGEMENT SYSTEM IMPLEMENTATION

3.1 Foreword

This section discusses the first task of this project: Implementing a pavement management system. One of the CMAP’s primary desires was to have a non-proprietary pavement management system for participating agencies. This section provides an overview of PAVER, a brief description of the modules available to the Village in PAVER, and insight into the PAVER database development.

(Note: The information presented in the section may be supplemented by the PAVER User Manual, which is available as a navigable PDF file in the PAVER software.)



3.2 Objective

The objective of this task was to implement a pavement management system for the Village’s roadway pavements. G&AI implemented PAVER, which is developed and continually updated by the US Army Corps of Engineers. This task required developing an inventory of the Village’s roadway pavements and collecting current pavement condition data and entering it in PAVER.

3.3 PAVER Pavement Management System overview

PAVER assists agencies in determining when, where, and what level of pavement M&R is required and approximately how much it will cost. The system provides a suite of pavement management tools, or “modules”, that will help the Village with the following tasks:

- Developing and organizing their pavement inventory.
- Assessing the current condition of their pavements.
- Developing models to predict future pavement conditions.
- Reporting on past and future pavement performance.
- Developing scenarios for M&R based on either funding or pavement condition goals.
- Planning M&R projects.

PAVER modules include:

- Inventory
- M&R history
- Inspection
- Prediction modeling
- Condition analysis
- M&R planning
- Project planning
- Reporting

A brief description of these modules is presented in the following sub-sections.

Note: Upon request by the municipality, a one-year PAVER license shall be purchased by CMAP for the municipality from Colorado State University (CSU). The PAVER license does not expire. However, after the first year, the municipality will be responsible for purchasing software updates and technical support, if desired. Current pricing for PAVER may be found at: www.paver.colostate.edu.

3.3.1 Inventory and maintenance and rehabilitation (M&R) history modules

The PAVER **Inventory** and **M&R History** modules, shown in Figure 4 and Figure 5, are based on a hierarchical structure composed of networks (groups of roadways managed with one source of funding), branches (specific roadways), and sections. Sections are the smallest area for which conditions are reported and M&R activities recommended. Sections typically conform to existing GIS segmentation and are commonly defined from intersection to intersection by default.

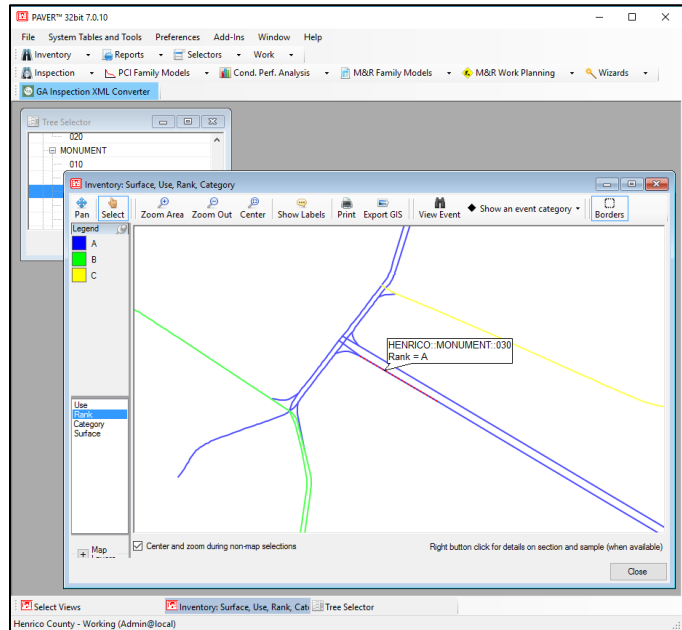


Figure 4. Example roadway functional classifications (ranks) stored in the Inventory module.

One network is defined for the Village and each roadway is a branch. Pavement sections are defined within each branch following the Village’s existing GIS segmentation in the Illinois Roadway Information System (IRIS). This structure allows the Village to easily organize their inventory and historical M&R data and provides a simple and efficient way for rolling-up data to higher levels of the pavement hierarchy. The Village provided G&AI with historical M&R records, and this information was entered in PAVER.

3.3.2 Inspection module

PAVER uses the PCI as the primary measure of pavement condition. The **Inspection** module, shown in Figure 6, enables agencies to store raw pavement condition survey data and then calculate PCI values. IRI values are also stored in the **Inspection** module.

3.3.3 Prediction modeling module

The **Prediction Modeling** module in PAVER enables the user to group pavements of similar construction that are subjected to similar traffic, weather, and any other factors affecting pavement performance into “families.” Historical pavement condition

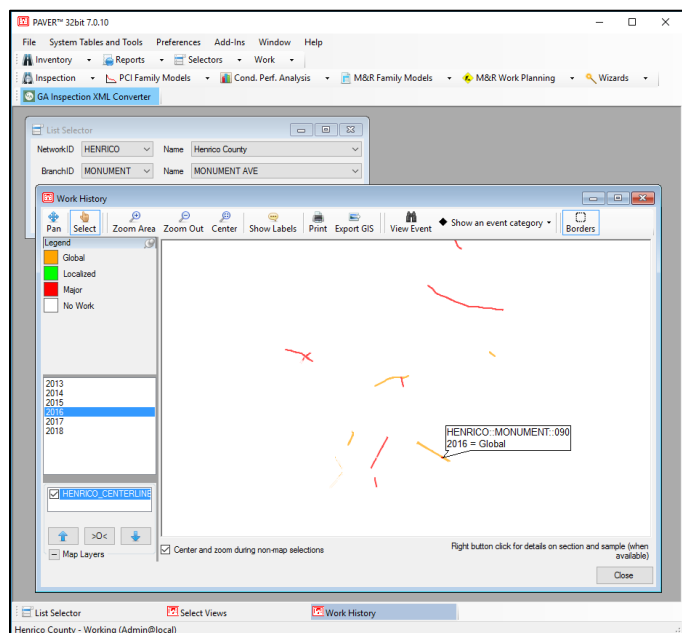


Figure 5. Example historical M&R records stored in the M&R History module.

data are used to build models that can be used to predict future pavement performance. The **Prediction Modeling** module is a hands-on module and prediction models should be updated by the Village following each condition survey. If historical pavement condition data are not available, PAVER provides default pavement prediction curves (shown in Figure 7) and allows the user to develop site specific prediction models.

3.3.4 Condition analysis module

The Condition Analysis module allows the Village to view the condition of the entire pavement network or any subset of the network over time. The module reports past conditions based on interpolated values between historical condition data, and it reports projected conditions based on the application of prediction models developed using the **Prediction Modeling** module.

3.3.5 M&R planning module

The **M&R Planning** module can determine the consequence of a predetermined funding level on pavement conditions and estimate the resulting backlog of major work. This information assists in determining funding requirements to meet specific Village pavement condition goals. These capabilities will enable the Village to develop more optimal M&R programs based on available resources and to justify M&R needs.

3.3.6 Reporting module

Each previously described module of PAVER can generate various reports that will assist the Village in analyzing, interpreting, and presenting pavement data. In addition to module-specific reports, PAVER also comes equipped with several “canned” reports, which include:

- GIS reports – *Internal/external reporting of inventory and condition data*
- Summary Charts – *Simple graphs and data tables of inventory and inspection data*
- Inspection Reports – *Summary of collected pavement condition data*
- Work History – *Summary of historical maintenance, repair, and rehabilitation data*
- Branch Listing – *Summary of overall pavement inventory data*

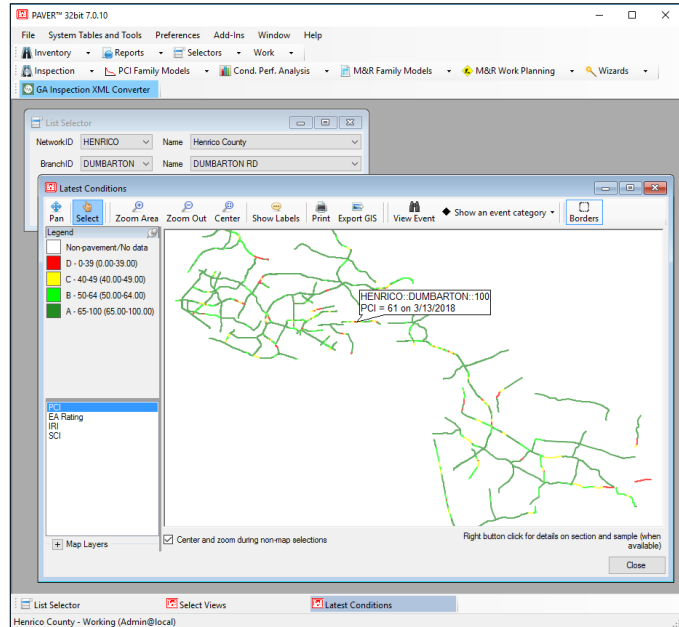


Figure 6. Example PCI values in the Inspection module.

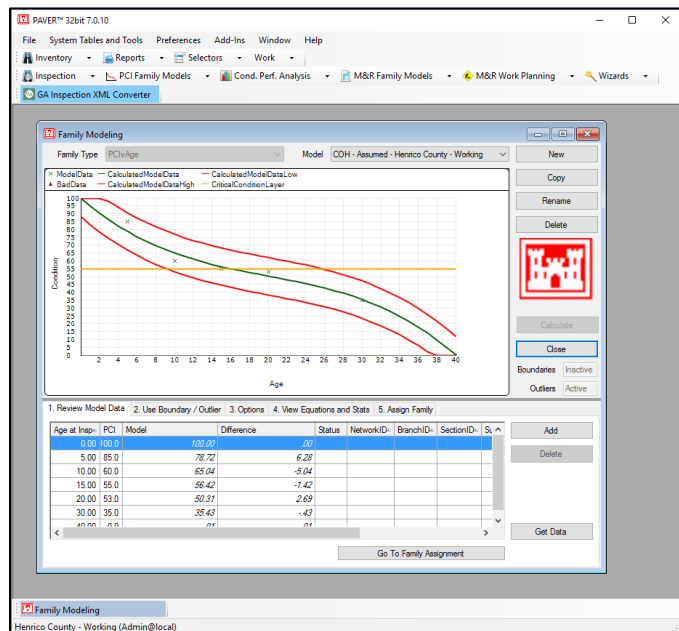


Figure 7. Example deterioration trend developed using the Prediction Modeling module.

- Branch Condition – *Summary of overall pavement condition data*
- Section Condition – *Summary of individual section data*

PAVER can generate on-the-fly “user-defined” reports, which can be tailored to meet the Village’s specific reporting needs. PAVER’s user-defined reporting capability enables the user to extract any data stored in the system and export it to a GIS shapefile, spreadsheet, or text file.

3.4 Summary

This section discussed the first task of this project: Implementing a pavement management system. This section provided an overview of the non-proprietary PAVER system, a brief description of the modules available to the Village in PAVER, and insight into the PAVER database development. The Village’s PAVER database has been developed to include specific and relevant data pertaining to the Village’s roadway pavement network. PAVER’s suite of analysis and planning tools will enable the Village to more effectively manage its roadway pavement network.

4 PAVEMENT INVENTORY

4.1 Foreword

This section describes the Village’s roadway pavement inventory as it exists in PAVER. The data sources used in developing the inventory are discussed in this section, and summary data are presented.

4.2 Objective

The objective of this task was to develop a comprehensive inventory of the Village’s roadway pavements for inclusion in PAVER. The roadway pavement inventory provides the underlying data on which analysis and reporting is performed with PAVER. In addition, the inventory provides the framework in which all routinely collected pavement condition data and historical work data are stored.

Moving forward, the Village should update the pavement inventory in PAVER to reflect the addition, realignment, widening, and/or removal of roadways managed by the Village. Typically, these types of changes are infrequent and may be done annually or prior to performing any analysis or reporting tasks with PAVER.

4.3 PAVER inventory development

The Village’s PAVER inventory was based on the IRIS GIS provided by CMAP. Relevant pavement data available in the IRIS GIS were supplemented with aerial imagery and field observations and entered in the Village’s PAVER database. These data included: number of lanes, pavement surface type, approximate roadway width, and from/to intersections for each pavement section.

Roadways were also assigned “ranks” (i.e., priorities) of primary (P), secondary (S), and tertiary (T). Federal aid eligible roads were assigned the rank of primary, since these tend to be the more heavily trafficked roadways. Residential roads were assigned the rank of secondary, and unpaved roadways and roadways in industrial zones were assigned the rank of tertiary.

A shapefile generated from the Village’s GIS was linked to the PAVER database. This enables the Village to conveniently navigate the roadways within PAVER and generate a variety of map-based inventory and condition reports in PAVER. Historical M&R records provided by the Village were entered in the PAVER database as well as unit cost data.

4.4 Inventory summary

The Village’s roadway network consists of approximately 92.9 centerline miles of predominantly asphalt surfaced, two-lane roadways. Table 2 shows the distribution of the Village’s roadway network in mileage and area by pavement rank, and Table 3 shows the distribution by pavement surface type.

Table 2. Roadway summary data by pavement rank.

Rank	Centerline Miles	Lane Miles	Area (SY)
Primary, P	6.9	19.2	140,161
Secondary, S	85.7	171.2	1,406,339
Tertiary, T	0.4	0.5	3,248
Total	92.9	190.9	1,549,748

Table 3. Roadway summary data by pavement surface type.

Surface Type	Centerline Miles	Lane Miles	Area (SY)
Asphalt, AC	92.2	189.7	1,540,555
Concrete, PCC	0.4	0.7	5,945
Gravel, GR	0.4	0.5	3,248
Total	92.9	190.9	1,549,748

Appendix A maps A-1 and A-2 present pavement rank and surface type data graphically.

5 PAVEMENT CONDITION INSPECTION

5.1 Foreword

This section discusses the second task of this project: Performing a comprehensive pavement condition survey of the Village’s roadways. The condition survey included the collection of high-resolution pavement imagery and profile measurements using a state-of-the-art PathRunner pavement condition survey system. The collected data were analyzed and PCI and IRI values were calculated for each of the Village’s roadways surveyed. This section describes the pavement condition survey system, the data collection methodology, how the collected data were analyzed, and a discussion of field observations. It concludes with several examples of pavement conditions from the Village’s roadways.

5.2 Objective

The objective of the pavement condition survey is to assess the existing structural integrity and surface operational condition of the Village’s roadways. The survey provides a comprehensive snapshot of pavement conditions at the time of data collection.

Moving forward, the Village should perform pavement condition surveys on a routine basis to objectively monitor pavement performance, determine near-term M&R needs, evaluate the effectiveness of M&R activities, develop pavement deterioration trends, and forecast near- and long-term pavement M&R needs.

5.3 Pavement condition data acquisition

G&AI deployed a state-of-the-art PathRunner pavement data collection system to capture high-resolution pavement imagery and surface data necessary to assess the condition of the Village’s roadways. The PathRunner system is shown in Figure 8.

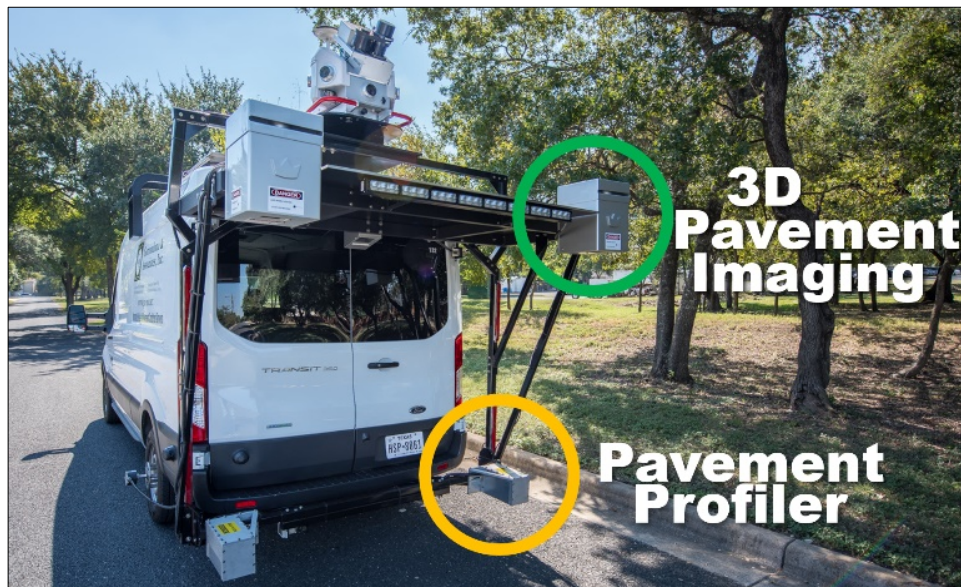


Figure 8. PathRunner pavement condition data collection system.

The PathRunner was driven on all roadways within the Village. By agreement with CMAP, only a single lane of two-lane roadways was collected and the outermost lanes in both directions of four-lane and greater roadways were collected. Based on G&AI’s experience, contiguous lanes are usually of similar

character, and this inspection approach was deemed to be cost effective for the Village while still providing sufficiently detailed information to assess existing pavement conditions. The PathRunner system continuously collected the following data for each roadway:

- High-resolution 2D and 3D pavement images for evaluating pavement distresses and determining Pavement Condition Index (PCI) values.
- Transverse profiles to measure rutting.
- Longitudinal profiles to calculate International Roughness Index (IRI) values.
- High-resolution, forward-facing, right-of-way images for manual review of all data.

These data were processed using automated tools verified by manual review to assess pavement conditions, and the results were entered in the Village’s PAVER database.

5.4 Pavement Condition Index (PCI) method

The pavement condition survey was performed following the PCI method. The PCI method is based on a set of definitions and procedures for measuring pavement distress types, severities, and quantities during a field inspection. This information is then distilled into a PCI value, which provides an indication of the structural integrity and surface operational condition (roughness) for a pavement section. The PCI method is widely used and provides a significantly more objective and repeatable method for assessing pavement condition than inherently subjective windshield surveys commonly used in the past.

The Village’s roadway network consists primarily of asphalt pavements with only a few concrete and gravel roadways. During a PCI inspection, several distress types are identified and evaluated for asphalt pavements, as shown in Table 4. The severity and quantity of each observed distress is recorded, and these data are then input into the PCI algorithm to calculate a PCI value, as shown in Figure 9.

Table 4. Asphalt and concrete pavement distress types.

Asphalt Pavement Distresses		Concrete Pavement Distresses	
Distress	Cause	Distress	Cause
Alligator Cracking	Load	Blowup/Buckling	Climate/Durability
Bleeding	Other	Corner Break	Load
Block Cracking	Climate/Durability	Divided Slab	Load
Bumps and Sags	Other	Durability ("D") Cracking	Climate/Durability
Corrugation	Other	Faulting	Other
Depression	Other	Joint Seal Damage	Climate/Durability
Edge Cracking	Load	Lane/Shoulder Drop-Off	Other
Joint Reflection Cracking	Climate/Durability	Linear Cracking	Load
Lane/Shoulder Drop-Off	Other	Patching, Large and Utility Cuts	Other
Longitudinal and Transverse Cracking	Climate/Durability	Patching, Small	Other
Patching and Utility Cut Patching	Other	Polished Aggregate	Other
Polished Aggregate	Other	Popouts	Other
Pothole	Load	Pumping	Other
Railroad Crossing	Other	Punchout	Load
Rutting	Load	Railroad Crossing	Other
Shoving	Other	Scaling, Map Cracking, and Cracking	Other
Slippage Cracking	Other	Shrinkage Cracks	Climate/Durability
Swell	Other	Spalling, Corner	Climate/Durability
Raveling	Climate/Durability	Spalling, Joint	Climate/Durability
Weathering	Climate/Durability		

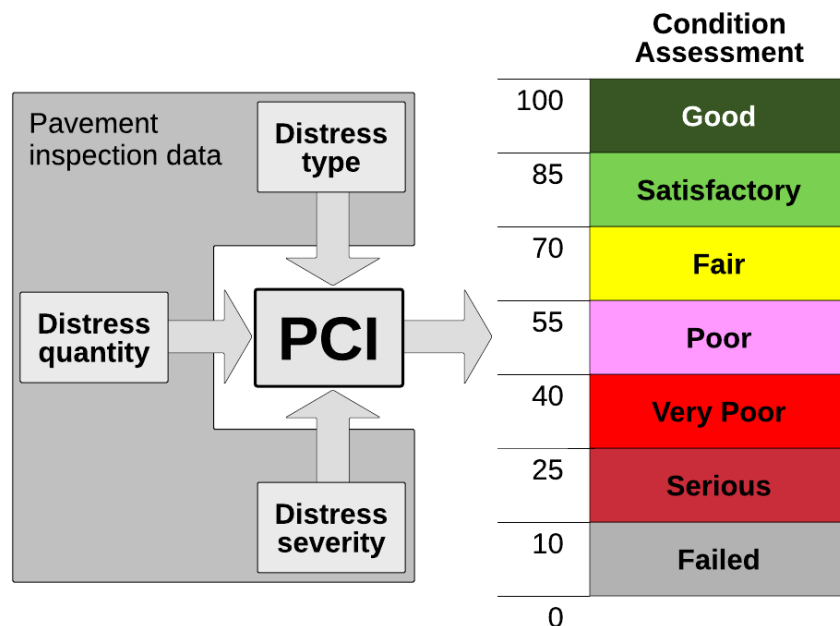


Figure 9. PCI inputs and the Village’s assessment scale.

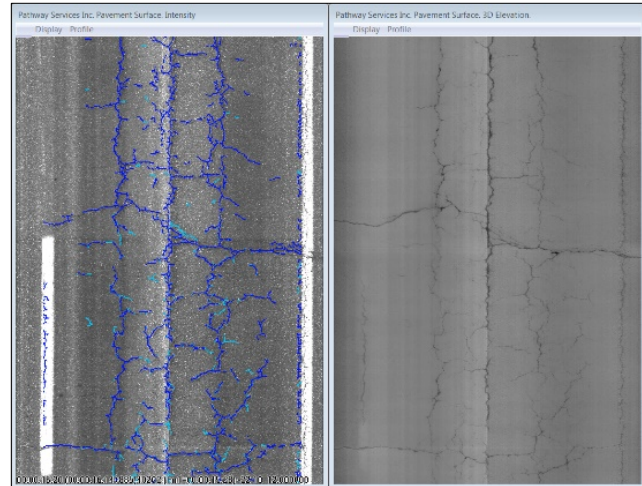
If properly designed and constructed, a new pavement begins its service life with a PCI of 100. Because of distress caused by vehicle loads, environmental factors, and aging, a pavement deteriorates over time. For each combination of distress type, severity level, and quantity observed during the inspection, points

are deducted from the initial value of 100, thereby decreasing the PCI. When multiple distresses are present, the “deduct values” are modified such that the impact of multiple distresses is not unnecessarily compounded. Due to the complexity of the PCI algorithm, PCI values are typically computed using a pavement management software package, such as PAVER. It is important to note that the PCI method does not directly measure the load carrying capacity or the rideability of a pavement. Structural testing combined with coring is needed to determine permissible pavement loadings.

5.5 Pavement Condition Index (PCI) data interpretation

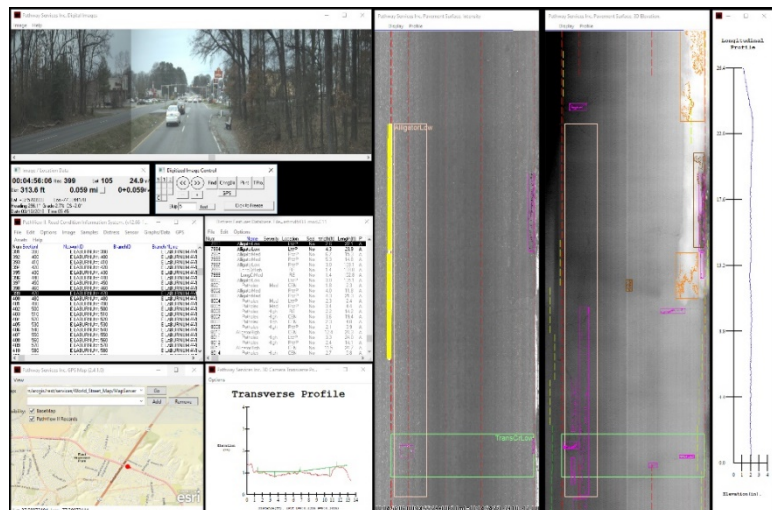
The PathRunner system captures 2D and 3D images of the roadway surface from which pavement surface distresses are evaluated. During the data collection effort, G&AI extracted pavement distress data from georeferenced digital images and rutting measurements from transverse profile measurement to determine PCI values. This process involves four distinct steps:

1. **AutoCrack Software** – This software detects cracking in the pavement imagery.
2. **AutoClass Software** – This software classifies the type of cracking detected.
3. **Manual image rating** – G&AI’s team of trained and experienced raters review the imagery and identify any distress types that the automated crack detection and classification software did not observe or incorrectly identified. Performing this manual image rating is considered the Quality Control (QC) review assuring detailed accuracy and completeness of the ratings.
4. **Quality Assurance (QA) rating** – An independent team of G&AI’s raters and project engineers perform a systematic QA review of the rated data to ensure proper evaluation of the collected imagery prior to import into PAVER.



Steps 1 and 2: Initial Automated Crack Detection and Rutting Analyses

The QC and QA ratings are the most important steps in the project. G&AI uses the PathView software for evaluating distresses using both automated algorithms and manual supplemental rating. All QC/QA is performed by highly trained and experienced engineers and technicians using PathView. The same software system has been used for more than 25 state DOTs and several municipal agency pavement condition survey projects and is a well proven review tool.



Steps 3 and 4: Manual Rating and QC/QA of Pavements using PathView

In addition to capturing 2D and 3D imagery from which pavement surface

distresses are evaluated, the PathRunner system also captures high-resolution longitudinal and transverse profiles of the roadway surface at 2mm intervals. The longitudinal profile data are analyzed to determine the IRI values, or the “roughness” of the roadway, and the transverse profiles are used to measure rutting.

5.6 Existing pavement conditions and field observations

The collected pavement survey data were used to calculate a PCI value for each pavement section in the Village. Table 5 shows the pavement condition assessment criteria used to analyze the pavement network.

Table 5. Village’s pavement condition categories.

Category	Typical Distresses and Typical Level of M&R Needed	PCI Range
Good	Longitudinal and transverse cracking and weathering of surface Preventive maintenance: <i>Crack sealing and surface treatments</i>	86-100
Satisfactory	More extensive longitudinal and transverse cracking and weathering of surface Preventive maintenance: <i>Crack sealing and surface treatments</i>	71-85
Fair	Extensive longitudinal and transverse cracking, early stage alligator (fatigue) cracking, early stage rutting, and weathering of surface Global preventive maintenance and localized repairs: <i>Localized surface and/or full-depth patching, surface treatments, and thin overlays</i>	56-70
Poor	More extensive and severe longitudinal and transverse cracking, alligator (fatigue) cracking, rutting, and weathering of surface Major rehabilitation: <i>Localized full-depth patching, mill and overlays, and traditional overlays</i>	41-55
Very Poor	More extensive and more severe longitudinal and transverse cracking, alligator (fatigue) cracking, rutting, weathering of surface, potholes Major rehabilitation: <i>Full-depth patching, mill and overlays, traditional overlays, and reconstruction</i>	26-40
Serious	Extensive and severe failure of pavement surface Major rehabilitation: <i>Reconstruction</i>	11-25
Failed	Complete failure of pavement surface Major rehabilitation: <i>Reconstruction</i>	0-10

At the time of G&AI’s inspection, the Village’s pavements were found to be in overall “fair” condition and have an average PCI of 60. The condition distribution of the Village’s pavements at the time of inspection is shown in Figure 10, and detailed condition maps can be found in Appendix A.

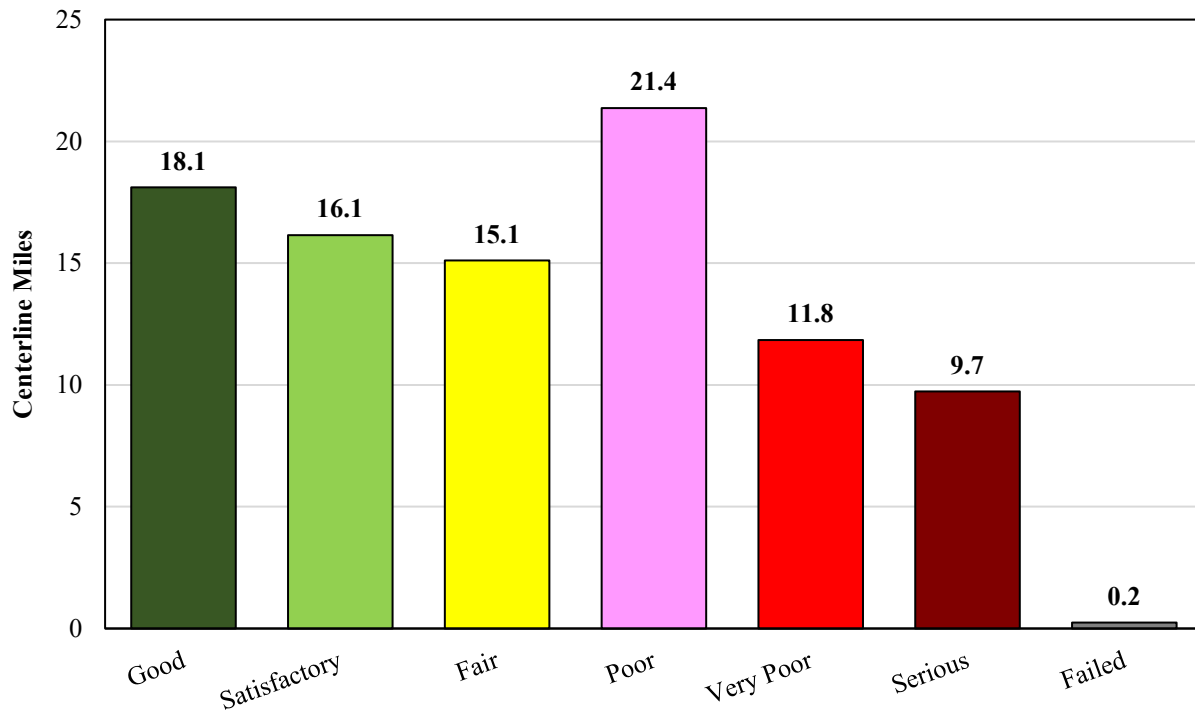


Figure 10. Village's roadway pavement condition distribution by PCI category.
 (Note: Excludes gravel roadways.)

Pavement condition data summarized by pavement ranks and surface types are presented in the following two tables, respectively.

Table 6. Roadway summary condition data by pavement rank.

Rank	Centerline Miles	Lane Miles	Area (SY)	PCI	IRI
Primary, P	6.9	19.2	140,161	67	221
Secondary, S	85.7	171.2	1,406,339	60	308
Tertiary, T	0.4	0.5	3,248	--*	--*
Total	92.9	190.9	1,549,748	60	300

*Note: Tertiary roads were gravel and PCI values are not applicable.

Table 7. Roadway summary condition data by pavement surface type.

Surface Type	Centerline Miles	Lane Miles	Area (SY)	PCI	IRI
Asphalt, AC	92.2	189.7	1,540,555	60	299
Concrete, PCC	0.4	0.7	5,945	77	485
Gravel, GR	0.4	0.5	3,248	--*	--*
Total	92.9	190.9	1,549,748	60	300

*Note: PCI values are not applicable to gravel roads.

The causes of pavement deterioration as quantified by the PCI may be divided into three general categories:

- Vehicle load related.
- Climate/durability related.
- Other (construction defects and material issues).

Pavement deterioration and ultimate failure is a complex process that often involves a combination of several deterioration mechanisms working together. The deterioration observed on the Village’s pavements was caused primarily by a mixture of load- and climate-related distresses. Vehicle load-related distresses, including alligator cracking and rutting, were pronounced on many of the Village’s roadways and accounted for most of the distress negatively impacting overall roadway conditions. In addition, climate-related distresses, including longitudinal and transverse cracking and block cracking, were found across the Village’s pavement inventory.

In practice, visually observed pavement distresses collected during a network-level condition survey are used to determine the likely mechanism(s) contributing to the deterioration of a roadway. However, prior to developing a specific M&R strategy, the root cause of pavement deterioration should be determined. Determining the root cause of pavement deterioration may be accomplished through an appropriate combination of traffic load analyses, drainage investigations, structural testing, coring, and material testing.

For example, vehicle load-related distresses such as alligator cracking may be addressed through load analyses and material testing. Contributing root causes may range from the roadway consistently exposed to loads in excess of its design loading to the pavement section having simply reached the end of its design life. Climate/durability-related distresses, such as transverse cracking, may result from a combination of freeze/thaw cycling and oxidation (embrittlement) of the asphalt layer. The cause(s) of “other” distresses may be determined through a combination of coring, boring, and material testing.

In addition to PCI values, IRI values were determined for each of the Village’s roadways. IRI values, reported in inches per mile, describe the amount of roughness in both wheel paths over a given length of pavement. The IRI is a standard measure of roughness used worldwide. The Village’s IRI assessment scale is shown in Table 8.

Table 8: Village’s IRI assessment criteria.

Category	IRI Value
Smooth	0-200
Marginal	201-400
Rough	>401





At the time of G&AI’s inspection, the Village’s pavements were found to be in overall “marginally smooth” condition, with an average IRI of 300. Detailed condition maps can be found in Appendix A.

It is worth noting that IRI and PCI values do not necessarily correlate with one another. A roadway can ride well yet still be structurally deficient and in need of major M&R, and vice versa. For example, asphalt-surfaced roadways supported by structurally adequate base (e.g., crushed rock) and subgrade (e.g., existing soil) layers may exhibit extensive cracking in the asphalt surface layer due to fatigue failure of the asphalt. In situations such as these, removal of the existing asphalt layer and replacement with a thicker layer may be enough to rehabilitate the pavement. Conversely, a roadway that rides poorly may be structurally adequate and may only require minimal rehabilitation. Poor construction practices may unfortunately lead to roughness being “built into” an otherwise structurally adequate roadway at the time

of construction. Roadways exhibiting this type of roughness may require grinding and/or an additional surface course to remedy the issue.

5.7 Example pavement conditions through the Village

Figure 11 illustrates a variety of pavement conditions observed throughout the Village during the pavement condition survey. The figure includes PCI and IRI values for each pavement section along with observed distress types and recommended M&R.

	Location + History	PCI (IRI)	Recommended M&R Activity (Typical)
	<p>Chappel Ave. (Section 20)</p> <p>Last resurfacing date unknown</p>	<p>94 (170)</p>	<p>Preventive maintenance</p> <p>Seal joints between pavement and curb and gutter.</p>
	<p>177th St. (Section 10)</p> <p>Last resurfacing date unknown</p>	<p>82 (332)</p>	<p>Preventive maintenance</p> <p>Seal cracks as well as paving lane joint and joints between pavement and curb and gutter + surface treatment.</p>
	<p>Hickory St. (Section 40)</p> <p>Last resurfacing date unknown</p>	<p>60 (343)</p>	<p>Preventive maintenance</p> <p>Seal cracks as well as paving lane joint and joints between pavement and curb and gutter + localized patching <u>or</u> cold mill and overlay</p>
	<p>Paxton Ave. (Section 40)</p> <p>Last resurfacing date 2015</p>	<p>50 (273)</p>	<p>Major M&R</p> <p>Localized structural patching + cold mill and overlay <u>or</u> reconstruction</p>




	<p>177th St. (Section 130) <i>Last resurfacing date unknown</i></p>	<p>26 (367)</p>	<p>Major M&R Reconstruction</p>
	<p>DeJong Ln. (Section 20) <i>Last resurfacing date unknown</i></p>	<p>23 (371)</p>	<p>Major M&R Reconstruction</p>
	<p>Dante Ave. (Section 10) <i>Last resurfacing date unknown</i></p>	<p>8 (205)</p>	<p>Major M&R Reconstruction</p>

Figure 11. Pavement conditions observed during PCI inspection.

A distress observed on some of the Village’s pavements was unsealed paving lane seams (cracks), as shown in several of the photos above. If left unsealed, paving lane seams can deteriorate rapidly and significantly reduce the life of the pavement. By sealing paving lane seams immediately following paving and routinely resealing them, this type of deterioration may be minimized or prevented.

5.8 Summary

This section presented an overview of the methodology used to perform the 2019/2020 pavement condition survey and the results of the survey. A state-of-the-art PathRunner pavement condition survey system was deployed to collect pavement imagery and profile data on the Village’s roadways. The collected data were analyzed, and PCI values and IRI values were determined for each of the roadways surveyed. The Village’s roadways were found to be in overall “fair” condition with an average PCI of 60. Furthermore, the Village’s roadways were found to be in overall “marginally smooth” condition, with an average IRI of 300 inches/mile.

6 MAINTENANCE AND REHABILITATION FUNDING ANALYSES

6.1 Foreword

This section discusses the third task of this project: M&R needs analyses. This section discusses the results of the analyses performed for the Village’s consideration, assumptions which shaped the analyses, and results of the analyses. The recommendations of these analyses are provided in this section and in Appendixes A through D.

6.2 Objective

The M&R Planning module in PAVER provides *raw recommendations* of when and where pavement M&R activities are needed and approximately how much they will cost. The Village should use these raw recommendations to develop programmatic M&R plans for the Village’s roadway network. These programmatic plans may be generated based on anticipated annual funding or with the goal of maintaining or achieving a desired pavement condition.

For the Village’s roadways, two preliminary M&R analyses were performed:

- A series of **five-year analyses** was performed to determine the impact of several funding levels on overall roadway conditions. The analyses included:
 - Assessing the impact of the Village’s existing funding level.
 - Determining the annual funding level needed to maintain the Village’s existing overall average roadway condition.
 - Determining the annual funding level needed to modestly increase the Village’s overall average roadway condition to approximately 65.
 - Determining the annual funding level needed to eliminate the Village’s major M&R backlog over a five-year period.
- A **one-year analysis** was performed to identify pavements that may benefit from preventive maintenance activities, such as crack sealing or localized patching. Only pavements with a PCI of 65 or better were considered in this analysis.

The purpose of these analyses is to determine the appropriate funding level needed to manage the Village’s roadways and provide general recommendations that will assist the Village in developing and evolving its M&R program. Additional analyses may be performed to assess either the impact of anticipated funding levels or to determine the funding levels needed to achieve a desired overall, network-average condition.

6.3 Assumptions

The M&R analyses were based on the results of the fall of 2019 and spring of 2020 Pavement Condition Index (PCI) survey and the pavement inventory and historical work records provided by the Village and stored in the Village’s PAVER database. The following assumptions were made in our analyses.

- Pavements considered candidates for preventive maintenance were determined based on their overall PCI values and the distresses observed on the pavement at the time of inspection. Pavements with PCI values of 65 or better were considered candidates for preventive maintenance.
- Recommended preventive maintenance policies for asphalt and concrete pavements are shown in Appendix D Tables D-1 and D-2, respectively. The policy tables show what type of repair activity should be applied to each distress type and severity combination. Table D-3

presents estimated unit costs for the maintenance activities recommended in tables D-1 and D-2.

- A pavement deterioration rate of roughly 2.5 points per year was used based on the performance of the Village’s resurfaced roads, which equates to a pavement life between resurfacings of approximately nineteen years. This deterioration rate will be refined as more historical work records are entered in PAVER and more PCI inspection data become available over time.
- A Critical PCI value (the PCI value below which a pavement is considered a candidate for major M&R) of 55 was assumed for all pavement sections. Pavements at or below the Critical PCI during the five-year analysis period triggered major M&R recommendations. *(Note: A PCI value of 55 has been initially chosen for all the Village’s roadways as this numerical value straddles the “Fair” to “Poor” condition categories in the Village’s PCI scale. Performing major M&R on pavements that are closer to a PCI of 55, rather than waiting for these pavements to deteriorate further is generally more cost effective.)*
- Unit costs used in these analyses were based on bid tabs provided by the Village and by costs reported by nearby municipalities.
 - ✓ Asphalt resurfacing ranged from approximately \$1.50 to more than \$5.00 a square foot depending roadway condition (i.e., lower PCI values may result in more patching and thicker resurfacing). Reconstruction was set at \$6.50 a square foot.
 - ✓ Concrete slab replacement costs ranged from \$5.00 to \$15.00 a square foot depending on roadway condition (i.e., lower PCI values result in more slab replacement). Reconstruction was set at \$20.00 a square foot.
- All analyses began in the fall of 2020 (November 1 start date), and an inflation rate of 3% was assumed.

6.4 Results

The results of the PAVER M&R analyses are shown in the following two figures. Figure 12 illustrates the estimated five-year change in pavement condition resulting from the analyzed funding scenarios, and Figure 13 depicts the estimated change in the Village’s major M&R backlog for each funding scenario.

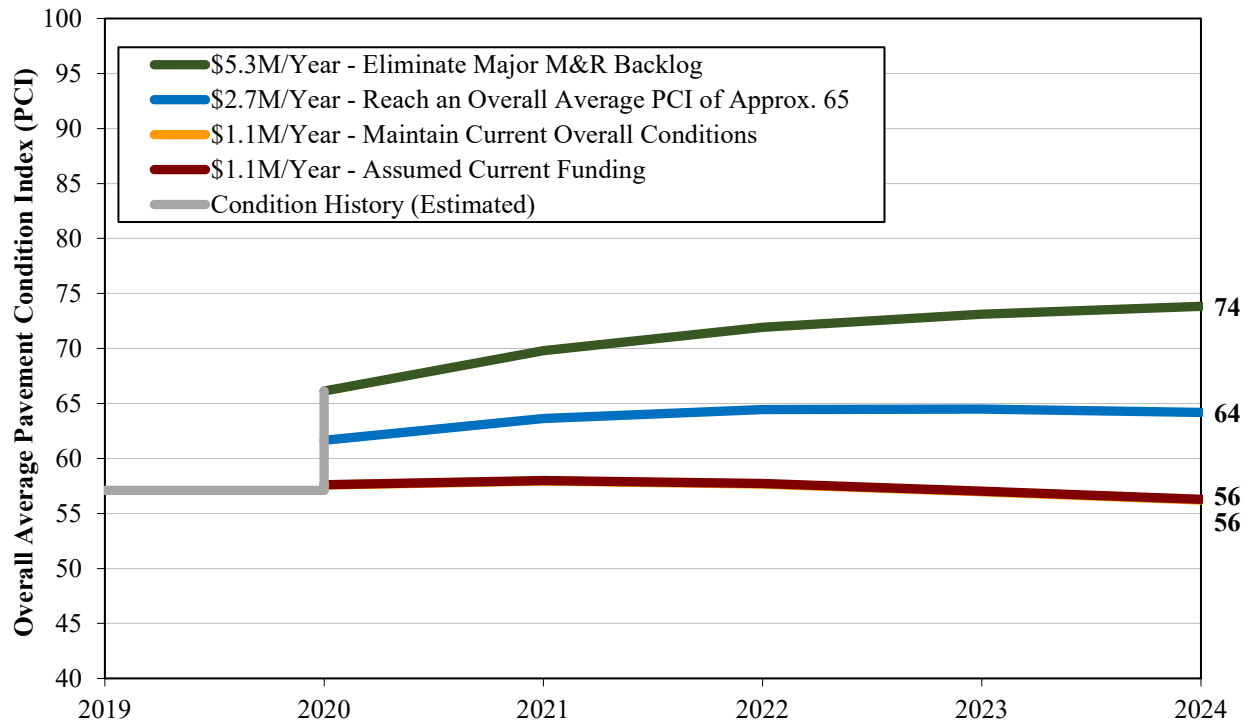


Figure 12: Impact of funding levels on overall pavement conditions by year.

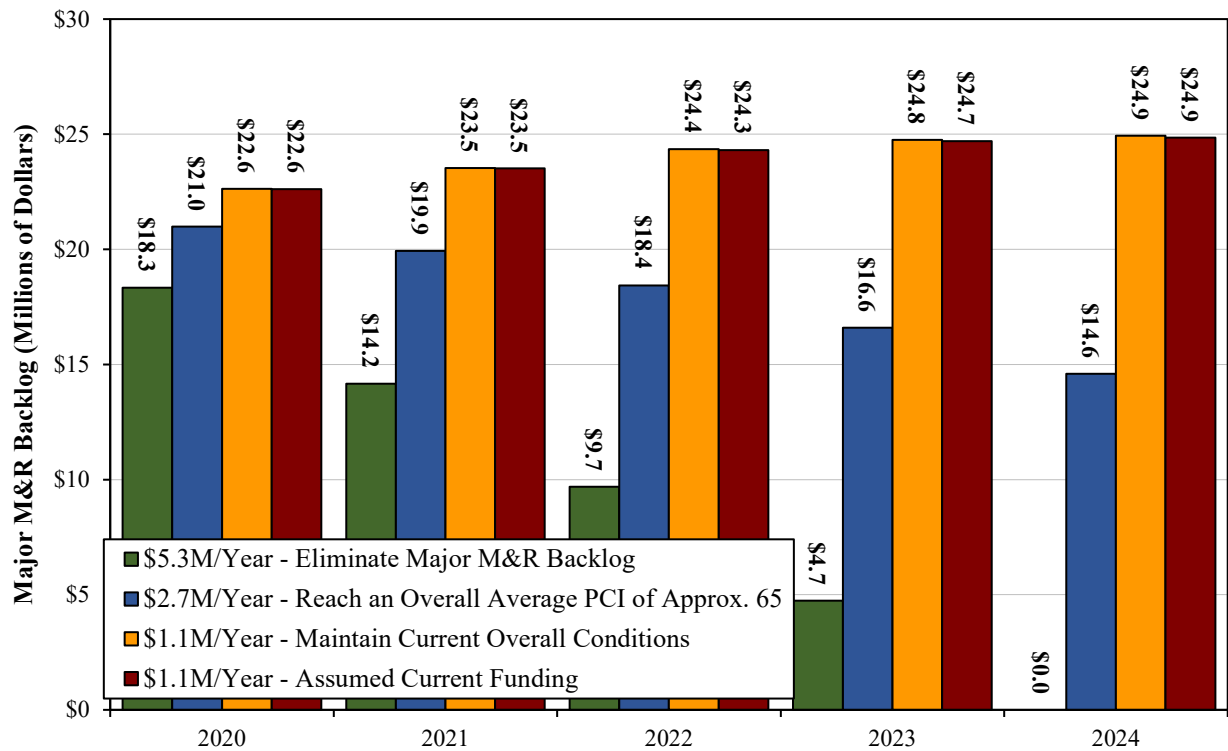


Figure 13: Impact of funding levels on major M&R backlog by year.

The consequences of the annual funding scenarios are shown in Table 9. This table illustrates the concept of “total cost.” By treating both the total annual M&R expenditures and the remaining major M&R backlog at the end of the five-year period as costs to the Village, the benefit of increasing annual funding – which results in a smaller major M&R backlog – is clearly illustrated. Consequently, eliminating the major M&R backlog over a five-year period results in the lowest total cost to the Village.

Table 9. Estimated Five-Year Pavement M&R Costs

Funding Scenario	Total Five-Year M&R Costs (2020-2024)	Remaining M&R Backlog ¹⁾ (2024)	Total Five-Year Cost ²⁾	Projected PCI (2024)
\$1.1M/YR (Assumed Current Funding)	\$5.50M	\$24.86M	\$30.36M	56
Maintain Existing Overall Average Conditions (\$1.09M/YR)	\$5.45M	\$24.94M	\$30.39M	56
Increase Overall Average PCI to Approximately 65 (\$2.7M/YR)	\$13.7M	\$14.6M	\$28.30M	64
Backlog Elimination (\$5.3M/YR)	\$26.4M	\$0	\$26.40M	74

- 1) “M&R Backlog” equals the lump-sum cost to resurface/reconstruct all pavements at or below their critical PCI value.
- 2) “Total five-year cost” equals the sum of the five-year major M&R expenditures plus the remaining major M&R backlog at the end of the five-year analysis period.

Appendix A maps A-5 and A-6 present major M&R recommendations. Map A-5 shows all roadways recommended for major M&R over the upcoming five years based on the Village’s existing funding level. Map A-6 shows all roadways recommended for major M&R over the upcoming five years given an unlimited budget. The maps show which roadways are recommended each year by PAVER. These recommendations do not consider geographic proximity. Consequently, these recommendations should be grouped into practical projects during the Village’s planning process.

Map A-7 shows all roads that are candidates for preventive maintenance, such as crack sealing and localized patching. While crack sealing can be an effective treatment for preserving roadways in good condition, its utility diminishes when applied to roadways that are already in poor condition or are exhibiting signs of structural failure.

Appendix B presents tabular data showing the estimated cost to repair each of the roads recommended for major M&R over the next five years based on the Village’s existing funding level. Appendix C presents similar data assuming unlimited funding. *The costs presented in Appendixes B and C should be considered rough estimates only and should not be considered engineering estimates.* These costs are based on a simple relationship between predicted PCI value and typical level of major M&R. Unit costs used in developing these relationships were based on bid tabs provided by the Village and by costs reported by neighboring municipalities.

Appendix E presents tabular data showing one-year estimated costs to apply preventive maintenance to each of the candidate roadways (i.e., roadways with PCI values of 65 or better). The total one-year preventive maintenance cost is estimated to be approximately \$472,000, as shown in Table 10. *The estimated costs presented in Appendix E should be considered rough estimates based on the assumed unit costs only and should not be considered engineering estimates.*

Table 10. Preventive Maintenance Summary

Maintenance Type	Quantity	Units	Est. Cost
Patching - AC Shallow	4,574	SF	\$25,158
Patching - AC Deep	27,121	SF	\$298,336
Crack Sealing - AC	143,516	FT	\$143,515
Joint Seal (Localized)	3,596	FT	\$5,395
Total:			\$472,403

7 SUMMARY AND RECOMMENDATIONS

7.1 Summary

A pavement condition survey was performed in the fall of 2019 and spring of 2020 on the Village’s roadways. The results of the survey provide a snapshot of roadway conditions at the time of the survey. PAVER was implemented for the Village’s roadways and was populated with collected pavement condition data and available M&R history data provided by the Village.

For the Village to get the most return on investment out PAVER, the system must be considered a living entity and be updated regularly with M&R activities as they are performed, M&R unit cost data, and routinely collected pavement condition data. With such attention, PAVER becomes a repository of accurate, up-to-date data and can aid the Village in more cost-effectively programming M&R funding and objectively analyzing the true cost-effectiveness of presently employed M&R activities.

Five-year M&R funding analyses were performed on the Village’s roadways using PAVER to: 1) evaluate the adequacy of the Village’s existing funding level, 2) estimate the funding level needed to maintain the Village’s existing roadway conditions, 3) estimate the funding level needed to modestly raise the overall condition of the Village’s roadways, and 4) estimate the funding level needed to eliminate the Village’s backlog of major M&R.

It was determined that the Village’s existing funding level for major M&R is likely adequate to maintain the current condition of the Village’s roadway pavements. To maintain existing conditions, a slight increase in funding may be needed.

Based on this initial set of PCI data collection and analysis on the Village’s roadways, G&AI respectfully offers the following broad recommendations.

7.2 Recommendations

7.2.1 Implement pavement preservation techniques

As discussed in Section 2.6, preventive maintenance activities, such as crack sealing, localized patching, and surface treatments, can cost-effectively extend the life of a pavement. The Village should incorporate these strategies into its M&R planning.

The Village does not appear to have an active crack sealing program for its roadways. Moisture penetrates unsealed cracks and compromises the base structure of the pavement. Freeze/thaw cycling exacerbates the damage. Sealing cracks on roadways that are in relatively good condition is a simple, cost-effective method for pavement preservation. Crack sealing is a preventive maintenance activity and should not be applied on roadways that require major M&R.

Furthermore, the Village should focus on applying routine preventive maintenance to newly resurfaced or reconstructed roadways. It was observed that some paving lane seams throughout the Village had not been sealed. Like crack sealing, sealing the paving lane seams is a simple method for pavement preservation, and it may be included in construction specifications.

7.2.2 Determine when pavements should be reconstructed rather than resurfaced

As the Village’s asphalt-surfaced pavements age and are resurfaced multiple times, the performance of successive resurfacing projects will diminish. These “diminishing returns” occur because the sublayers of

the pavement (the pavement structure below the asphalt surface) continue to deteriorate due to moisture infiltration, freeze-thaw damage, and damage due to vehicular loading. The M&R history and performance of resurfaced roadways should be closely tracked to determine the optimal number of resurfacing projects that may be performed prior to reconstructing the pavement.

7.2.3 Perform regular pavement condition inspections – every three years

To capitalize on the pavement condition survey and better track the condition of its pavements, the Village should continue to perform PCI surveys on a regular, three-year cycle. Doing so will enable the Village to:

1. Better track the deterioration of its pavements over time,
2. Identify pavement deterioration trends and use these trends to better predict future pavement conditions and then strategically apply M&R funding, and
3. Assess and track the effectiveness of its pavement preservation and major M&R activities.

The deterioration trends developed for this project were based on only one set of inspection data. Additional inspection data will help validate these trends and will improve forecasts, which may impact forecasted pavement conditions and recommended future M&R funding needs.

7.2.4 Routinely update PAVER

PAVER should be updated annually following the paving season to capture major M&R activities, routine maintenance activities, and pavement inventory changes (new roadways, jurisdictional changes, realignments). PAVER relies on updated inventory and work history data in order to generate meaningful recommendations.

7.2.5 Increase funding for pavement M&R

Based on the results of the pavement condition survey and forecasts of future pavement condition, the Village’s current level of funding is likely adequate to maintain the overall current condition of the Village’s roadways. Managing a pavement network at an overall average PCI between 70 and 80 is more cost effective since funding is spent on less costly preventive maintenance and preservation activities rather than more expensive major M&R. As the Village moves forward, it is recommended that additional funding be allocated for M&R to improve the overall condition of the roadways so that they may be managed more cost-effectively.

7.2.6 Prioritize existing M&R funding to maximize shared benefit

Currently, the Village’s roadway M&R funding needs exceed available funding. The Village should focus major M&R activities on its most trafficked roadways. Doing so will maximize the overall shared benefit of the funds spent.

APPENDIX A – PAVEMENT INVENTORY, CONDITION, AND RECOMMENDED M&R MAPS

Map A-1: Pavement Ranks

Map A-2: Pavement Surface Types

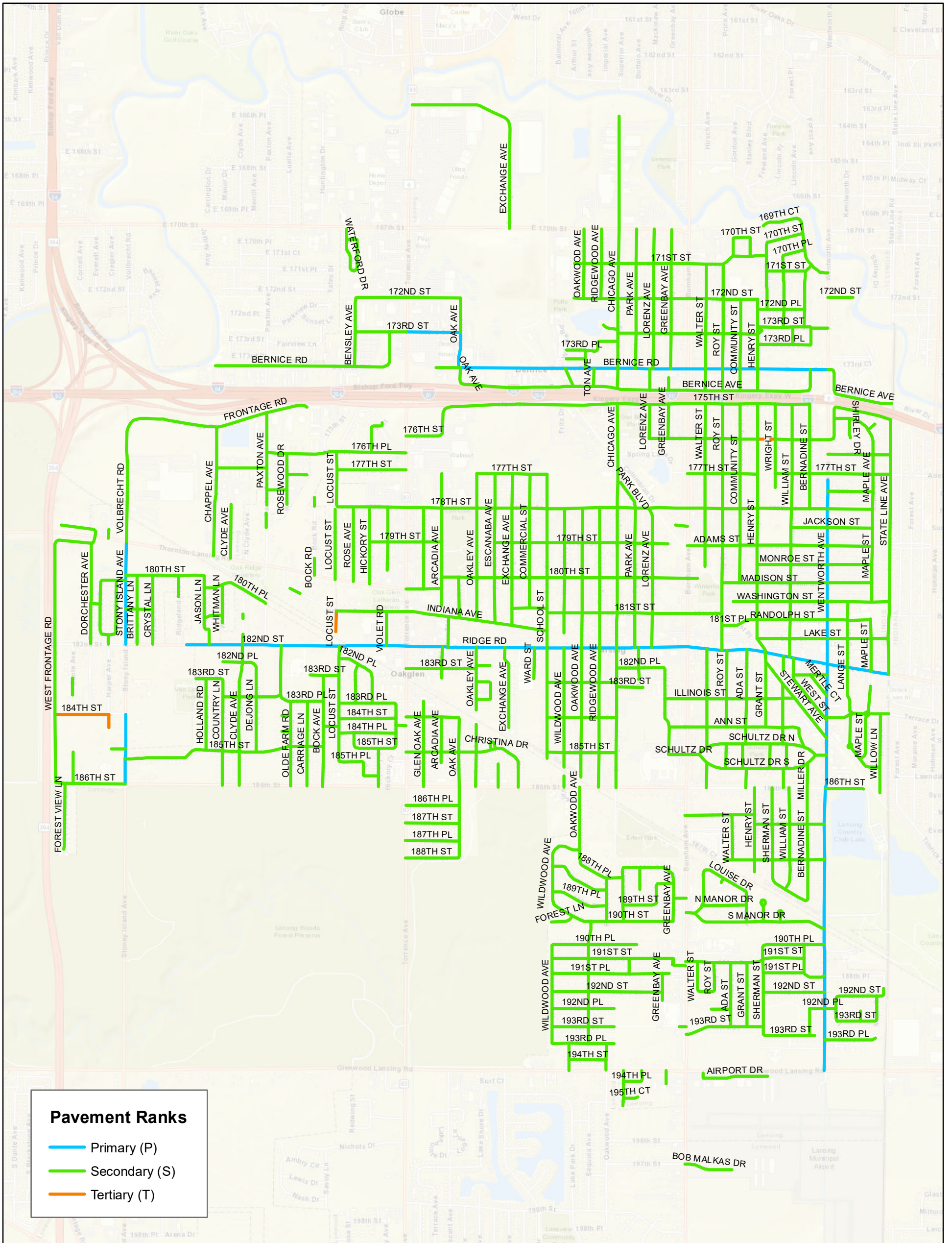
Map A-3: Pavement Condition Index (PCI) values

Map A-4: International Roughness Index (IRI) values

Map A-5: Five-year major M&R recommendations – *Recommendations assuming current funding*

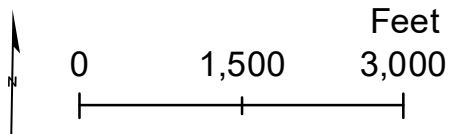
Map A-6: Five-year major M&R recommendations – *Recommendations assuming unlimited funding*

Map A-7: Pavement preservation candidates – *Current recommendations*



Pavement Ranks

- Primary (P)
- Secondary (S)
- Tertiary (T)



Map A-1:
Pavement Ranks

Lansing, Illinois

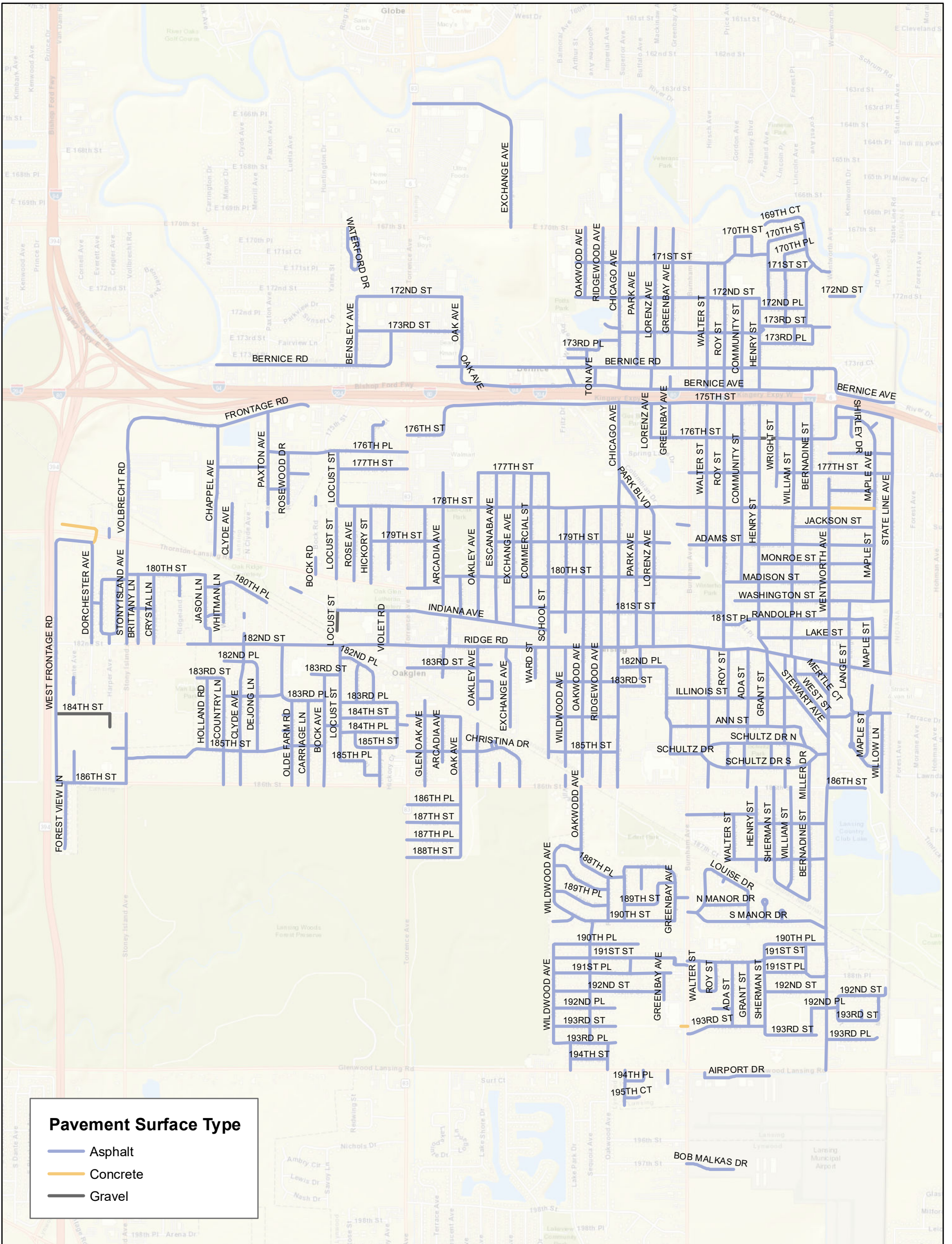
Pavement Management Program



Gorrondona & Associates, Inc.

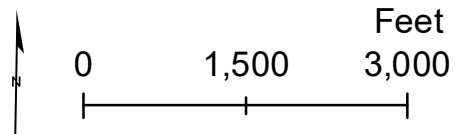


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Pavement Surface Type

- Asphalt
- Concrete
- Gravel

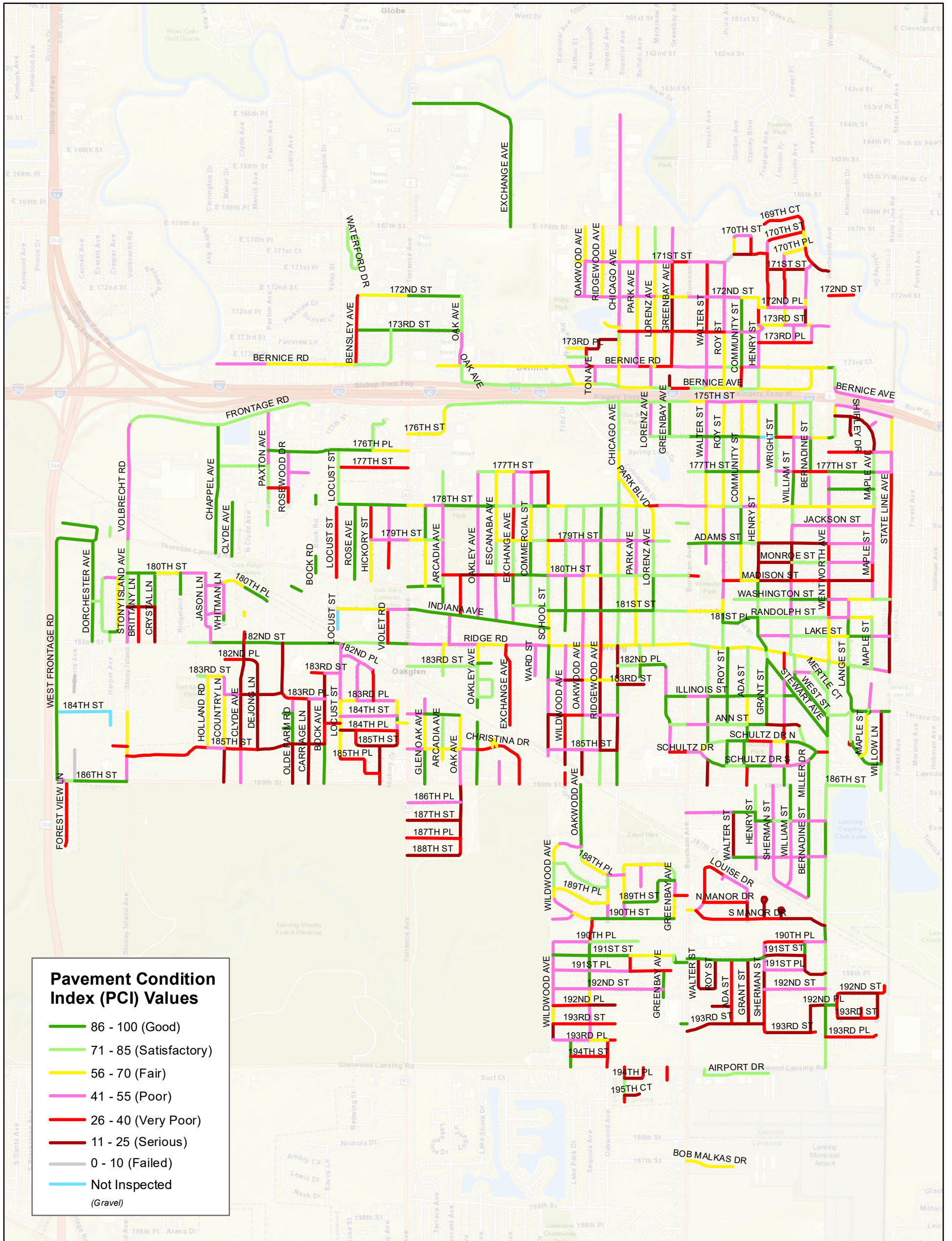


Map A-2:
Pavement Surface Types

Lansing, Illinois

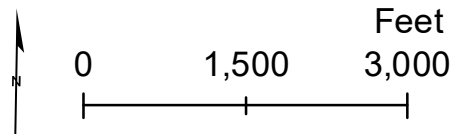
Pavement Management Program





Pavement Condition Index (PCI) Values

- 86 - 100 (Good)
- 71 - 85 (Satisfactory)
- 56 - 70 (Fair)
- 41 - 55 (Poor)
- 26 - 40 (Very Poor)
- 11 - 25 (Serious)
- 0 - 10 (Failed)
- Not Inspected (Gravel)



Map A-3:
Pavement Condition Index (PCI) Values

Lansing, Illinois

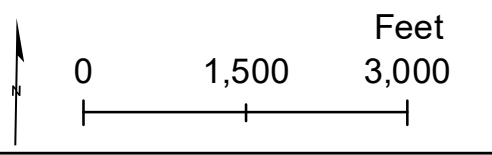
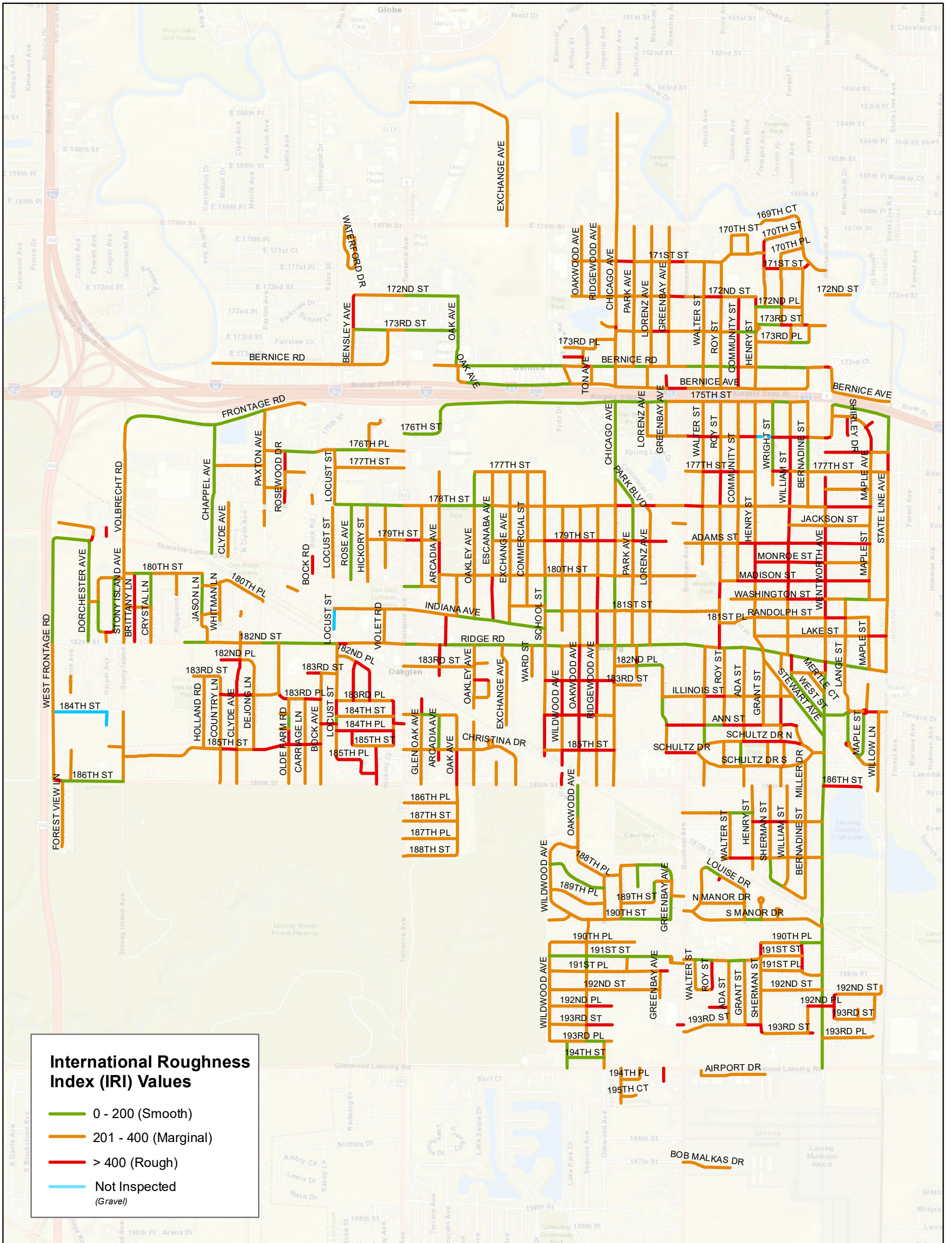
Pavement Management Program



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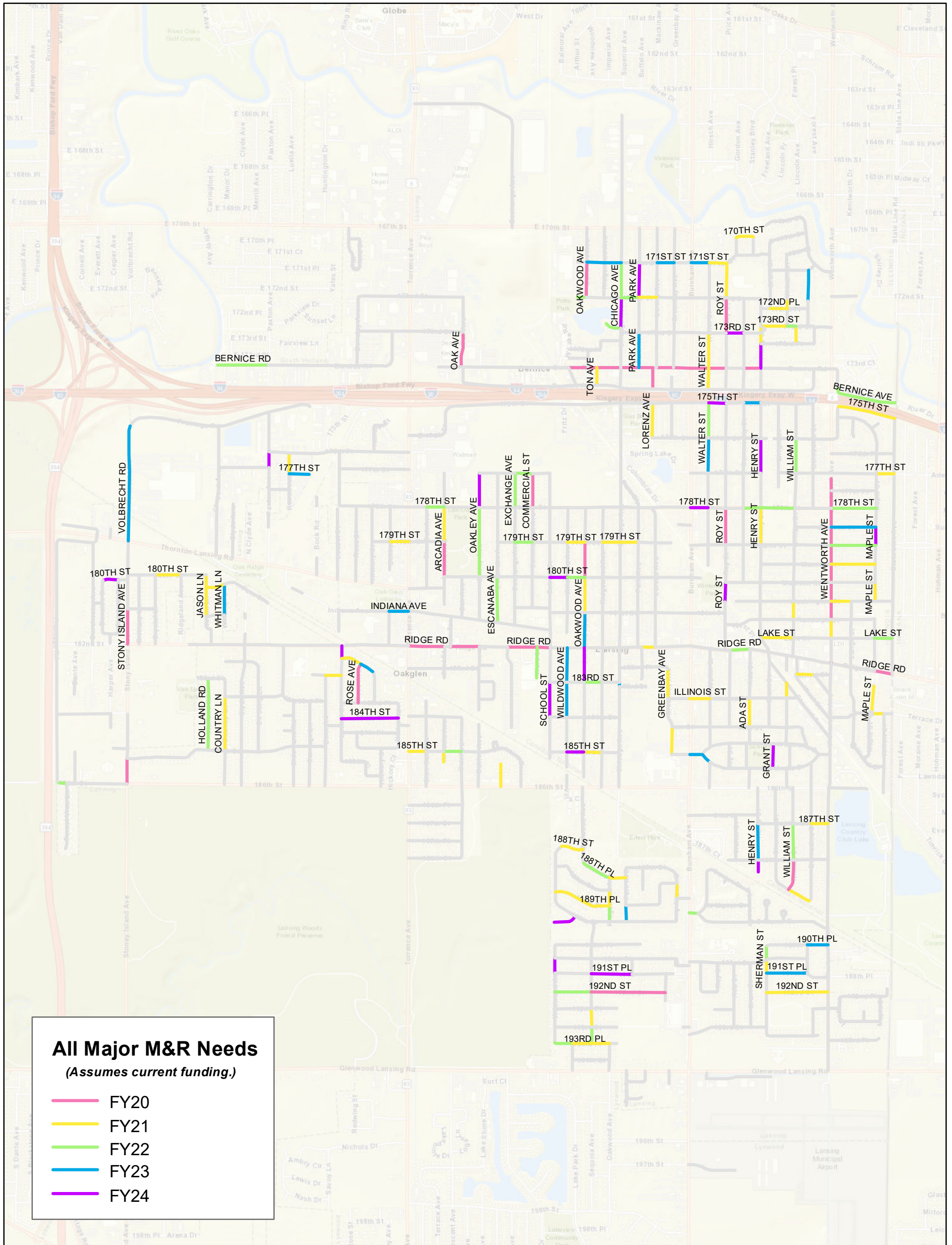


Map A-4:
International Roughness
Index (IRI) Values

Lansing, Illinois

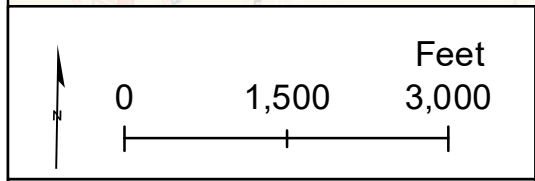
Pavement Management Program





All Major M&R Needs
(Assumes current funding.)

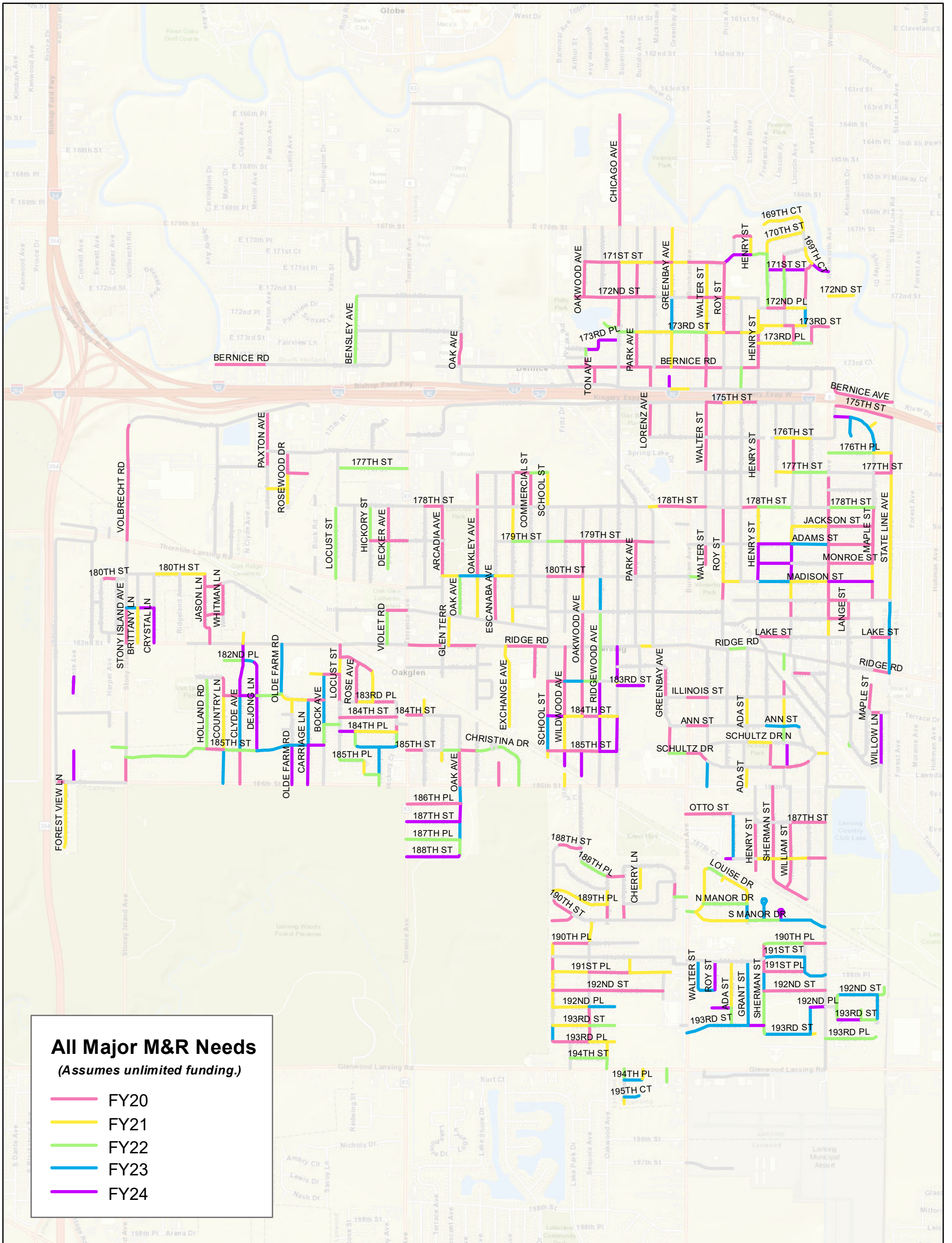
- FY20
- FY21
- FY22
- FY23
- FY24



Map A-5:
All Major M&R Needs
(Assumes current funding.)

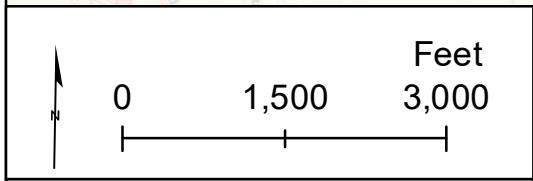
Lansing, Illinois
Pavement Management Program





All Major M&R Needs
(Assumes unlimited funding.)

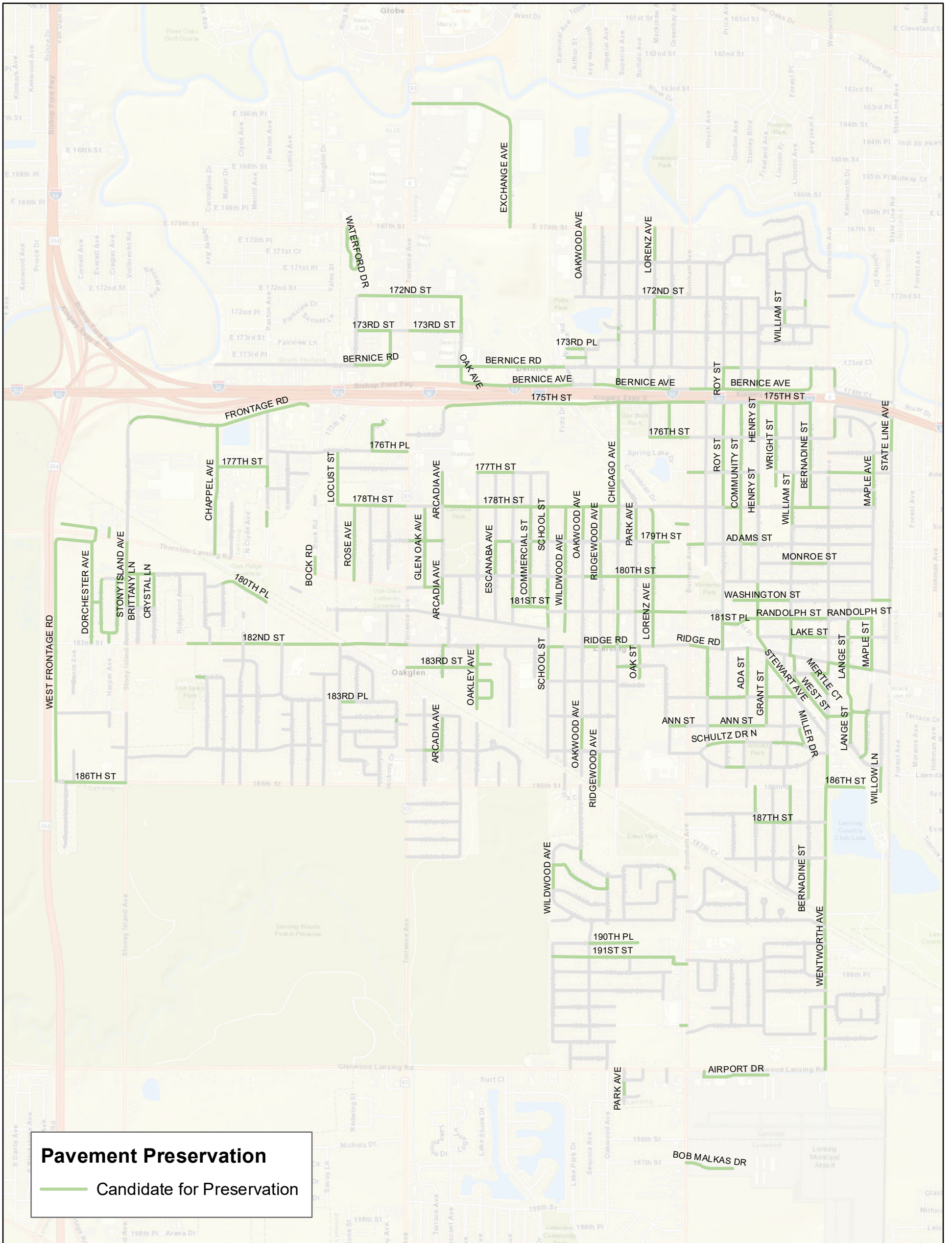
- FY20
- FY21
- FY22
- FY23
- FY24



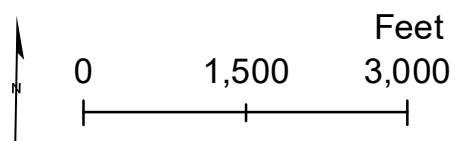
Map A-6:
All Major M&R Needs
(Assumes unlimited funding.)

Lansing, Illinois
 Pavement Management Program





Pavement Preservation
 — Candidate for Preservation



Map A-7:
 Pavement Preservation
 Candidates

Lansing, Illinois
 Pavement Management Program



**APPENDIX B – TABULATED FIVE-YEAR MAJOR M&R RECOMMENDATIONS AND
ESTIMATED COSTS – *ASSUMING CURRENT FUNDING***

Pavement ID	Road Name	From	To	Area	PCI	Year	Cost
LNSG::192ND ST::20	192ND STREET	GREENBAY AVENUE	OAKWOOD AVENUE	39,147	54	2020	\$47,305
LNSG::BRNC RD::100	BERNICE ROAD	TON AVENUE	CHICAGO AVENUE	12,646	44	2020	\$33,183
LNSG::BRNC RD::110	BERNICE ROAD	CHICAGO AVENUE	PARK AVENUE	9,526	45	2020	\$23,205
LNSG::BRNC RD::120	BERNICE ROAD	PARK AVENUE	LORENZ AVENUE	7,399	28	2020	\$38,869
LNSG::BRNC RD::140	BERNICE ROAD	LORENZ AVENUE	GREENBAY AVENUE	9,241	40	2020	\$32,702
LNSG::BRNC RD::150	BERNICE ROAD	GREENBAY AVENUE	BURNHAM AVENUE	9,013	40	2020	\$31,039
LNSG::BRNC RD::160	BERNICE ROAD	BURNHAM AVENUE	WALTER STREET	9,431	52	2020	\$13,053
LNSG::BRNC RD::170	BERNICE ROAD	WALTER STREET	ROY STREET	9,271	47	2020	\$18,952
LNSG::BRNC RD::190	BERNICE ROAD	COMMUNITY STREET	HENRY STREET	9,195	50	2020	\$13,492
LNSG::BRNC RD::80	BERNICE ROAD	RAILROAD AVENUE	OAKWOOD AVENUE	10,490	43	2020	\$31,278
LNSG::BRNC RD::90	BERNICE ROAD	OAKWOOD AVENUE	TON AVENUE	4,583	38	2020	\$17,697
LNSG::CMMRCL ST::40	COMMERCIAL STREET	178TH STREET	177TH STREET	18,111	54	2020	\$21,885
LNSG::KWD AVE::190	OAKWOOD AVENUE	180TH STREET	179TH STREET	18,509	54	2020	\$22,366
LNSG::KWD AVE::230	OAKWOOD AVENUE	172ND STREET	171ST STREET	18,587	54	2020	\$22,460
LNSG::LRNZ AVE::70	LORENZ AVENUE	BERNICE ROAD	BERNICE AVENUE	11,350	54	2020	\$13,716
LNSG::OK AVE::90	OAK AVENUE	BERNICE ROAD	OAK AVENUE	17,057	46	2020	\$38,243
LNSG::RCD AVE::50	ARCADIA AVENUE	180TH STREET	179TH STREET	18,435	54	2020	\$22,276
LNSG::RDG RD::10	RIDGE ROAD	TORRENCE AVENUE	RIDGE ROAD	10,528	50	2020	\$15,448
LNSG::RDG RD::20	RIDGE ROAD	RIDGE ROAD	FRED STREET	16,844	44	2020	\$44,198
LNSG::RDG RD::270	RIDGE ROAD	MAPLE STREET	STATE LINE AVENUE	13,331	49	2020	\$21,707
LNSG::RDG RD::40	RIDGE ROAD	GLEN TERRACE	OAKLEY AVENUE	21,675	54	2020	\$26,192
LNSG::RDG RD::60	RIDGE ROAD	EXCHANGE AVENUE	WARD STREET	22,260	52	2020	\$30,808
LNSG::RDG RD::70	RIDGE ROAD	WARD STREET	SCHOOL STREET	9,925	52	2020	\$13,736
LNSG::RS AVE::30	ROSE AVENUE	183RD PLACE	182ND PLACE	20,850	54	2020	\$25,195
LNSG::RY ST::100	ROY STREET	ADAMS STREET	178TH STREET	19,489	54	2020	\$23,551
LNSG::RY ST::160	ROY STREET	173RD STREET	172ND STREET	18,478	54	2020	\$22,328
LNSG::STN L AVE::10	STONY ISLAND AVENUE	186TH STREET	185TH STREET	13,365	43	2020	\$37,501
LNSG::STN L AVE::40	STONY ISLAND AVENUE	182ND STREET	181ST STREET	18,646	50	2020	\$27,361
LNSG::WLLM ST::10	WILLIAM STREET	188TH STREET	SHERMAN STREET	16,199	54	2020	\$19,575
LNSG::WWRTH AVE::170	WENTWORTH AVENUE	RIDGE ROAD	WENTWORTH AVENUE	1,941	52	2020	\$2,686
LNSG::WWRTH AVE::200	WENTWORTH AVENUE	RANDOLPH STREET	WASHINGTON STREET	14,048	34	2020	\$61,157
LNSG::WWRTH AVE::210	WENTWORTH AVENUE	WASHINGTON STREET	MADISON STREET	14,025	26	2020	\$77,892
LNSG::WWRTH AVE::220	WENTWORTH AVENUE	MADISON STREET	MONROE STREET	13,774	42	2020	\$43,357
LNSG::WWRTH AVE::230	WENTWORTH AVENUE	MONROE STREET	ADAMS STREET	14,392	40	2020	\$50,929
LNSG::WWRTH AVE::240	WENTWORTH AVENUE	ADAMS STREET	JACKSON STREET	13,663	42	2020	\$43,008
LNSG::WWRTH AVE::250	WENTWORTH AVENUE	JACKSON STREET	178TH STREET	14,187	49	2020	\$23,100
LNSG::WWRTH AVE::260	WENTWORTH AVENUE	178TH STREET	177TH PLACE	13,316	48	2020	\$24,478
LNSG::WWRTH AVE::270	WENTWORTH AVENUE	177TH PLACE	END	8,838	45	2020	\$21,529
LNSG::170TH ST::10	170TH STREET	COMMUNITY STREET	HENRY STREET	9,286	50	2021	\$14,733
LNSG::171ST ST::90	171ST STREET	WALTER STREET	ROY STREET	9,112	51	2021	\$13,328
LNSG::172ND PL::20	172ND PLACE	GRANT STREET	WILLIAM STREET	8,859	52	2021	\$12,012
LNSG::172ND ST::70	172ND STREET	PARK AVENUE	LORENZ AVENUE	9,274	50	2021	\$14,713
LNSG::173RD ST::130	173RD STREET	HENRY STREET	WILLIAM STREET	13,732	54	2021	\$17,017
LNSG::175TH ST::120	175TH STREET	WENTWORTH AVENUE	STATE LINE AVENUE	30,262	51	2021	\$44,263
LNSG::177TH ST::160	177TH STREET	MAPLE AVENUE	STATE LINE AVENUE	9,275	51	2021	\$13,567
LNSG::179TH ST::110	179TH STREET	RIDGEWOOD AVENUE	CHICAGO AVENUE	9,277	50	2021	\$14,718
LNSG::179TH ST::120	179TH STREET	CHICAGO AVENUE	PARK AVENUE	9,380	50	2021	\$14,882
LNSG::179TH ST::20	179TH STREET	DECKER AVENUE	TORRENCE AVENUE	9,916	50	2021	\$15,732
LNSG::179TH ST::90	179TH STREET	WILDWOOD AVENUE	OAKWOOD AVENUE	9,355	50	2021	\$14,842
LNSG::180TH PL::10	180TH PLACE	JASON LANE	WHITMAN LANE	9,973	51	2021	\$14,587
LNSG::180TH ST::50	180TH STREET	CRYSTAL LANE	RIDGELAND AVENUE	13,668	54	2021	\$16,938
LNSG::182ND PL::30	182ND PLACE	LOCUST STREET	ROSE AVENUE	10,872	51	2021	\$15,902
LNSG::183RD ST::40	183RD STREET	LOCUST STREET	BOCK AVENUE	8,334	51	2021	\$12,190
LNSG::185TH ST::100	185TH STREET	TORRENCE AVENUE	GLEN OAK AVENUE	9,314	50	2021	\$14,777
LNSG::185TH ST::160	185TH STREET	OAKWOOD AVENUE	RIDGEWOOD AVENUE	9,459	52	2021	\$12,826
LNSG::187TH ST::70	187TH STREET	BERNADINE STREET	WENTWORTH AVENUE	8,927	52	2021	\$12,105
LNSG::188TH PL::20	188TH PLACE	OAKWOOD AVENUE	RIDGEWOOD AVENUE	8,554	51	2021	\$12,511
LNSG::188TH ST::20	188TH STREET	WILDWOOD AVENUE	OAKWOOD AVENUE	11,342	51	2021	\$16,589
LNSG::189TH PL::10	189TH PLACE	RIDGEWOOD AVENUE	WILDWOOD AVENUE	30,394	54	2021	\$37,664
LNSG::192ND ST::40	192ND STREET	SHERMAN STREET	WENTWORTH AVENUE	32,658	50	2021	\$51,815

Pavement ID	Road Name	From	To	Area	PCI	Year	Cost
LNSG::193RD PL::20	193RD PLACE	OAKWOOD AVENUE	WILDWOOD AVENUE	10,340	52	2021	\$14,021
LNSG::193RD PL::30	193RD PLACE	OAKWOOD AVENUE	OAKWOOD AVENUE	9,412	54	2021	\$11,663
LNSG::AD ST::60	ADA STREET	ANN STREET	ILLINOIS STREET	15,180	54	2021	\$18,811
LNSG::CNTRL ST::10	CENTRAL STREET	WEST STREET	MERTLE COURT	8,973	52	2021	\$12,167
LNSG::CNTRY LN::20	COUNTRY LANE	185TH STREET	183RD PLACE	28,740	54	2021	\$35,614
LNSG::GRNBY AVE::100	GREENBAY AVENUE	ILLINOIS STREET	182ND PLACE	15,729	52	2021	\$21,328
LNSG::GRNBY AVE::60	GREENBAY AVENUE	189TH STREET	188TH PLACE	7,895	52	2021	\$10,706
LNSG::GRNBY AVE::80	GREENBAY AVENUE	SCHULTZ DRIVE	ANN STREET	14,576	51	2021	\$21,320
LNSG::HNRV ST::150	HENRY STREET	173RD PLACE	173RD STREET	5,417	51	2021	\$7,923
LNSG::HNRV ST::160	HENRY STREET	173RD STREET	173RD STREET	3,889	54	2021	\$4,820
LNSG::HNRV ST::90	HENRY STREET	ADAMS STREET	178TH STREET	19,642	54	2021	\$24,341
LNSG::JSN LN::20	JASON LANE	181ST STREET	180TH PLACE	15,492	50	2021	\$24,579
LNSG::JSN LN::30	JASON LANE	180TH PLACE	180TH STREET	6,406	51	2021	\$9,370
LNSG::KWD AVE::180	OAKWOOD AVENUE	181ST STREET	180TH STREET	18,656	54	2021	\$23,119
LNSG::KWD AVE::40	OAKWOOD AVENUE	193RD STREET	192ND PLACE	9,770	50	2021	\$15,501
LNSG::LK ST::10	LAKE STREET	HENRY STREET	WILLIAM STREET	17,512	50	2021	\$27,784
LNSG::LLNS ST::20	ILLINOIS STREET	BURNHAM AVENUE	WALTER STREET	10,577	50	2021	\$16,782
LNSG::LRNZ AVE::60	LORENZ AVENUE	176TH STREET	175TH STREET	18,070	50	2021	\$28,669
LNSG::MNR ST::30	MONROE STREET	WENTWORTH AVENUE	MAPLE STREET	23,542	51	2021	\$34,434
LNSG::MPL ST::100	MAPLE STREET	WASHINGTON STREET	MADISON STREET	10,038	50	2021	\$15,927
LNSG::MPL ST::70	MAPLE STREET	TERRACE DRIVE	END	13,823	52	2021	\$18,744
LNSG::RCD AVE::10	ARCADIA AVENUE	185TH STREET	END	5,636	54	2021	\$6,985
LNSG::RCD AVE::60	ARCADIA AVENUE	179TH STREET	178TH STREET	18,274	51	2021	\$26,729
LNSG::RDG RD::140	RIDGE ROAD	OAK STREET	LORENZ AVENUE	10,087	54	2021	\$12,500
LNSG::RDGWD AVE::20	RIDGEWOOD AVENUE	189TH PLACE	189TH STREET	8,180	54	2021	\$10,137
LNSG::RSWD DR::30	ROSEWOOD DRIVE	177TH STREET	176TH PLACE	10,987	51	2021	\$16,070
LNSG::RY ST::170	ROY STREET	172ND STREET	171ST STREET	18,416	51	2021	\$26,937
LNSG::SCNB AVE::10	ESCANABA AVENUE	186TH STREET	END	12,108	51	2021	\$17,710
LNSG::SHRMN ST::140	SHERMAN STREET	ILLINOIS STREET	STEWART AVENUE	7,957	51	2021	\$11,638
LNSG::SHRMN ST::40	SHERMAN STREET	191ST PLACE	191ST STREET	6,972	52	2021	\$9,454
LNSG::SHRMN ST::60	SHERMAN STREET	WILLIAM STREET	BERNADINE STREET	13,932	51	2021	\$20,378
LNSG::TN AVE::10	TON AVENUE	BERNICE ROAD	BERNICE AVENUE	9,461	51	2021	\$13,839
LNSG::TRRC DR::30	TERRACE DRIVE	MAPLE STREET	WILLOW LANE	6,188	52	2021	\$8,391
LNSG::WLLM ST::150	WILLIAM STREET	173RD PLACE	173RD STREET	9,295	52	2021	\$12,603
LNSG::WLLM ST::170	WILLIAM STREET	172ND PLACE	WILLIAM STREET	3,822	49	2021	\$7,001
LNSG::WLLM ST::60	WILLIAM STREET	RANDOLPH STREET	WASHINGTON STREET	9,755	50	2021	\$15,478
LNSG::WLTR ST::120	WALTER STREET	BERNICE ROAD	BERNICE AVENUE	11,568	50	2021	\$18,354
LNSG::WLTR ST::130	WALTER STREET	BERNICE ROAD	173RD STREET	18,565	51	2021	\$27,155
LNSG::WSHG TN ST::40	WASHINGTON STREET	WENTWORTH AVENUE	LANE STREET	10,686	51	2021	\$15,630
LNSG::WWRTH AVE::190	WENTWORTH AVENUE	LAKE STREET	RANDOLPH STREET	13,972	54	2021	\$17,314
LNSG::172ND ST::30	172ND STREET	OAKWOOD AVENUE	END	4,971	47	2022	\$11,340
LNSG::172ND ST::60	172ND STREET	CHICAGO AVENUE	PARK AVENUE	9,515	47	2022	\$21,705
LNSG::173RD ST::140	173RD STREET	WILLIAM STREET	WILLIAM STREET	4,870	47	2022	\$11,109
LNSG::177TH ST::60	177TH STREET	EXCHANGE AVENUE	COMMERCIAL STREET	9,315	47	2022	\$21,249
LNSG::178TH ST::200	178TH STREET	COMMUNITY STREET	HENRY STREET	9,107	47	2022	\$20,773
LNSG::178TH ST::210	178TH STREET	HENRY STREET	WILLIAM STREET	18,165	55	2022	\$22,216
LNSG::178TH ST::240	178TH STREET	WENTWORTH AVENUE	MAPLE STREET	23,893	55	2022	\$66,032
LNSG::178TH ST::30	178TH STREET	GLEN OAK AVENUE	ARCADIA AVENUE	9,440	47	2022	\$21,533
LNSG::179TH ST::60	179TH STREET	EXCHANGE AVENUE	COMMERCIAL STREET	9,293	55	2022	\$11,365
LNSG::180TH ST::140	180TH STREET	WILDWOOD AVENUE	OAKWOOD AVENUE	9,357	47	2022	\$21,345
LNSG::183RD ST::100	183RD STREET	OAKWOOD AVENUE	RIDGEWOOD AVENUE	9,436	55	2022	\$11,541
LNSG::185TH ST::120	185TH STREET	ARCADIA AVENUE	OAK AVENUE	9,387	55	2022	\$11,481
LNSG::186TH ST::10	186TH STREET	FOREST VIEW LANE	WEST FRONTAGE ROAD	4,947	48	2022	\$10,016
LNSG::188TH PL::10	188TH PLACE	RIDGEWOOD AVENUE	OAKWOOD AVENUE	16,001	55	2022	\$19,570
LNSG::192ND ST::10	192ND STREET	OAKWOOD AVENUE	WILDWOOD AVENUE	19,437	46	2022	\$48,832
LNSG::193RD PL::10	193RD PLACE	WILDWOOD AVENUE	WILDWOOD AVENUE	9,191	48	2022	\$18,606
LNSG::BRNC AVE::140	BERNICE AVENUE	174TH COURT	END	32,758	46	2022	\$82,298
LNSG::BRNC RD::10	BERNICE ROAD	PAXTON AVENUE	END	27,046	46	2022	\$67,947
LNSG::CHCG AVE::200	CHICAGO AVENUE	172ND STREET	171ST STREET	18,639	47	2022	\$42,518
LNSG::DMS ST::70	ADAMS STREET	WENTWORTH AVENUE	MAPLE STREET	23,661	47	2022	\$53,972

Pavement ID	Road Name	From	To	Area	PCI	Year	Cost
LNSG::FLNGN DR::10	FLANIGAN DRIVE	CHICAGO AVENUE	END	8,319	55	2022	\$10,175
LNSG::HLLND RD::10	HOLLAND ROAD	185TH STREET	183RD STREET	37,947	55	2022	\$46,411
LNSG::KLY AVE::90	OAKLEY AVENUE	180TH STREET	178TH STREET	36,786	46	2022	\$92,418
LNSG::KWD AVE::30	OAKWOOD AVENUE	193RD PLACE	193RD STREET	8,805	48	2022	\$17,826
LNSG::LK ST::50	LAKE STREET	MAPLE STREET	STATE LINE AVENUE	9,214	48	2022	\$18,654
LNSG::RDG RD::190	RIDGE ROAD	ROY STREET	ADA STREET	15,020	55	2022	\$18,370
LNSG::RDGWD AVE::10	RIDGEWOOD AVENUE	190TH STREET	189TH PLACE	8,541	48	2022	\$17,291
LNSG::S MNR DR::10	S MANOR DRIVE	BURNHAM AVENUE	W LOUISE DRIVE	4,731	55	2022	\$5,786
LNSG::SCNB AVE::20	ESCANABA AVENUE	INDIANA AVENUE	180TH STREET	24,012	47	2022	\$54,774
LNSG::SHRMN ST::50	SHERMAN STREET	191ST STREET	190TH PLACE	7,832	48	2022	\$15,856
LNSG::WLLM ST::130	WILLIAM STREET	177TH STREET	176TH STREET	18,065	47	2022	\$41,208
LNSG::WLLM ST::20	WILLIAM STREET	188TH STREET	187TH STREET	19,026	47	2022	\$43,400
LNSG::WLTR ST::110	WALTER STREET	176TH STREET	175TH STREET	18,586	47	2022	\$42,396
LNSG::WRD ST::10	WARD STREET	RIDGE ROAD	END	16,294	47	2022	\$37,169
LNSG::XCHNG AVE::60	EXCHANGE AVENUE	178TH STREET	177TH STREET	18,246	47	2022	\$41,620
LNSG::171ST ST::20	171ST STREET	OAKWOOD AVENUE	RIDGEWOOD AVENUE	9,285	44	2023	\$27,536
LNSG::171ST ST::30	171ST STREET	RIDGEWOOD AVENUE	CHICAGO AVENUE	9,132	44	2023	\$27,082
LNSG::171ST ST::60	171ST STREET	LORENZ AVENUE	GREENBAY AVENUE	9,287	44	2023	\$27,541
LNSG::171ST ST::80	171ST STREET	BURNHAM AVENUE	WALTER STREET	9,769	44	2023	\$28,972
LNSG::175TH ST::80	175TH STREET	COMMUNITY STREET	HENRY STREET	9,131	44	2023	\$27,078
LNSG::177TH ST::20	177TH STREET	ROSEWOOD DRIVE	END	10,487	44	2023	\$31,102
LNSG::182ND PL::40	182ND PLACE	ROSE AVENUE	HICKORY STREET	7,432	44	2023	\$22,039
LNSG::190TH PL::40	190TH PLACE	BERNADINE STREET	WENTWORTH AVENUE	10,811	44	2023	\$32,060
LNSG::191ST PL::50	191ST PLACE	SHERMAN STREET	BERNADINE STREET	21,847	43	2023	\$68,990
LNSG::BRNDN ST::140	BERNADINE STREET	172ND PLACE	171ST STREET	16,335	44	2023	\$48,444
LNSG::CHCG AVE::50	CHICAGO AVENUE	183RD STREET	183RD STREET	2,281	40	2023	\$8,837
LNSG::HNRV ST::10	HENRY STREET	188TH STREET	187TH STREET	19,198	44	2023	\$56,934
LNSG::INDN AVE::30	INDIANA AVENUE	VIOLET ROAD	TORRENCE AVENUE	10,356	44	2023	\$30,712
LNSG::JCKSN ST::20	JACKSON STREET	WENTWORTH AVENUE	MAPLE STREET	23,774	44	2023	\$70,505
LNSG::KWD AVE::170	OAKWOOD AVENUE	RIDGE ROAD	181ST STREET	18,590	43	2023	\$58,704
LNSG::PRK AVE::120	PARK AVENUE	BERNICE ROAD	173RD STREET	18,747	43	2023	\$59,200
LNSG::PRK AVE::50	PARK AVENUE	190TH STREET	189TH STREET	8,443	44	2023	\$25,039
LNSG::SCHLZ DR N::20	SCHULTZ DRIVE	BURNHAM AVENUE	SCHULTZ DRIVE N	6,771	44	2023	\$20,081
LNSG::SCHLZ DR S::10	SCHULTZ DRIVE S	SCHULTZ DRIVE	WALTER STREET	5,661	44	2023	\$16,790
LNSG::VLBRCHT RD::10	VOLBRECHT ROAD	THORNTON LANSING ROAD	FRONTAGE ROAD	60,818	43	2023	\$192,055
LNSG::WHTMN LN::20	WHITMAN LANE	181ST STREET	180TH PLACE	16,415	44	2023	\$48,679
LNSG::WLDWD AVE::150	WILDWOOD AVENUE	184TH STREET	183RD STREET	18,412	43	2023	\$58,143
LNSG::WLDWD AVE::160	WILDWOOD AVENUE	183RD STREET	RIDGE ROAD	18,396	43	2023	\$58,092
LNSG::WLTR ST::100	WALTER STREET	177TH STREET	176TH STREET	18,585	44	2023	\$55,116
LNSG::173RD ST::100	173RD STREET	ROY STREET	COMMUNITY STREET	9,275	42	2024	\$32,986
LNSG::175TH ST::60	175TH STREET	WALTER STREET	ROY STREET	9,201	42	2024	\$32,721
LNSG::178TH ST::170	178TH STREET	BURNHAM AVENUE	WALTER STREET	9,218	42	2024	\$32,781
LNSG::180TH ST::10	180TH STREET	DONATUS DRIVE	CHARLOTTE DRIVE	8,042	42	2024	\$28,601
LNSG::180TH ST::130	180TH STREET	SCHOOL STREET	WILDWOOD AVENUE	9,237	42	2024	\$32,850
LNSG::184TH ST::20	184TH STREET	LOCUST STREET	VIOLET ROAD	30,238	41	2024	\$111,619
LNSG::185TH ST::150	185TH STREET	WILDWOOD AVENUE	OAKWOOD AVENUE	8,972	42	2024	\$31,909
LNSG::191ST PL::20	191ST PLACE	PARK AVENUE	OAKWOOD AVENUE	21,381	41	2024	\$78,924
LNSG::CHCG AVE::190	CHICAGO AVENUE	FLANIGAN DRIVE	172ND STREET	16,136	41	2024	\$61,305
LNSG::FRST LN::10	FOREST LANE	190TH STREET	END	12,461	42	2024	\$44,317
LNSG::GRNT ST::20	GRANT STREET	SCHULTZ DRIVE S	SCHULTZ DRIVE N	12,985	42	2024	\$46,180
LNSG::HNRV CT::10	HENRY COURT	188TH STREET	END	6,104	41	2024	\$23,192
LNSG::HNRV ST::110	HENRY STREET	177TH STREET	176TH STREET	18,523	41	2024	\$70,376
LNSG::HNRV ST::140	HENRY STREET	BERNICE ROAD	173RD PLACE	13,169	41	2024	\$50,034
LNSG::KLY AVE::100	OAKLEY AVENUE	178TH STREET	177TH STREET	18,113	41	2024	\$68,817
LNSG::KWD AVE::160	OAKWOOD AVENUE	183RD STREET	RIDGE ROAD	18,419	41	2024	\$67,993
LNSG::LCST ST::70	LOCUST STREET	182ND PLACE	182ND STREET	6,554	42	2024	\$23,310
LNSG::MPL ST::130	MAPLE STREET	ADAMS STREET	JACKSON STREET	9,637	42	2024	\$34,272
LNSG::PRK AVE::140	PARK AVENUE	172ND STREET	171ST STREET	18,465	41	2024	\$68,160
LNSG::PXTN AVE::40	PAXTON AVENUE	177TH STREET	176TH PLACE	8,092	42	2024	\$28,779
LNSG::RY ST::80	ROY STREET	ROY STREET	MADISON STREET	10,186	42	2024	\$36,225

Pavement ID	Road Name	From	To	Area	PCI	Year	Cost
LNSG::SCHL ST::30	SCHOOL STREET	184TH STREET	183RD STREET	18,371	42	2024	\$65,333
LNSG::WLDWD AVE::70	WILDWOOD AVENUE	191ST PLACE	191ST STREET	7,996	42	2024	\$28,437

**APPENDIX C – TABULATED FIVE-YEAR MAJOR M&R RECOMMENDATIONS AND
ESTIMATED COSTS – ASSUMING UNLIMITED FUNDING**

Pavement ID	Road Name	From	To	Area	PCI	Year	Cost
LNSG::170TH ST::10	170TH STREET	COMMUNITY STREET	HENRY STREET	9,286	52	2020	\$12,852
LNSG::171ST ST::150	171ST STREET	BERNADINE STREET	169TH COURT	4,729	44	2020	\$12,410
LNSG::171ST ST::20	171ST STREET	OAKWOOD AVENUE	RIDGEWOOD AVENUE	9,285	49	2020	\$15,119
LNSG::171ST ST::30	171ST STREET	RIDGEWOOD AVENUE	CHICAGO AVENUE	9,132	49	2020	\$14,870
LNSG::171ST ST::60	171ST STREET	LORENZ AVENUE	GREENBAY AVENUE	9,287	49	2020	\$15,122
LNSG::171ST ST::80	171ST STREET	BURNHAM AVENUE	WALTER STREET	9,769	49	2020	\$15,908
LNSG::171ST ST::90	171ST STREET	WALTER STREET	ROY STREET	9,112	53	2020	\$11,825
LNSG::172ND PL::20	172ND PLACE	GRANT STREET	WILLIAM STREET	8,859	54	2020	\$10,705
LNSG::172ND ST::100	172ND STREET	BURNHAM AVENUE	WALTER STREET	9,612	44	2020	\$25,221
LNSG::172ND ST::110	172ND STREET	WALTER STREET	ROY STREET	9,164	46	2020	\$20,548
LNSG::172ND ST::30	172ND STREET	OAKWOOD AVENUE	END	4,971	50	2020	\$7,294
LNSG::172ND ST::40	172ND STREET	OAKWOOD AVENUE	RIDGEWOOD AVENUE	9,246	44	2020	\$24,261
LNSG::172ND ST::50	172ND STREET	RIDGEWOOD AVENUE	CHICAGO AVENUE	9,077	45	2020	\$22,113
LNSG::172ND ST::60	172ND STREET	CHICAGO AVENUE	PARK AVENUE	9,515	50	2020	\$13,962
LNSG::172ND ST::70	172ND STREET	PARK AVENUE	LORENZ AVENUE	9,274	52	2020	\$12,835
LNSG::173RD ST::100	173RD STREET	ROY STREET	COMMUNITY STREET	9,275	48	2020	\$17,050
LNSG::173RD ST::140	173RD STREET	WILLIAM STREET	WILLIAM STREET	4,870	50	2020	\$7,146
LNSG::173RD ST::170	173RD STREET	BERNADINE STREET	WENTWORTH AVENUE	8,842	45	2020	\$21,539
LNSG::175TH ST::120	175TH STREET	WENTWORTH AVENUE	STATE LINE AVENUE	30,262	53	2020	\$39,271
LNSG::175TH ST::60	175TH STREET	WALTER STREET	ROY STREET	9,201	48	2020	\$16,913
LNSG::175TH ST::80	175TH STREET	COMMUNITY STREET	HENRY STREET	9,131	49	2020	\$14,868
LNSG::177TH ST::160	177TH STREET	MAPLE AVENUE	STATE LINE AVENUE	9,275	53	2020	\$12,037
LNSG::177TH ST::20	177TH STREET	ROSEWOOD DRIVE	END	10,487	49	2020	\$17,077
LNSG::177TH ST::60	177TH STREET	EXCHANGE AVENUE	COMMERCIAL STREET	9,315	50	2020	\$13,669
LNSG::178TH ST::160	178TH STREET	LORENZ AVENUE	BURNHAM AVENUE	15,262	44	2020	\$40,046
LNSG::178TH ST::170	178TH STREET	BURNHAM AVENUE	WALTER STREET	9,218	48	2020	\$16,944
LNSG::178TH ST::200	178TH STREET	COMMUNITY STREET	HENRY STREET	9,107	50	2020	\$13,363
LNSG::178TH ST::30	178TH STREET	GLEN OAK AVENUE	ARCADIA AVENUE	9,440	50	2020	\$13,851
LNSG::179TH ST::10	179TH STREET	HICKORY STREET	DECKER AVENUE	9,518	47	2020	\$19,457
LNSG::179TH ST::100	179TH STREET	OAKWOOD AVENUE	RIDGEWOOD AVENUE	9,291	43	2020	\$26,069
LNSG::179TH ST::110	179TH STREET	RIDGEWOOD AVENUE	CHICAGO AVENUE	9,277	52	2020	\$12,839
LNSG::179TH ST::120	179TH STREET	CHICAGO AVENUE	PARK AVENUE	9,380	52	2020	\$12,982
LNSG::179TH ST::130	179TH STREET	PARK AVENUE	LORENZ AVENUE	9,144	44	2020	\$23,995
LNSG::179TH ST::20	179TH STREET	DECKER AVENUE	TORRENCE AVENUE	9,916	52	2020	\$13,724
LNSG::179TH ST::90	179TH STREET	WILDWOOD AVENUE	OAKWOOD AVENUE	9,355	52	2020	\$12,947
LNSG::180TH PL::10	180TH PLACE	JASON LANE	WHITMAN LANE	9,973	53	2020	\$12,942
LNSG::180TH ST::10	180TH STREET	DONATUS DRIVE	CHARLOTTE DRIVE	8,042	48	2020	\$14,783
LNSG::180TH ST::130	180TH STREET	SCHOOL STREET	WILDWOOD AVENUE	9,237	48	2020	\$16,980
LNSG::180TH ST::140	180TH STREET	WILDWOOD AVENUE	OAKWOOD AVENUE	9,357	50	2020	\$13,730
LNSG::181ST ST::40	181ST STREET	JASON LANE	WHITMAN LANE	9,753	45	2020	\$23,760
LNSG::182ND PL::30	182ND PLACE	LOCUST STREET	ROSE AVENUE	10,872	53	2020	\$14,108
LNSG::182ND PL::40	182ND PLACE	ROSE AVENUE	HICKORY STREET	7,432	49	2020	\$12,101
LNSG::183RD PL::10	183RD PLACE	COUNTRY LANE	CLYDE AVENUE	8,655	45	2020	\$21,085
LNSG::183RD PL::50	183RD PLACE	CARRIAGE LANE	BOCK AVENUE	8,456	47	2020	\$17,286
LNSG::183RD PL::80	183RD PLACE	HICKORY STREET	VIOLET ROAD	12,004	45	2020	\$29,243
LNSG::183RD ST::40	183RD STREET	LOCUST STREET	BOCK AVENUE	8,334	53	2020	\$10,815
LNSG::183RD ST::80	183RD STREET	SCHOOL STREET	WILDWOOD AVENUE	9,019	47	2020	\$18,436
LNSG::184TH ST::20	184TH STREET	LOCUST STREET	VIOLET ROAD	30,238	47	2020	\$61,812
LNSG::184TH ST::30	184TH STREET	TORRENCE AVENUE	GLEN OAK AVENUE	9,429	44	2020	\$24,741
LNSG::184TH ST::80	184TH STREET	OAKWOOD AVENUE	RIDGEWOOD AVENUE	9,448	44	2020	\$24,790
LNSG::185TH ST::100	185TH STREET	TORRENCE AVENUE	GLEN OAK AVENUE	9,314	52	2020	\$12,890
LNSG::185TH ST::130	185TH STREET	OAK AVENUE	OAKLEY AVENUE	9,090	45	2020	\$22,143
LNSG::185TH ST::140	185TH STREET	SCHOOL STREET	WILDWOOD AVENUE	9,124	46	2020	\$20,457
LNSG::185TH ST::150	185TH STREET	WILDWOOD AVENUE	OAKWOOD AVENUE	8,972	48	2020	\$16,494
LNSG::185TH ST::160	185TH STREET	OAKWOOD AVENUE	RIDGEWOOD AVENUE	9,459	54	2020	\$11,430
LNSG::185TH ST::20	185TH STREET	HOLLAND ROAD	COUNTRY LANE	8,850	47	2020	\$18,090
LNSG::186TH PL::10	186TH PLACE	TORRENCE AVENUE	OAK AVENUE	28,257	44	2020	\$74,146
LNSG::186TH ST::10	186TH STREET	FOREST VIEW LANE	WEST FRONTAGE ROAD	4,947	52	2020	\$6,847
LNSG::187TH ST::60	187TH STREET	WILLIAM STREET	BERNADINE STREET	9,510	47	2020	\$19,439
LNSG::187TH ST::70	187TH STREET	BERNADINE STREET	WENTWORTH AVENUE	8,927	54	2020	\$10,787
LNSG::188TH PL::20	188TH PLACE	OAKWOOD AVENUE	RIDGEWOOD AVENUE	8,554	53	2020	\$11,100

Pavement ID	Road Name	From	To	Area	PCI	Year	Cost
LNSG::188TH ST::100	188TH STREET	BERNADINE STREET	WENTWORTH AVENUE	8,956	44	2020	\$23,501
LNSG::188TH ST::20	188TH STREET	WILDWOOD AVENUE	OAKWOOD AVENUE	11,342	53	2020	\$14,718
LNSG::190TH PL::10	190TH PLACE	OAKWOOD AVENUE	WILDWOOD AVENUE	18,734	43	2020	\$55,862
LNSG::190TH PL::40	190TH PLACE	BERNADINE STREET	WENTWORTH AVENUE	10,811	49	2020	\$17,603
LNSG::190TH ST::10	190TH STREET	FOREST LANE	WILDWOOD AVENUE	12,851	46	2020	\$28,812
LNSG::191ST PL::20	191ST PLACE	PARK AVENUE	OAKWOOD AVENUE	21,381	47	2020	\$43,706
LNSG::191ST PL::50	191ST PLACE	SHERMAN STREET	BERNADINE STREET	21,847	48	2020	\$40,161
LNSG::192ND ST::10	192ND STREET	OAKWOOD AVENUE	WILDWOOD AVENUE	19,437	49	2020	\$31,651
LNSG::192ND ST::20	192ND STREET	GREENBAY AVENUE	OAKWOOD AVENUE	39,147	54	2020	\$47,305
LNSG::192ND ST::40	192ND STREET	SHERMAN STREET	WENTWORTH AVENUE	32,658	52	2020	\$45,200
LNSG::193RD PL::10	193RD PLACE	WILDWOOD AVENUE	WILDWOOD AVENUE	9,191	52	2020	\$12,720
LNSG::193RD PL::20	193RD PLACE	OAKWOOD AVENUE	WILDWOOD AVENUE	10,340	54	2020	\$12,495
LNSG::ANN ST::20	ANN STREET	BURNHAM AVENUE	WALTER STREET	10,518	47	2020	\$21,500
LNSG::BRNC AVE::140	BERNICE AVENUE	174TH COURT	END	32,758	49	2020	\$53,342
LNSG::BRNC RD::10	BERNICE ROAD	PAXTON AVENUE	END	27,046	49	2020	\$44,040
LNSG::BRNC RD::100	BERNICE ROAD	TON AVENUE	CHICAGO AVENUE	12,646	44	2020	\$33,183
LNSG::BRNC RD::110	BERNICE ROAD	CHICAGO AVENUE	PARK AVENUE	9,526	45	2020	\$23,205
LNSG::BRNC RD::120	BERNICE ROAD	PARK AVENUE	LORENZ AVENUE	7,399	28	2020	\$38,869
LNSG::BRNC RD::140	BERNICE ROAD	LORENZ AVENUE	GREENBAY AVENUE	9,241	40	2020	\$32,702
LNSG::BRNC RD::150	BERNICE ROAD	GREENBAY AVENUE	BURNHAM AVENUE	9,013	40	2020	\$31,039
LNSG::BRNC RD::160	BERNICE ROAD	BURNHAM AVENUE	WALTER STREET	9,431	52	2020	\$13,053
LNSG::BRNC RD::170	BERNICE ROAD	WALTER STREET	ROY STREET	9,271	47	2020	\$18,952
LNSG::BRNC RD::190	BERNICE ROAD	COMMUNITY STREET	HENRY STREET	9,195	50	2020	\$13,492
LNSG::BRNC RD::80	BERNICE ROAD	RAILROAD AVENUE	OAKWOOD AVENUE	10,490	43	2020	\$31,278
LNSG::BRNC RD::90	BERNICE ROAD	OAKWOOD AVENUE	TON AVENUE	4,583	38	2020	\$17,697
LNSG::BRNDN ST::110	BERNADINE STREET	173RD PLACE	173RD STREET	9,121	44	2020	\$23,933
LNSG::BRNDN ST::140	BERNADINE STREET	172ND PLACE	171ST STREET	16,335	49	2020	\$26,600
LNSG::CHCG AVE::190	CHICAGO AVENUE	FLANIGAN DRIVE	172ND STREET	16,136	46	2020	\$36,178
LNSG::CHCG AVE::200	CHICAGO AVENUE	172ND STREET	171ST STREET	18,639	50	2020	\$27,350
LNSG::CHCG AVE::220	CHICAGO AVENUE	170TH STREET	END	59,777	43	2020	\$178,244
LNSG::CLYD AVE::10	CLYDE AVENUE	186TH STREET	185TH STREET	17,868	44	2020	\$46,884
LNSG::CMMRCL ST::40	COMMERCIAL STREET	178TH STREET	177TH STREET	18,111	54	2020	\$21,885
LNSG::CNTRL ST::10	CENTRAL STREET	WEST STREET	MERTLE COURT	8,973	54	2020	\$10,843
LNSG::CNTRY LN::30	COUNTRY LANE	183RD PLACE	183RD STREET	9,364	47	2020	\$19,142
LNSG::DCKR AVE::20	DECKER AVENUE	179TH STREET	END	17,794	45	2020	\$43,347
LNSG::DMS ST::70	ADAMS STREET	WENTWORTH AVENUE	MAPLE STREET	23,661	50	2020	\$34,719
LNSG::FRST LN::10	FOREST LANE	190TH STREET	END	12,461	48	2020	\$22,907
LNSG::GRNBY AVE::100	GREENBAY AVENUE	ILLINOIS STREET	182ND PLACE	15,729	54	2020	\$19,007
LNSG::GRNBY AVE::60	GREENBAY AVENUE	189TH STREET	188TH PLACE	7,895	54	2020	\$9,540
LNSG::GRNBY AVE::80	GREENBAY AVENUE	SCHULTZ DRIVE	ANN STREET	14,576	53	2020	\$18,915
LNSG::GRNT ST::20	GRANT STREET	SCHULTZ DRIVE S	SCHULTZ DRIVE N	12,985	48	2020	\$23,870
LNSG::HNRV CT::10	HENRY COURT	188TH STREET	END	6,104	46	2020	\$13,686
LNSG::HNRV ST::10	HENRY STREET	188TH STREET	187TH STREET	19,198	49	2020	\$31,261
LNSG::HNRV ST::110	HENRY STREET	177TH STREET	176TH STREET	18,523	46	2020	\$41,531
LNSG::HNRV ST::140	HENRY STREET	BERNICE ROAD	173RD PLACE	13,169	46	2020	\$29,526
LNSG::HNRV ST::150	HENRY STREET	173RD PLACE	173RD STREET	5,417	53	2020	\$7,030
LNSG::INDN AVE::30	INDIANA AVENUE	VIOLET ROAD	TORRENCE AVENUE	10,356	49	2020	\$16,863
LNSG::JCKSN ST::20	JACKSON STREET	WENTWORTH AVENUE	MAPLE STREET	23,774	49	2020	\$38,713
LNSG::JSN LN::10	JASON LANE	182ND STREET	181ST STREET	16,544	45	2020	\$40,303
LNSG::JSN LN::20	JASON LANE	181ST STREET	180TH PLACE	15,492	52	2020	\$21,441
LNSG::JSN LN::30	JASON LANE	180TH PLACE	180TH STREET	6,406	53	2020	\$8,313
LNSG::KLY AVE::100	OAKLEY AVENUE	178TH STREET	177TH STREET	18,113	46	2020	\$40,611
LNSG::KLY AVE::90	OAKLEY AVENUE	180TH STREET	178TH STREET	36,786	49	2020	\$59,901
LNSG::KWD AVE::150	OAKWOOD AVENUE	184TH STREET	183RD STREET	18,454	44	2020	\$48,423
LNSG::KWD AVE::160	OAKWOOD AVENUE	183RD STREET	RIDGE ROAD	18,419	47	2020	\$37,653
LNSG::KWD AVE::170	OAKWOOD AVENUE	RIDGE ROAD	181ST STREET	18,590	48	2020	\$34,173
LNSG::KWD AVE::190	OAKWOOD AVENUE	180TH STREET	179TH STREET	18,509	54	2020	\$22,366
LNSG::KWD AVE::210	OAKWOOD AVENUE	BERNICE ROAD	WEST LAKE ROAD	4,950	45	2020	\$12,059
LNSG::KWD AVE::230	OAKWOOD AVENUE	172ND STREET	171ST STREET	18,587	54	2020	\$22,460
LNSG::KWD AVE::30	OAKWOOD AVENUE	193RD PLACE	193RD STREET	8,805	52	2020	\$12,187
LNSG::KWD AVE::40	OAKWOOD AVENUE	193RD STREET	192ND PLACE	9,770	52	2020	\$13,522

Pavement ID	Road Name	From	To	Area	PCI	Year	Cost
LNSG::LCST ST::50	LOCUST STREET	183RD PLACE	183RD STREET	14,442	43	2020	\$43,063
LNSG::LCST ST::70	LOCUST STREET	182ND PLACE	182ND STREET	6,554	48	2020	\$12,049
LNSG::LK ST::10	LAKE STREET	HENRY STREET	WILLIAM STREET	17,512	52	2020	\$24,237
LNSG::LK ST::50	LAKE STREET	MAPLE STREET	STATE LINE AVENUE	9,214	52	2020	\$12,753
LNSG::LLNS ST::20	ILLINOIS STREET	BURNHAM AVENUE	WALTER STREET	10,577	52	2020	\$14,639
LNSG::LNG ST::90	LANGE STREET	RANDOLPH STREET	WASHINGTON STREET	9,888	44	2020	\$25,947
LNSG::LRNZ AVE::60	LORENZ AVENUE	176TH STREET	175TH STREET	18,070	52	2020	\$25,009
LNSG::LRNZ AVE::70	LORENZ AVENUE	BERNICE ROAD	BERNICE AVENUE	11,350	54	2020	\$13,716
LNSG::MNR ST::30	MONROE STREET	WENTWORTH AVENUE	MAPLE STREET	23,542	53	2020	\$30,550
LNSG::MPL ST::100	MAPLE STREET	WASHINGTON STREET	MADISON STREET	10,038	52	2020	\$13,893
LNSG::MPL ST::120	MAPLE STREET	MONROE STREET	ADAMS STREET	9,920	46	2020	\$22,242
LNSG::MPL ST::130	MAPLE STREET	ADAMS STREET	JACKSON STREET	9,637	48	2020	\$17,715
LNSG::MPL ST::140	MAPLE STREET	JACKSON STREET	178TH STREET	10,011	43	2020	\$29,851
LNSG::MPL ST::70	MAPLE STREET	TERRACE DRIVE	END	13,823	54	2020	\$16,704
LNSG::OK AVE::50	OAK AVENUE	186TH STREET	185TH STREET	18,447	44	2020	\$48,405
LNSG::OK AVE::90	OAK AVENUE	BERNICE ROAD	OAK AVENUE	17,057	46	2020	\$38,243
LNSG::OTT ST::10	OTTO STREET	BURNHAM AVENUE	WALTER STREET	25,184	43	2020	\$70,665
LNSG::PRK AVE::100	PARK AVENUE	180TH STREET	179TH STREET	18,497	45	2020	\$45,060
LNSG::PRK AVE::120	PARK AVENUE	BERNICE ROAD	173RD STREET	18,747	48	2020	\$34,462
LNSG::PRK AVE::140	PARK AVENUE	172ND STREET	171ST STREET	18,465	47	2020	\$37,745
LNSG::PRK AVE::50	PARK AVENUE	190TH STREET	189TH STREET	8,443	49	2020	\$13,748
LNSG::PXTN AVE::40	PAXTON AVENUE	177TH STREET	176TH PLACE	8,092	48	2020	\$14,876
LNSG::PXTN AVE::50	PAXTON AVENUE	176TH PLACE	FRONTAGE ROAD	21,226	45	2020	\$51,707
LNSG::RCD AVE::50	ARCADIA AVENUE	180TH STREET	179TH STREET	18,435	54	2020	\$22,276
LNSG::RCD AVE::60	ARCADIA AVENUE	179TH STREET	178TH STREET	18,274	53	2020	\$23,714
LNSG::RDG RD::10	RIDGE ROAD	TORRENCE AVENUE	RIDGE ROAD	10,528	50	2020	\$15,448
LNSG::RDG RD::20	RIDGE ROAD	RIDGE ROAD	FRED STREET	16,844	44	2020	\$44,198
LNSG::RDG RD::270	RIDGE ROAD	MAPLE STREET	STATE LINE AVENUE	13,331	49	2020	\$21,707
LNSG::RDG RD::40	RIDGE ROAD	GLEN TERRACE	OAKLEY AVENUE	21,675	54	2020	\$26,192
LNSG::RDG RD::60	RIDGE ROAD	EXCHANGE AVENUE	WARD STREET	22,260	52	2020	\$30,808
LNSG::RDG RD::70	RIDGE ROAD	WARD STREET	SCHOOL STREET	9,925	52	2020	\$13,736
LNSG::RDGWD AVE::10	RIDGEWOOD AVENUE	190TH STREET	189TH PLACE	8,541	52	2020	\$11,821
LNSG::RNDLPH ST::30	RANDOLPH STREET	WENTWORTH AVENUE	LANGE STREET	10,616	43	2020	\$29,789
LNSG::RS AVE::30	ROSE AVENUE	183RD PLACE	182ND PLACE	20,850	54	2020	\$25,195
LNSG::RSWD DR::30	ROSEWOOD DRIVE	177TH STREET	176TH PLACE	10,987	53	2020	\$14,257
LNSG::RY ST::100	ROY STREET	ADAMS STREET	178TH STREET	19,489	54	2020	\$23,551
LNSG::RY ST::160	ROY STREET	173RD STREET	172ND STREET	18,478	54	2020	\$22,328
LNSG::RY ST::170	ROY STREET	172ND STREET	171ST STREET	18,416	53	2020	\$23,899
LNSG::RY ST::80	ROY STREET	ROY STREET	MADISON STREET	10,186	48	2020	\$18,724
LNSG::SCHL ST::30	SCHOOL STREET	184TH STREET	183RD STREET	18,371	48	2020	\$33,770
LNSG::SCHLZ DR N::20	SCHULTZ DRIVE	BURNHAM AVENUE	SCHULTZ DRIVE N	6,771	49	2020	\$11,026
LNSG::SCHLZ DR S::10	SCHULTZ DRIVE S	SCHULTZ DRIVE	WALTER STREET	5,661	49	2020	\$9,219
LNSG::SCHLZ DR S::60	SCHULTZ DRIVE S	GRANT STREET	SHERMAN STREET	7,169	46	2020	\$16,074
LNSG::SCHLZ DR S::80	SCHULTZ DRIVE S	SHERMAN STREET	MILLER DRIVE	11,840	47	2020	\$24,203
LNSG::SCNB AVE::10	ESCANABA AVENUE	186TH STREET	END	12,108	53	2020	\$15,713
LNSG::SCNB AVE::20	ESCANABA AVENUE	INDIANA AVENUE	180TH STREET	24,012	50	2020	\$35,235
LNSG::SHRMN ST::140	SHERMAN STREET	ILLINOIS STREET	STEWART AVENUE	7,957	53	2020	\$10,326
LNSG::SHRMN ST::40	SHERMAN STREET	191ST PLACE	191ST STREET	6,972	54	2020	\$8,425
LNSG::SHRMN ST::50	SHERMAN STREET	191ST STREET	190TH PLACE	7,832	52	2020	\$10,840
LNSG::SHRMN ST::60	SHERMAN STREET	WILLIAM STREET	BERNADINE STREET	13,932	53	2020	\$18,079
LNSG::SHRMN ST::70	SHERMAN STREET	188TH STREET	WILLIAM STREET	18,982	44	2020	\$49,807
LNSG::SHRMN ST::80	SHERMAN STREET	188TH STREET	187TH STREET	19,111	43	2020	\$53,623
LNSG::SHRMN ST::90	SHERMAN STREET	187TH STREET	186TH STREET	19,082	45	2020	\$46,486
LNSG::ST LN AVE::40	STATE LINE AVENUE	177TH STREET	176TH PLACE	10,505	43	2020	\$29,476
LNSG::STN L AVE::10	STONY ISLAND AVENUE	186TH STREET	185TH STREET	13,365	43	2020	\$37,501
LNSG::STN L AVE::40	STONY ISLAND AVENUE	182ND STREET	181ST STREET	18,646	50	2020	\$27,361
LNSG::TN AVE::10	TON AVENUE	BERNICE ROAD	BERNICE AVENUE	9,461	53	2020	\$12,278
LNSG::TRRC DR::30	TERRACE DRIVE	MAPLE STREET	WILLOW LANE	6,188	54	2020	\$7,478
LNSG::VLBRCHT RD::10	VOLBRECHT ROAD	THORNTON LANSING ROAD	FRONTAGE ROAD	60,818	48	2020	\$111,800
LNSG::VLT RD::20	VIOLET ROAD	184TH PLACE	FOREST COURT	4,222	44	2020	\$11,079
LNSG::WHTMN LN::20	WHITMAN LANE	181ST STREET	180TH PLACE	16,415	49	2020	\$26,729

Pavement ID	Road Name	From	To	Area	PCI	Year	Cost
LNSG::WLDWD AVE::150	WILDWOOD AVENUE	184TH STREET	183RD STREET	18,412	48	2020	\$33,846
LNSG::WLDWD AVE::160	WILDWOOD AVENUE	183RD STREET	RIDGE ROAD	18,396	48	2020	\$33,817
LNSG::WLDWD AVE::70	WILDWOOD AVENUE	191ST PLACE	191ST STREET	7,996	48	2020	\$14,699
LNSG::WLLM ST::10	WILLIAM STREET	188TH STREET	SHERMAN STREET	16,199	54	2020	\$19,575
LNSG::WLLM ST::130	WILLIAM STREET	177TH STREET	176TH STREET	18,065	50	2020	\$26,508
LNSG::WLLM ST::150	WILLIAM STREET	173RD PLACE	173RD STREET	9,295	54	2020	\$11,232
LNSG::WLLM ST::170	WILLIAM STREET	172ND PLACE	WILLIAM STREET	3,822	50	2020	\$5,608
LNSG::WLLM ST::190	WILLIAM STREET	171ST STREET	170TH PLACE	8,246	47	2020	\$16,856
LNSG::WLLM ST::20	WILLIAM STREET	188TH STREET	187TH STREET	19,026	50	2020	\$27,918
LNSG::WLLM ST::60	WILLIAM STREET	RANDOLPH STREET	WASHINGTON STREET	9,755	52	2020	\$13,502
LNSG::WLTR ST::100	WALTER STREET	177TH STREET	176TH STREET	18,585	49	2020	\$30,263
LNSG::WLTR ST::110	WALTER STREET	176TH STREET	175TH STREET	18,586	50	2020	\$27,272
LNSG::WLTR ST::120	WALTER STREET	BERNICE ROAD	BERNICE AVENUE	11,568	52	2020	\$16,011
LNSG::WLTR ST::130	WALTER STREET	BERNICE ROAD	173RD STREET	18,565	53	2020	\$24,092
LNSG::WLTR ST::70	WALTER STREET	ADAMS STREET	END	10,932	45	2020	\$26,630
LNSG::WRD ST::10	WARD STREET	RIDGE ROAD	END	16,294	50	2020	\$23,910
LNSG::WSHG TN ST::40	WASHINGTON STREET	WENTWORTH AVENUE	LANGE STREET	10,686	53	2020	\$13,867
LNSG::WWRTH AVE::170	WENTWORTH AVENUE	RIDGE ROAD	WENTWORTH AVENUE	1,941	52	2020	\$2,686
LNSG::WWRTH AVE::200	WENTWORTH AVENUE	RANDOLPH STREET	WASHINGTON STREET	14,048	34	2020	\$61,157
LNSG::WWRTH AVE::210	WENTWORTH AVENUE	WASHINGTON STREET	MADISON STREET	14,025	26	2020	\$77,892
LNSG::WWRTH AVE::220	WENTWORTH AVENUE	MADISON STREET	MONROE STREET	13,774	42	2020	\$43,357
LNSG::WWRTH AVE::230	WENTWORTH AVENUE	MONROE STREET	ADAMS STREET	14,392	40	2020	\$50,929
LNSG::WWRTH AVE::240	WENTWORTH AVENUE	ADAMS STREET	JACKSON STREET	13,663	42	2020	\$43,008
LNSG::WWRTH AVE::250	WENTWORTH AVENUE	JACKSON STREET	178TH STREET	14,187	49	2020	\$23,100
LNSG::WWRTH AVE::260	WENTWORTH AVENUE	178TH STREET	177TH PLACE	13,316	48	2020	\$24,478
LNSG::WWRTH AVE::270	WENTWORTH AVENUE	177TH PLACE	END	8,838	45	2020	\$21,529
LNSG::XCHNG AVE::60	EXCHANGE AVENUE	178TH STREET	177TH STREET	18,246	50	2020	\$26,773
LNSG::169TH CT::10	169TH COURT	170TH STREET	END	28,104	36	2021	\$120,589
LNSG::169TH CT::30	169TH COURT	171ST STREET	170TH PLACE	9,041	40	2021	\$33,038
LNSG::170TH ST::20	170TH STREET	GRANT STREET	169TH COURT	19,065	32	2021	\$93,592
LNSG::171ST ST::40	171ST STREET	CHICAGO AVENUE	PARK AVENUE	9,430	41	2021	\$31,369
LNSG::171ST ST::50	171ST STREET	PARK AVENUE	LORENZ AVENUE	9,308	40	2021	\$33,130
LNSG::171ST ST::70	171ST STREET	GREENBAY AVENUE	BURNHAM AVENUE	8,782	36	2021	\$36,813
LNSG::172ND PL::30	172ND PLACE	WILLIAM STREET	BERNADINE STREET	10,960	41	2021	\$37,858
LNSG::172ND ST::120	172ND STREET	ROY STREET	COMMUNITY STREET	9,288	40	2021	\$33,711
LNSG::172ND ST::140	172ND STREET	WENTWORTH AVENUE	END	13,259	36	2021	\$55,579
LNSG::173RD PL::30	173RD PLACE	HENRY STREET	WILLIAM STREET	18,646	36	2021	\$80,008
LNSG::173RD ST::110	173RD STREET	COMMUNITY STREET	HENRY STREET	9,087	42	2021	\$28,939
LNSG::173RD ST::120	173RD STREET	HENRY STREET	HENRY STREET	5,943	42	2021	\$18,926
LNSG::173RD ST::130	173RD STREET	HENRY STREET	WILLIAM STREET	13,732	54	2021	\$17,017
LNSG::173RD ST::150	173RD STREET	WILLIAM STREET	BERNADINE STREET	6,137	40	2021	\$22,274
LNSG::173RD ST::50	173RD STREET	PARK AVENUE	LORENZ AVENUE	9,246	36	2021	\$39,674
LNSG::173RD ST::60	173RD STREET	LORENZ AVENUE	GREENBAY AVENUE	9,309	33	2021	\$44,261
LNSG::173RD ST::90	173RD STREET	WALTER STREET	ROY STREET	9,218	34	2021	\$42,400
LNSG::175TH ST::70	175TH STREET	ROY STREET	COMMUNITY STREET	9,349	40	2021	\$33,277
LNSG::176TH PL::70	176TH PLACE	MAPLE AVENUE	STATE LINE AVENUE	9,171	41	2021	\$30,508
LNSG::176TH ST::100	176TH STREET	WRIGHT STREET	WILLIAM STREET	9,199	33	2021	\$43,736
LNSG::176TH ST::110	176TH STREET	WILLIAM STREET	BERNADINE STREET	9,216	40	2021	\$32,803
LNSG::177TH PL::10	177TH PLACE	PAXTON AVENUE	ROSEWOOD DRIVE	10,777	36	2021	\$46,242
LNSG::177TH ST::120	177TH STREET	WRIGHT STREET	WILLIAM STREET	9,261	41	2021	\$31,992
LNSG::178TH ST::150	178TH STREET	LORENZ AVENUE	COLUMBIAN DRIVE	5,583	40	2021	\$20,399
LNSG::180TH ST::100	180TH STREET	ESCANABA AVENUE	EXCHANGE AVENUE	9,298	36	2021	\$38,976
LNSG::180TH ST::50	180TH STREET	CRYSTAL LANE	RIDGELAND AVENUE	13,668	54	2021	\$16,938
LNSG::180TH ST::60	180TH STREET	RIDGELAND AVENUE	JASON LANE	13,516	40	2021	\$49,055
LNSG::180TH ST::70	180TH STREET	ARCADIA AVENUE	OAK AVENUE	9,184	40	2021	\$33,560
LNSG::183RD PL::40	183RD PLACE	OLDE FARM ROAD	CARRIAGE LANE	8,697	35	2021	\$38,661
LNSG::183RD PL::70	183RD PLACE	ROSE AVENUE	HICKORY STREET	8,693	41	2021	\$28,917
LNSG::183RD ST::30	183RD STREET	BOCK AVENUE	END	7,060	36	2021	\$29,595
LNSG::184TH PL::20	184TH PLACE	ROSE AVENUE	VIOLET ROAD	22,029	36	2021	\$94,524
LNSG::184TH ST::70	184TH STREET	WILDWOOD AVENUE	OAKWOOD AVENUE	9,072	32	2021	\$44,536
LNSG::184TH ST::90	184TH STREET	RIDGEWOOD AVENUE	CHICAGO AVENUE	9,198	41	2021	\$30,597

Pavement ID	Road Name	From	To	Area	PCI	Year	Cost
LNSG::188TH ST::70	188TH STREET	HENRY STREET	SHERMAN STREET	9,457	41	2021	\$31,459
LNSG::188TH ST::80	188TH STREET	SHERMAN STREET	WILLIAM STREET	9,156	41	2021	\$30,457
LNSG::188TH ST::90	188TH STREET	WILLIAM STREET	BERNADINE STREET	9,382	40	2021	\$33,395
LNSG::189TH PL::10	189TH PLACE	RIDGEWOOD AVENUE	WILDWOOD AVENUE	30,394	54	2021	\$37,664
LNSG::191ST PL::10	191ST PLACE	OAKWOOD AVENUE	WILDWOOD AVENUE	19,451	40	2021	\$69,233
LNSG::191ST PL::30	191ST PLACE	GREENBAY AVENUE	PARK AVENUE	17,927	33	2021	\$85,232
LNSG::191ST PL::40	191ST PLACE	GREENBAY AVENUE	END	3,624	35	2021	\$16,112
LNSG::192ND PL::10	192ND PLACE	OAKWOOD AVENUE	WILDWOOD AVENUE	19,455	36	2021	\$83,478
LNSG::193RD PL::30	193RD PLACE	OAKWOOD AVENUE	OAKWOOD AVENUE	9,412	54	2021	\$11,663
LNSG::193RD PL::40	193RD PLACE	OAKWOOD AVENUE	END	4,032	37	2021	\$16,570
LNSG::193RD ST::10	193RD STREET	OAKWOOD AVENUE	WILDWOOD AVENUE	19,483	36	2021	\$83,599
LNSG::193RD ST::70	193RD STREET	BERNADINE STREET	END	2,665	40	2021	\$9,740
LNSG::AD ST::20	ADA STREET	192ND PLACE	191ST STREET	24,659	32	2021	\$121,048
LNSG::AD ST::30	ADA STREET	186TH STREET	SCHULTZ DRIVE S	10,405	35	2021	\$46,254
LNSG::AD ST::60	ADA STREET	ANN STREET	ILLINOIS STREET	15,180	54	2021	\$18,811
LNSG::BRNC AVE::60	BERNICE AVENUE	GREENBAY AVENUE	BURNHAM AVENUE	10,024	40	2021	\$36,627
LNSG::BRITNY LN::10	BRITTANY LANE	182ND STREET	181ST STREET	18,506	40	2021	\$67,624
LNSG::CHCG AVE::50	CHICAGO AVENUE	183RD STREET	183RD STREET	2,281	40	2021	\$8,117
LNSG::CHRRY LN::10	CHERRY LANE	188TH STREET	END	11,603	41	2021	\$40,081
LNSG::CLYD AVE::50	CLYDE AVENUE	182ND STREET	END	5,732	40	2021	\$20,944
LNSG::CNTRY LN::20	COUNTRY LANE	185TH STREET	183RD PLACE	28,740	54	2021	\$35,614
LNSG::FRST VW LN::10	FOREST VIEW LANE	186TH STREET	END	34,909	35	2021	\$155,183
LNSG::GLN OK AVE::30	GLEN OAK AVENUE	184TH STREET	END	8,233	41	2021	\$28,439
LNSG::GLN TERR::10	GLEN TERRACE	RIDGE ROAD	INDIANA AVENUE	15,994	41	2021	\$55,247
LNSG::GRNBY AVE::130	GREENBAY AVENUE	BERNICE ROAD	173RD STREET	18,686	37	2021	\$76,799
LNSG::GRNBY AVE::150	GREENBAY AVENUE	172ND STREET	171ST STREET	18,458	35	2021	\$82,050
LNSG::GRNBY AVE::160	GREENBAY AVENUE	171ST STREET	170TH STREET	18,121	40	2021	\$64,498
LNSG::GRNT ST::80	GRANT STREET	171ST STREET	170TH STREET	7,333	41	2021	\$25,329
LNSG::HCKRY ST::30	HICKORY STREET	183RD PLACE	182ND PLACE	17,042	36	2021	\$71,437
LNSG::HNRV ST::160	HENRY STREET	173RD STREET	173RD STREET	3,889	54	2021	\$4,820
LNSG::HNRV ST::170	HENRY STREET	173RD STREET	172ND PLACE	9,273	35	2021	\$41,220
LNSG::HNRV ST::190	HENRY STREET	171ST STREET	170TH STREET	9,789	37	2021	\$40,232
LNSG::HNRV ST::90	HENRY STREET	ADAMS STREET	178TH STREET	19,642	54	2021	\$24,341
LNSG::JCKSN ST::10	JACKSON STREET	WILLIAM STREET	WENTWORTH AVENUE	19,315	40	2021	\$70,102
LNSG::KLY AVE::80	OAKLEY AVENUE	INDIANA AVENUE	180TH STREET	22,815	40	2021	\$82,806
LNSG::KWD AVE::180	OAKWOOD AVENUE	181ST STREET	180TH STREET	18,656	54	2021	\$23,119
LNSG::KWD AVE::20	OAKWOOD AVENUE	194TH STREET	193RD PLACE	8,635	36	2021	\$37,051
LNSG::KWD AVE::90	OAKWOOD AVENUE	190TH PLACE	190TH STREET	12,237	34	2021	\$56,289
LNSG::LCST ST::60	LOCUST STREET	183RD STREET	182ND PLACE	9,176	41	2021	\$31,696
LNSG::LD FRM RD::40	OLDE FARM ROAD	HEARTH CIRCLE	183RD PLACE	7,936	41	2021	\$27,415
LNSG::LD FRM RD::50	OLDE FARM ROAD	183RD PLACE	183RD PLACE	6,737	40	2021	\$24,619
LNSG::LS CT::10	LOUISE COURT	LOUISE DRIVE	END	2,415	42	2021	\$7,691
LNSG::LS DR::20	LOUISE DRIVE	LOUISE COURT	N MANOR DRIVE	21,288	41	2021	\$73,536
LNSG::MDSN ST::10	MADISON STREET	ROY STREET	COMMUNITY STREET	9,340	37	2021	\$38,389
LNSG::MDSN ST::40	MADISON STREET	WILLIAM STREET	WENTWORTH AVENUE	19,669	35	2021	\$87,434
LNSG::MPL CT::10	MAPLE COURT	MAPLE AVENUE	END	4,451	42	2021	\$14,175
LNSG::MPL ST::110	MAPLE STREET	MADISON STREET	MONROE STREET	9,599	40	2021	\$35,077
LNSG::OK ST::10	OAK STREET	GLENWOOD LANSING ROAD	194TH PLACE	5,256	35	2021	\$23,363
LNSG::PRK AVE::10	PARK AVENUE	195TH COURT	END	3,834	35	2021	\$17,043
LNSG::PRK AVE::40	PARK AVENUE	191ST PLACE	191ST STREET	8,233	36	2021	\$35,326
LNSG::RCD AVE::10	ARCADIA AVENUE	185TH STREET	END	5,636	54	2021	\$6,985
LNSG::RDG RD::140	RIDGE ROAD	OAK STREET	LORENZ AVENUE	10,087	54	2021	\$12,500
LNSG::RDGWD AVE::20	RIDGEWOOD AVENUE	189TH PLACE	189TH STREET	8,180	54	2021	\$10,137
LNSG::RS AVE::10	ROSE AVENUE	185TH COURT	185TH PLACE	8,076	36	2021	\$34,652
LNSG::RSWD DR::10	ROSEWOOD DRIVE	177TH PLACE	END	7,606	34	2021	\$34,985
LNSG::RSWD DR::20	ROSEWOOD DRIVE	177TH PLACE	177TH STREET	8,379	40	2021	\$30,411
LNSG::RY ST::90	ROY STREET	MADISON STREET	ADAMS STREET	19,853	40	2021	\$70,665
LNSG::S MNR DR::20	S MANOR DRIVE	W LOUISE DRIVE	LOUISE DRIVE	29,018	32	2021	\$142,446
LNSG::SCHL ST::10	SCHOOL STREET	185TH STREET	END	3,145	40	2021	\$11,194
LNSG::SCHL ST::110	SCHOOL STREET	178TH STREET	177TH STREET	18,216	34	2021	\$83,793
LNSG::SCHLZ DR N::120	SCHULTZ DRIVE	SCHULTZ DRIVE N	WENTWORTH AVENUE	5,924	31	2021	\$29,998

Pavement ID	Road Name	From	To	Area	PCI	Year	Cost
LNSG::SCHLZ DR N::70	SCHULTZ DRIVE	ADA STREET	GRANT STREET	12,708	36	2021	\$54,527
LNSG::SHRMN ST::130	SHERMAN STREET	ANN STREET	ILLINOIS STREET	15,396	40	2021	\$54,801
LNSG::ST LN AVE::30	STATE LINE AVENUE	WASHINGTON STREET	177TH STREET	67,505	36	2021	\$282,973
LNSG::W LS DR::10	W LOUISE DRIVE	S MANOR DRIVE	N MANOR DRIVE	5,808	41	2021	\$20,064
LNSG::W LS DR::20	W LOUISE DRIVE	N MANOR DRIVE	LOUISE DRIVE	19,403	32	2021	\$95,246
LNSG::WLDWD AVE::120	WILDWOOD AVENUE	186TH STREET	END	6,067	36	2021	\$26,033
LNSG::WLDWD AVE::140	WILDWOOD AVENUE	185TH STREET	184TH STREET	18,548	32	2021	\$91,053
LNSG::WLDWD AVE::50	WILDWOOD AVENUE	192ND PLACE	192ND STREET	8,895	42	2021	\$28,328
LNSG::WLDWD AVE::60	WILDWOOD AVENUE	192ND STREET	191ST PLACE	9,787	40	2021	\$34,835
LNSG::WLDWD AVE::80	WILDWOOD AVENUE	191ST STREET	190TH PLACE	7,520	42	2021	\$23,948
LNSG::WLLM ST::100	WILLIAM STREET	ADAMS STREET	JACKSON STREET	9,505	40	2021	\$33,831
LNSG::WLLM ST::70	WILLIAM STREET	WASHINGTON STREET	MADISON STREET	9,863	40	2021	\$35,107
LNSG::WLTR ST::140	WALTER STREET	173RD STREET	172ND STREET	18,484	37	2021	\$75,970
LNSG::WLTR ST::150	WALTER STREET	172ND STREET	171ST STREET	18,519	36	2021	\$77,631
LNSG::WWRTH AVE::190	WENTWORTH AVENUE	LAKE STREET	RANDOLPH STREET	13,972	54	2021	\$17,314
LNSG::XCHNG AVE::10	EXCHANGE AVENUE	RIDGE ROAD	END	43,285	40	2021	\$158,168
LNSG::XCHNG AVE::20	EXCHANGE AVENUE	INDIANA AVENUE	181ST STREET	9,300	40	2021	\$33,101
LNSG::XCHNG AVE::50	EXCHANGE AVENUE	179TH STREET	178TH STREET	18,305	33	2021	\$87,028
LNSG::171ST ST::120	171ST STREET	HENRY STREET	GRANT STREET	8,739	27	2022	\$50,795
LNSG::173RD PL::40	173RD PLACE	WILLIAM STREET	BERNADINE STREET	9,238	30	2022	\$49,280
LNSG::173RD ST::40	173RD STREET	CHICAGO AVENUE	PARK AVENUE	9,520	26	2022	\$56,846
LNSG::173RD ST::70	173RD STREET	GREENBAY AVENUE	BURNHAM AVENUE	9,108	30	2022	\$48,590
LNSG::173RD ST::80	173RD STREET	BURNHAM AVENUE	WALTER STREET	9,372	25	2022	\$57,456
LNSG::176TH PL::60	176TH PLACE	WENTWORTH AVENUE	MAPLE STREET	24,161	24	2022	\$151,969
LNSG::177TH ST::130	177TH STREET	WILLIAM STREET	BERNADINE STREET	9,207	24	2022	\$57,912
LNSG::177TH ST::140	177TH STREET	BERNADINE STREET	WENTWORTH AVENUE	9,425	24	2022	\$59,280
LNSG::177TH ST::30	177TH STREET	LOCUST STREET	TORRENCE AVENUE	36,708	29	2022	\$201,673
LNSG::177TH ST::70	177TH STREET	COMMERCIAL STREET	SCHOOL STREET	9,033	26	2022	\$53,939
LNSG::178TH ST::210	178TH STREET	HENRY STREET	WILLIAM STREET	18,165	55	2022	\$22,216
LNSG::178TH ST::240	178TH STREET	WENTWORTH AVENUE	MAPLE STREET	23,893	55	2022	\$66,032
LNSG::179TH ST::60	179TH STREET	EXCHANGE AVENUE	COMMERCIAL STREET	9,293	55	2022	\$11,365
LNSG::179TH ST::80	179TH STREET	SCHOOL STREET	WILDWOOD AVENUE	9,407	26	2022	\$56,176
LNSG::180TH ST::200	180TH STREET	BURNHAM AVENUE	END	5,270	30	2022	\$28,115
LNSG::182ND PL::10	182ND PLACE	CLYDE AVENUE	END	10,106	24	2022	\$63,563
LNSG::183RD PL::30	183RD PLACE	DEJONG LANE	OLDE FARM ROAD	13,217	23	2022	\$85,234
LNSG::183RD ST::100	183RD STREET	OAKWOOD AVENUE	RIDGEWOOD AVENUE	9,436	55	2022	\$11,541
LNSG::185TH CT::10	185TH COURT	HICKORY STREET	ROSE AVENUE	7,426	30	2022	\$39,615
LNSG::185TH PL::10	185TH PLACE	ROSE AVENUE	LOCUST STREET	12,991	28	2022	\$73,441
LNSG::185TH ST::10	185TH STREET	STONY ISLAND AVENUE	HOLLAND ROAD	44,401	27	2022	\$258,073
LNSG::185TH ST::120	185TH STREET	ARCADIA AVENUE	OAK AVENUE	9,387	55	2022	\$11,481
LNSG::185TH ST::30	185TH STREET	COUNTRY LANE	CLYDE AVENUE	8,698	24	2022	\$54,708
LNSG::185TH ST::40	185TH STREET	CLYDE AVENUE	DEJONG LANE	9,017	24	2022	\$56,714
LNSG::187TH PL::10	187TH PLACE	TORRENCE AVENUE	OAK AVENUE	28,669	26	2022	\$171,194
LNSG::188TH PL::10	188TH PLACE	RIDGEWOOD AVENUE	OAKWOOD AVENUE	16,001	55	2022	\$19,570
LNSG::189TH ST::30	189TH STREET	BURNHAM AVENUE	GREENBAY AVENUE	6,552	29	2022	\$35,997
LNSG::190TH PL::30	190TH PLACE	SHERMAN STREET	BERNADINE STREET	21,340	26	2022	\$127,431
LNSG::192ND ST::60	192ND STREET	MAPLE STREET	END	7,153	24	2022	\$44,989
LNSG::193RD CT::10	193RD COURT	193RD STREET	END	5,273	26	2022	\$31,490
LNSG::193RD PL::50	193RD PLACE	WENTWORTH AVENUE	END	27,106	27	2022	\$157,547
LNSG::193RD ST::20	193RD STREET	OAKWOOD AVENUE	END	13,540	28	2022	\$76,543
LNSG::193RD ST::90	193RD STREET	193RD COURT	MAPLE STREET	8,731	25	2022	\$53,525
LNSG::194TH ST::10	194TH STREET	WILDWOOD AVENUE	OAKWOOD AVENUE	19,633	24	2022	\$123,490
LNSG::AD ST::10	ADA STREET	193RD STREET	192ND PLACE	9,338	27	2022	\$54,277
LNSG::BCK AVE::20	BOCK AVENUE	185TH STREET	FOREST COURT	10,764	24	2022	\$67,706
LNSG::BNSLY AVE::10	BENSLEY AVENUE	173RD STREET	BERNICE ROAD	18,383	28	2022	\$103,921
LNSG::BNSLY AVE::20	BENSLEY AVENUE	173RD STREET	172ND STREET	18,116	25	2022	\$111,061
LNSG::CHRSTN DR::10	CHRISTINA DRIVE	OAKLEY AVENUE	186TH STREET	38,758	28	2022	\$219,103
LNSG::CMMNTY ST::60	COMMUNITY STREET	BERNICE ROAD	BERNICE AVENUE	11,527	29	2022	\$63,331
LNSG::DCKR AVE::10	DECKER AVENUE	179TH STREET	END	14,900	30	2022	\$79,486
LNSG::FLNGN DR::10	FLANIGAN DRIVE	CHICAGO AVENUE	END	8,319	55	2022	\$10,175
LNSG::FRST CT::10	FOREST COURT	BOCK AVENUE	LOCUST STREET	8,324	28	2022	\$47,057

Pavement ID	Road Name	From	To	Area	PCI	Year	Cost
LNSG::GRNBAY AVE::10	GREENBAY AVENUE	GLENWOOD LANSING ROAD	END	6,985	29	2022	\$38,376
LNSG::GRNT ST::60	GRANT STREET	172ND PLACE	171ST STREET	20,334	29	2022	\$111,714
LNSG::GRNT ST::70	GRANT STREET	171ST STREET	171ST STREET	8,291	26	2022	\$49,511
LNSG::HCKRY ST::10	HICKORY STREET	186TH STREET	185TH COURT	5,729	28	2022	\$32,387
LNSG::HCKRY ST::50	HICKORY STREET	179TH STREET	END	17,216	25	2022	\$105,548
LNSG::HLLND RD::10	HOLLAND ROAD	185TH STREET	183RD STREET	37,947	55	2022	\$46,411
LNSG::KWD AVE::10	OAKWOOD AVENUE	GLENWOOD LANSING ROAD	194TH STREET	5,621	28	2022	\$31,774
LNSG::KWD AVE::220	OAKWOOD AVENUE	WEST LAKE ROAD	173RD PLACE	4,590	28	2022	\$25,946
LNSG::LCST ST::90	LOCUST STREET	THORNTON LANSING ROAD	END	37,042	27	2022	\$215,298
LNSG::LL ND DR::10	ILLI INDI DRIVE	LANGE STREET	ILLI INDI DRIVE	3,530	30	2022	\$18,830
LNSG::LNG ST::10	LANGE STREET	192ND PLACE	193RD STREET	6,941	25	2022	\$42,550
LNSG::LNG ST::20	LANGE STREET	192ND PLACE	192ND STREET	6,275	24	2022	\$39,466
LNSG::LS DR::10	LOUISE DRIVE	W LOUISE DRIVE	LOUISE COURT	10,491	29	2022	\$57,639
LNSG::N MNR DR::10	N MANOR DRIVE	W LOUISE DRIVE	LOUISE DRIVE	23,503	26	2022	\$140,348
LNSG::OK AVE::10	OAK AVENUE	188TH STREET	187TH PLACE	8,672	28	2022	\$49,021
LNSG::OK AVE::70	OAK AVENUE	INDIANA AVENUE	180TH STREET	21,608	27	2022	\$125,594
LNSG::PRK AVE::30	PARK AVENUE	GLENWOOD LANSING ROAD	194TH PLACE	5,890	29	2022	\$32,358
LNSG::RDG RD::190	RIDGE ROAD	ROY STREET	ADA STREET	15,020	55	2022	\$18,370
LNSG::RDGWD AVE::70	RIDGEWOOD AVENUE	183RD STREET	RIDGE ROAD	18,440	24	2022	\$115,983
LNSG::RDGWD AVE::80	RIDGEWOOD AVENUE	RIDGE ROAD	181ST STREET	18,570	27	2022	\$107,937
LNSG::RS AVE::20	ROSE AVENUE	185TH STREET	184TH PLACE	8,332	29	2022	\$45,778
LNSG::S MNR DR::10	S MANOR DRIVE	BURNHAM AVENUE	W LOUISE DRIVE	4,731	55	2022	\$5,786
LNSG::S MNR DR::30	S MANOR DRIVE	LOUISE DRIVE	SHARON COURT	8,922	25	2022	\$54,696
LNSG::S MNR DR::40	S MANOR DRIVE	SHARON COURT	BARBARA COURT	8,806	24	2022	\$55,385
LNSG::SCHLZ DR N::10	SCHULTZ DRIVE	GREENBAY AVENUE	BURNHAM AVENUE	9,685	26	2022	\$57,833
LNSG::SHRMN ST::10	SHERMAN STREET	193RD STREET	193RD STREET	4,340	28	2022	\$24,537
LNSG::SHRMN ST::120	SHERMAN STREET	SCHULTZ DRIVE N	ANN STREET	8,631	24	2022	\$54,285
LNSG::SHRMN ST::150	SHERMAN STREET	RIDGE ROAD	END	7,873	31	2022	\$40,747
LNSG::SHRMN ST::20	SHERMAN STREET	193RD STREET	192ND STREET	18,343	24	2022	\$115,373
LNSG::ST NDRW ST::10	ST ANDREWS STREET	STONY ISLAND AVENUE	END	7,592	24	2022	\$47,754
LNSG::VLT RD::10	VIOLET ROAD	185TH STREET	184TH PLACE	7,831	26	2022	\$46,762
LNSG::VLT RD::50	VIOLET ROAD	RIDGE ROAD	INDIANA AVENUE	17,980	31	2022	\$93,057
LNSG::WLLM ST::180	WILLIAM STREET	172ND PLACE	171ST STREET	16,546	24	2022	\$104,074
LNSG::180TH ST::80	180TH STREET	OAK AVENUE	OAKLEY AVENUE	9,456	22	2023	\$63,977
LNSG::180TH ST::90	180TH STREET	OAKLEY AVENUE	ESCANABA AVENUE	9,367	22	2023	\$63,371
LNSG::181ST ST::20	181ST STREET	STONY ISLAND AVENUE	BRITTANY LANE	5,859	21	2023	\$40,597
LNSG::182ND PL::20	182ND PLACE	CLYDE AVENUE	DEJONG LANE	6,292	21	2023	\$43,599
LNSG::183RD ST::90	183RD STREET	WILDWOOD AVENUE	OAKWOOD AVENUE	9,172	22	2023	\$62,052
LNSG::185TH ST::50	185TH STREET	DEJONG LANE	OLDE FARM ROAD	18,833	22	2023	\$127,411
LNSG::185TH ST::60	185TH STREET	OLDE FARM ROAD	CARRIAGE LANE	8,628	22	2023	\$58,371
LNSG::185TH ST::80	185TH STREET	ROSE AVENUE	HICKORY STREET	11,755	22	2023	\$79,531
LNSG::185TH ST::90	185TH STREET	HICKORY STREET	VIOLET ROAD	8,862	21	2023	\$61,409
LNSG::191ST PL::60	191ST PLACE	BERNADINE STREET	WENTWORTH AVENUE	11,900	21	2023	\$82,461
LNSG::191ST ST::110	191ST STREET	SHERMAN STREET	BERNADINE STREET	21,998	22	2023	\$148,827
LNSG::192ND PL::20	192ND PLACE	OAKWOOD AVENUE	END	13,391	22	2023	\$90,593
LNSG::192ND ST::30	192ND STREET	WALTER STREET	ROY STREET	8,872	22	2023	\$60,021
LNSG::192ND ST::50	192ND STREET	LANGE STREET	MAPLE STREET	20,931	21	2023	\$145,037
LNSG::193RD ST::30	193RD STREET	BURNHAM AVENUE	ADA STREET	24,405	22	2023	\$165,113
LNSG::193RD ST::40	193RD STREET	ADA STREET	GRANT STREET	8,769	22	2023	\$59,327
LNSG::193RD ST::60	193RD STREET	SHERMAN STREET	BERNADINE STREET	24,296	21	2023	\$168,356
LNSG::194TH PL::10	194TH PLACE	PARK AVENUE	OAK STREET	9,980	22	2023	\$67,517
LNSG::195TH CT::10	195TH COURT	PARK AVENUE	END	8,067	21	2023	\$55,897
LNSG::ANN ST::60	ANN STREET	GRANT STREET	SHERMAN STREET	9,229	21	2023	\$63,951
LNSG::ANN ST::70	ANN STREET	SHERMAN STREET	MILLER DRIVE	8,335	22	2023	\$56,388
LNSG::BCK AVE::30	BOCK AVENUE	FOREST COURT	183RD PLACE	13,140	22	2023	\$88,896
LNSG::BCK AVE::40	BOCK AVENUE	183RD PLACE	183RD STREET	13,674	22	2023	\$92,509
LNSG::BRNDN ST::10	BERNADINE STREET	193RD STREET	192ND PLACE	14,265	22	2023	\$96,511
LNSG::BRNDN ST::120	BERNADINE STREET	173RD STREET	172ND PLACE	9,400	22	2023	\$63,594
LNSG::BRNDN ST::20	BERNADINE STREET	191ST PLACE	191ST STREET	7,752	21	2023	\$53,716
LNSG::CLYD AVE::20	CLYDE AVENUE	185TH STREET	183RD PLACE	28,994	22	2023	\$196,160
LNSG::CNTRY LN::10	COUNTRY LANE	186TH STREET	185TH STREET	18,013	22	2023	\$121,867

Pavement ID	Road Name	From	To	Area	PCI	Year	Cost
LNSG::CRRG LN::20	CARRIAGE LANE	185TH STREET	183RD PLACE	23,979	21	2023	\$166,160
LNSG::DMS ST::60	ADAMS STREET	WILLIAM STREET	WENTWORTH AVENUE	19,476	21	2023	\$134,959
LNSG::GRNBY AVE::140	GREENBAY AVENUE	173RD STREET	172ND STREET	18,497	21	2023	\$128,175
LNSG::GRNT ST::10	GRANT STREET	193RD STREET	191ST STREET	34,103	21	2023	\$236,312
LNSG::HCKRY ST::20	HICKORY STREET	185TH COURT	185TH STREET	14,583	22	2023	\$98,661
LNSG::LD FRM RD::60	OLDE FARM ROAD	183RD PLACE	182ND STREET	26,412	21	2023	\$183,020
LNSG::LS DR::30	LOUISE DRIVE	S MANOR DRIVE	N MANOR DRIVE	8,794	21	2023	\$60,935
LNSG::MDSN ST::30	MADISON STREET	HENRY STREET	WILLIAM STREET	17,638	21	2023	\$122,219
LNSG::MPL AVE::20	MAPLE AVENUE	SHIRLEY DRIVE	MAPLE COURT	11,685	21	2023	\$80,972
LNSG::MPL AVE::30	MAPLE AVENUE	176TH PLACE	MAPLE COURT	13,914	22	2023	\$94,133
LNSG::MPL ST::10	MAPLE STREET	193RD STREET	192ND STREET	12,829	21	2023	\$88,900
LNSG::OK AVE::20	OAK AVENUE	187TH PLACE	187TH STREET	9,347	21	2023	\$64,767
LNSG::OK AVE::40	OAK AVENUE	186TH PLACE	186TH STREET	9,783	22	2023	\$66,186
LNSG::RDGWD AVE::90	RIDGEWOOD AVENUE	181ST STREET	180TH STREET	18,626	21	2023	\$129,065
LNSG::S MNR DR::50	S MANOR DRIVE	BARBARA COURT	WENTWORTH AVENUE	24,347	21	2023	\$168,708
LNSG::SCHL ST::20	SCHOOL STREET	185TH STREET	184TH STREET	18,543	21	2023	\$128,490
LNSG::SHRMN ST::30	SHERMAN STREET	192ND STREET	191ST PLACE	10,351	22	2023	\$70,030
LNSG::SHRN CT::10	SHARON COURT	S MANOR DRIVE	END	16,349	22	2023	\$110,607
LNSG::SHRY DR::10	SHIRLEY DRIVE	MAPLE AVENUE	END	10,277	21	2023	\$71,211
LNSG::ST LN AVE::10	STATE LINE AVENUE	RIDGE ROAD	LAKE STREET	18,977	21	2023	\$131,499
LNSG::ST LN AVE::20	STATE LINE AVENUE	LAKE STREET	WASHINGTON STREET	19,491	21	2023	\$135,059
LNSG::WLDWD AVE::30	WILDWOOD AVENUE	193RD PLACE	193RD STREET	8,621	22	2023	\$58,322
LNSG::WLTR ST::10	WALTER STREET	191ST STREET	192ND STREET	15,385	22	2023	\$104,085
LNSG::WLTR ST::20	WALTER STREET	188TH STREET	OTTO STREET	23,838	20	2023	\$169,090
LNSG::WLTR ST::30	WALTER STREET	186TH STREET	SCHULTZ DRIVE S	13,410	22	2023	\$90,723
LNSG::169TH CT::40	169TH COURT	WENTWORTH AVENUE	171ST STREET	9,953	16	2024	\$72,817
LNSG::171ST ST::100	171ST STREET	ROY STREET	COMMUNITY STREET	6,767	4	2024	\$49,508
LNSG::171ST ST::110	171ST STREET	COMMUNITY STREET	HENRY STREET	8,781	9	2024	\$64,248
LNSG::171ST ST::130	171ST STREET	GRANT STREET	WILLIAM STREET	9,010	10	2024	\$65,922
LNSG::171ST ST::140	171ST STREET	WILLIAM STREET	BERNADINE STREET	11,621	8	2024	\$85,024
LNSG::173RD PL::20	173RD PLACE	OAKWOOD AVENUE	CHICAGO AVENUE	21,574	16	2024	\$157,842
LNSG::181ST ST::30	181ST STREET	BRITTANY LANE	CRYSTAL LANE	8,563	18	2024	\$62,649
LNSG::183RD PL::20	183RD PLACE	CLYDE AVENUE	DEJONG LANE	9,044	19	2024	\$66,167
LNSG::183RD ST::120	183RD STREET	CHICAGO AVENUE	END	13,857	17	2024	\$101,379
LNSG::184TH PL::10	184TH PLACE	LOCUST STREET	ROSE AVENUE	8,195	18	2024	\$59,957
LNSG::184TH ST::60	184TH STREET	SCHOOL STREET	WILDWOOD AVENUE	9,071	18	2024	\$66,367
LNSG::185TH ST::170	185TH STREET	RIDGEWOOD AVENUE	CHICAGO AVENUE	9,196	19	2024	\$67,280
LNSG::185TH ST::70	185TH STREET	CARRIAGE LANE	BOCK AVENUE	8,437	19	2024	\$61,729
LNSG::187TH ST::10	187TH STREET	TORRENCE AVENUE	OAK AVENUE	28,540	18	2024	\$208,806
LNSG::188TH ST::10	188TH STREET	TORRENCE AVENUE	OAK AVENUE	28,306	19	2024	\$207,092
LNSG::188TH ST::50	188TH STREET	WALTER STREET	END	4,037	4	2024	\$29,536
LNSG::192ND PL::30	192ND PLACE	ADA STREET	END	10,325	19	2024	\$75,543
LNSG::192ND PL::40	192ND PLACE	BERNADINE STREET	WENTWORTH AVENUE	7,856	18	2024	\$57,480
LNSG::193RD ST::50	193RD STREET	GRANT STREET	SHERMAN STREET	8,445	18	2024	\$61,783
LNSG::193RD ST::80	193RD STREET	LANGHE STREET	193RD COURT	12,129	19	2024	\$88,742
LNSG::BRBR CT::10	BARBARA COURT	S MANOR DRIVE	END	10,292	19	2024	\$75,300
LNSG::CHCG AVE::30	CHICAGO AVENUE	185TH STREET	184TH STREET	18,565	17	2024	\$135,827
LNSG::CHCG AVE::60	CHICAGO AVENUE	183RD STREET	182ND PLACE	7,699	18	2024	\$56,325
LNSG::CHCG AVE::70	CHICAGO AVENUE	182ND PLACE	RIDGE ROAD	10,763	19	2024	\$78,746
LNSG::CLYD AVE::30	CLYDE AVENUE	183RD PLACE	182ND PLACE	18,613	19	2024	\$136,180
LNSG::CLYD AVE::40	CLYDE AVENUE	182ND PLACE	182ND STREET	9,042	19	2024	\$66,155
LNSG::CRRG LN::10	CARRIAGE LANE	186TH STREET	185TH STREET	20,927	19	2024	\$153,109
LNSG::CRYSTL LN::10	CRYSTAL LANE	182ND STREET	181ST STREET	18,550	14	2024	\$135,717
LNSG::DJNG LN::10	DEJONG LANE	185TH STREET	183RD PLACE	29,115	19	2024	\$213,010
LNSG::DJNG LN::20	DEJONG LANE	183RD PLACE	182ND PLACE	17,999	19	2024	\$131,685
LNSG::DMS ST::50	ADAMS STREET	HENRY STREET	WILLIAM STREET	17,757	13	2024	\$129,913
LNSG::DNT AVE::10	DANTE AVENUE	186TH STREET	END	8,855	4	2024	\$64,783
LNSG::DNT AVE::20	DANTE AVENUE	START	END	6,267	4	2024	\$45,854
LNSG::GRNBY AVE::120	GREENBAY AVENUE	BERNICE AVENUE	END	6,629	17	2024	\$48,497
LNSG::HNRV ST::70	HENRY STREET	MADISON STREET	MONROE STREET	9,728	12	2024	\$71,175
LNSG::HNRV ST::80	HENRY STREET	MONROE STREET	ADAMS STREET	10,159	19	2024	\$74,326

Pavement ID	Road Name	From	To	Area	PCI	Year	Cost
LNSG::KWD AVE::130	OAKWOOD AVENUE	185TH STREET	END	13,206	19	2024	\$96,621
LNSG::LD FRM RD::10	OLDE FARM ROAD	186TH STREET	185TH STREET	20,827	19	2024	\$152,379
LNSG::MDSN ST::50	MADISON STREET	WENTWORTH AVENUE	MAPLE STREET	23,428	16	2024	\$171,404
LNSG::MNR ST::10	MONROE STREET	HENRY STREET	WILLIAM STREET	17,679	16	2024	\$129,343
LNSG::MPL AVE::10	MAPLE AVENUE	WENTWORTH AVENUE	SHIRLEY DRIVE	8,221	9	2024	\$60,147
LNSG::OK AVE::30	OAK AVENUE	187TH STREET	186TH PLACE	9,676	19	2024	\$70,790
LNSG::RDGWD AVE::50	RIDGEWOOD AVENUE	185TH STREET	184TH STREET	18,559	18	2024	\$135,786
LNSG::RDGWD AVE::60	RIDGEWOOD AVENUE	184TH STREET	183RD STREET	18,497	19	2024	\$135,332
LNSG::RY ST::10	ROY STREET	192ND STREET	191ST STREET	15,193	19	2024	\$111,154
LNSG::SHRMN ST::110	SHERMAN STREET	SCHULTZ DRIVE S	SCHULTZ DRIVE N	13,021	19	2024	\$95,264
LNSG::WLDWD AVE::130	WILDWOOD AVENUE	185TH STREET	END	7,908	19	2024	\$57,858
LNSG::WLLM ST::80	WILLIAM STREET	MADISON STREET	MONROE STREET	9,678	19	2024	\$70,804
LNSG::WLLM ST::90	WILLIAM STREET	MONROE STREET	ADAMS STREET	10,202	17	2024	\$74,641
LNSG::WLLW LN::20	WILLOW LANE	TERRACE DRIVE	ILLI INDI DRIVE	28,073	19	2024	\$205,391
LNSG::178TH ST::20	178TH STREET	TORRENCE AVENUE	GLEN OAK AVENUE	9,304	54	2025	\$13,107
LNSG::188TH ST::30	188TH STREET	CHERRY LANE	PARK AVENUE	8,690	54	2025	\$12,242
LNSG::CHCG AVE::210	CHICAGO AVENUE	171ST STREET	170TH STREET	18,359	54	2025	\$25,862
LNSG::GLN OK AVE::20	GLEN OAK AVENUE	185TH STREET	184TH STREET	18,644	54	2025	\$26,263
LNSG::KWD AVE::50	OAKWOOD AVENUE	192ND PLACE	192ND STREET	8,757	54	2025	\$12,335
LNSG::LK ST::30	LAKE STREET	WENTWORTH AVENUE	LANGE STREET	10,565	54	2025	\$14,882
LNSG::LRNZ AVE::100	LORENZ AVENUE	172ND STREET	171ST STREET	18,427	54	2025	\$25,957
LNSG::MDSN ST::20	MADISON STREET	COMMUNITY STREET	HENRY STREET	9,239	54	2025	\$13,015
LNSG::PRK AVE::60	PARK AVENUE	189TH STREET	188TH PLACE	14,853	54	2025	\$20,922
LNSG::PRK AVE::70	PARK AVENUE	188TH PLACE	188TH STREET	5,152	54	2025	\$7,257
LNSG::PRK AVE::90	PARK AVENUE	181ST STREET	180TH STREET	18,617	54	2025	\$26,225
LNSG::PRK BLVD::10	PARK BOULEVARD	178TH STREET	CHICAGO AVENUE	17,446	54	2025	\$24,575
LNSG::RDG RD::210	RIDGE ROAD	HENRY STREET	SHERMAN STREET	14,328	54	2025	\$20,184
LNSG::RDG RD::240	RIDGE ROAD	WEST STREET	WENTWORTH AVENUE	32,649	54	2025	\$45,992
LNSG::SCHLZ DR N::30	SCHULTZ DRIVE	SCHULTZ DRIVE	WALTER STREET	5,588	54	2025	\$7,871
LNSG::WWRTH AVE::180	WENTWORTH AVENUE	WENTWORTH AVENUE	LAKE STREET	17,466	54	2025	\$24,604

APPENDIX D – PAVEMENT MAINTENANCE POLICIES AND UNIT COSTS

Table D-1. Recommended Asphalt Pavement Maintenance Policy.

Pavement Distress	Severity	Recommended Maintenance Type	Units
Alligator Cracking	Low	Crack Sealing	FT
Alligator Cracking	Medium	Patching - AC Deep	SF
Alligator Cracking	High	Patching - AC Deep	SF
Block Cracking	Low	Crack Sealing - AC	FT
Block Cracking	Medium	Crack Sealing - AC	FT
Block Cracking	High	Patching - AC Shallow	SF
Bumps and Sags	Medium	Patching - AC Shallow	SF
Bumps and Sags	High	Patching - AC Deep	SF
Corrugation	Medium	Patching - AC Shallow	SF
Corrugation	High	Patching - AC Deep	SF
Depressions	Medium	Patching - AC Deep	SF
Depressions	High	Patching - AC Deep	SF
Edge Cracking	Low	Crack Sealing - AC	FT
Edge Cracking	Medium	Crack Sealing - AC	FT
Edge Cracking	High	Patching - AC Shallow	SF
Joint Reflection Cracking	Low	Crack Sealing - AC	FT
Joint Reflection Cracking	Medium	Crack Sealing - AC	FT
Joint Reflection Cracking	High	Patching - AC Shallow	SF
Lane/Shoulder Dropoff	Medium	Shoulder leveling	FT
Lane/Shoulder Dropoff	High	Shoulder leveling	FT
Long. and Trans. Cracking	Low	Crack Sealing - AC	FT
Long. and Trans. Cracking	Medium	Crack Sealing - AC	FT
Long. and Trans. Cracking	High	Patching - AC Shallow	SF
Patching and Utility Cuts	High	Patching - AC Deep	SF
Potholes	Low	Patching - AC Deep	SF
Potholes	Medium	Patching - AC Deep	SF
Potholes	High	Patching - AC Deep	SF
Rutting	Medium	Patching - AC Shallow	SF
Rutting	High	Patching - AC Deep	SF
Shoving	Medium	Grinding (Localized)	FT
Shoving	High	Grinding (Localized)	FT
Slippage Cracking	Low	Crack Sealing - AC	FT
Slippage Cracking	Medium	Patching - AC Shallow	SF
Slippage Cracking	High	Patching - AC Shallow	SF

Table D-2. Recommended Concrete Pavement Maintenance Policy.

Pavement Distress	Severity	Recommended Maintenance Type	Units
Blow ups	Medium	Patching - PCC Full Depth	SF
Blow ups	High	Patching - PCC Full Depth	SF
Corner Breaks	Low	Crack Sealing - PCC	FT
Corner Breaks	Medium	Patching - PCC Full Depth	FT
Corner Breaks	High	Patching - PCC Full Depth	SF
Divided (Shattered) Slabs	Low	Crack Sealing - PCC	FT
Divided (Shattered) Slabs	Medium	Slab Replacement - PCC	SF
Divided (Shattered) Slabs	High	Slab Replacement - PCC	SF
Durability (D) Cracking	Medium	Patching - PCC Full Depth	SF
Durability (D) Cracking	High	Slab Replacement - PCC	SF
Faulting	Medium	Grinding (Localized)	FT
Faulting	High	Grinding (Localized)	FT
Joint Seal Damage	Medium	Joint Seal (Localized)	FT
Joint Seal Damage	High	Joint Seal (Localized)	FT
Lane/Shoulder Dropoff	Medium	Shoulder leveling	FT
Lane/Shoulder Dropoff	High	Shoulder leveling	FT
Linear Cracking	Low	Crack Sealing - PCC	FT
Linear Cracking	Medium	Crack Sealing - PCC	FT
Linear Cracking	High	Patching - PCC Partial Depth	SF
Patches, Large	High	Patching - PCC Full Depth	SF
Patches, Small	High	Patching - PCC Partial Depth	SF
Punchouts	Medium	Patching - PCC Full Depth	SF
Punchouts	High	Slab Replacement - PCC	SF
Sealing	High	Slab Replacement - PCC	SF
Corner Spalls	Medium	Patching - PCC Partial Depth	SF
Corner Spalls	High	Patching - PCC Partial Depth	SF
Joint Spalls	Medium	Patching - PCC Partial Depth	SF
Joint Spalls	High	Patching - PCC Partial Depth	SF

Table D-3. Estimate Unit Cost for Maintenance Activities.

Maintenance Type	Est. Unit Cost	Units
Crack Sealing - AC	\$1.00	FT
Joint Seal - Silicon	\$2.75	FT
Crack Sealing - PCC	\$1.50	FT
Grinding (Localized)	\$4.00	FT
Joint Seal (Localized)	\$1.50	FT
Patching - AC Deep	\$11.00	SF
Patching - AC Leveling	\$1.20	SF
Patching - AC Shallow	\$5.50	SF
Patching - PCC Full Depth	\$30.00	SF
Patching - PCC Partial Depth	\$7.00	SF
Shoulder leveling	\$1.20	FT
Slab Replacement - PCC	\$20.00	SF

APPENDIX E – TABULATED PREVENTIVE MAINTENANCE RECOMMENDATIONS

Pavement ID	Road Name	From	To	Area	Distress Type	Density	Maint. Activity	Cost
LNSG::172ND ST::10	172ND STREET	TORRENCE AVENUE	BENSLEY AVENUE	26,975	ALLIGATOR CR	0.3%	Crack Sealing - AC	\$39
LNSG::172ND ST::10	172ND STREET	TORRENCE AVENUE	BENSLEY AVENUE	26,975	L & T CR	0.3%	Crack Sealing - AC	\$69
LNSG::172ND ST::10	172ND STREET	TORRENCE AVENUE	BENSLEY AVENUE	26,975	L & T CR	1.4%	Crack Sealing - AC	\$389
LNSG::172ND ST::10	172ND STREET	TORRENCE AVENUE	BENSLEY AVENUE	26,975	ALLIGATOR CR	1.7%	Patching - AC Deep	\$6,118
LNSG::172ND ST::10	172ND STREET	TORRENCE AVENUE	BENSLEY AVENUE	26,975	RUTTING	0.0%	Patching - AC Shallow	\$37
LNSG::172ND ST::20	172ND STREET	TORRENCE AVENUE	OAK AVENUE	28,372	ALLIGATOR CR	0.2%	Crack Sealing - AC	\$24
LNSG::172ND ST::20	172ND STREET	TORRENCE AVENUE	OAK AVENUE	28,372	L & T CR	0.3%	Crack Sealing - AC	\$95
LNSG::172ND ST::80	172ND STREET	LORENZ AVENUE	GREENBAY AVENUE	9,302	BLOCK CR	46.1%	Crack Sealing - AC	\$1,307
LNSG::173RD PL::10	173RD PLACE	OAKWOOD AVENUE	WEST LAKE ROAD	9,928	L & T CR	2.8%	Crack Sealing - AC	\$275
LNSG::173RD PL::10	173RD PLACE	OAKWOOD AVENUE	WEST LAKE ROAD	9,928	BLOCK CR	18.5%	Crack Sealing - AC	\$558
LNSG::173RD PL::10	173RD PLACE	OAKWOOD AVENUE	WEST LAKE ROAD	9,928	ALLIGATOR CR	0.3%	Patching - AC Deep	\$555
LNSG::173RD ST::10	173RD STREET	BERNICE ROAD	BENSLEY AVENUE	18,282	EDGE CR	0.2%	Crack Sealing - AC	\$39
LNSG::173RD ST::10	173RD STREET	BERNICE ROAD	BENSLEY AVENUE	18,282	L & T CR	0.4%	Crack Sealing - AC	\$72
LNSG::173RD ST::10	173RD STREET	BERNICE ROAD	BENSLEY AVENUE	18,282	ALLIGATOR CR	0.1%	Crack Sealing - AC	\$13
LNSG::173RD ST::10	173RD STREET	BERNICE ROAD	BENSLEY AVENUE	18,282	ALLIGATOR CR	0.2%	Patching - AC Deep	\$678
LNSG::173RD ST::10	173RD STREET	BERNICE ROAD	BENSLEY AVENUE	18,282	RUTTING	0.0%	Patching - AC Shallow	\$41
LNSG::173RD ST::30	173RD STREET	TORRENCE AVENUE	OAK AVENUE	28,652	L & T CR	0.6%	Crack Sealing - AC	\$158
LNSG::173RD ST::30	173RD STREET	TORRENCE AVENUE	OAK AVENUE	28,652	L & T CR	0.1%	Crack Sealing - AC	\$23
LNSG::175TH ST::10	175TH STREET	176TH STREET	CHICAGO AVENUE	91,808	ALLIGATOR CR	0.3%	Crack Sealing - AC	\$99
LNSG::175TH ST::10	175TH STREET	176TH STREET	CHICAGO AVENUE	91,808	L & T CR	0.7%	Crack Sealing - AC	\$648
LNSG::175TH ST::10	175TH STREET	176TH STREET	CHICAGO AVENUE	91,808	L & T CR	1.3%	Crack Sealing - AC	\$1,185
LNSG::175TH ST::10	175TH STREET	176TH STREET	CHICAGO AVENUE	91,808	ALLIGATOR CR	0.1%	Patching - AC Deep	\$1,440
LNSG::175TH ST::10	175TH STREET	176TH STREET	CHICAGO AVENUE	91,808	RUTTING	0.0%	Patching - AC Shallow	\$97
LNSG::175TH ST::20	175TH STREET	CHICAGO AVENUE	LORENZ AVENUE	17,073	L & T CR	0.3%	Crack Sealing - AC	\$58
LNSG::175TH ST::20	175TH STREET	CHICAGO AVENUE	LORENZ AVENUE	17,073	L & T CR	0.7%	Crack Sealing - AC	\$118
LNSG::175TH ST::20	175TH STREET	CHICAGO AVENUE	LORENZ AVENUE	17,073	ALLIGATOR CR	0.5%	Patching - AC Deep	\$1,380
LNSG::175TH ST::20	175TH STREET	CHICAGO AVENUE	LORENZ AVENUE	17,073	RUTTING	0.1%	Patching - AC Shallow	\$43
LNSG::175TH ST::30	175TH STREET	LORENZ AVENUE	GREENBAY AVENUE	9,911	RUTTING	0.1%	Patching - AC Shallow	\$31
LNSG::175TH ST::40	175TH STREET	GREENBAY AVENUE	BURNHAM AVENUE	10,137	ALLIGATOR CR	0.1%	Crack Sealing - AC	\$7
LNSG::175TH ST::40	175TH STREET	GREENBAY AVENUE	BURNHAM AVENUE	10,137	L & T CR	0.2%	Crack Sealing - AC	\$19
LNSG::175TH ST::40	175TH STREET	GREENBAY AVENUE	BURNHAM AVENUE	10,137	L & T CR	0.4%	Crack Sealing - AC	\$39
LNSG::175TH ST::40	175TH STREET	GREENBAY AVENUE	BURNHAM AVENUE	10,137	ALLIGATOR CR	0.5%	Patching - AC Deep	\$916
LNSG::175TH ST::50	175TH STREET	BURNHAM AVENUE	WALTER STREET	9,507	ALLIGATOR CR	1.5%	Crack Sealing - AC	\$58
LNSG::175TH ST::50	175TH STREET	BURNHAM AVENUE	WALTER STREET	9,507	L & T CR	4.9%	Crack Sealing - AC	\$462
LNSG::175TH ST::50	175TH STREET	BURNHAM AVENUE	WALTER STREET	9,507	L & T CR	0.2%	Crack Sealing - AC	\$19
LNSG::175TH ST::50	175TH STREET	BURNHAM AVENUE	WALTER STREET	9,507	ALLIGATOR CR	1.7%	Patching - AC Deep	\$2,384
LNSG::175TH ST::90	175TH STREET	HENRY STREET	WRIGHT STREET	9,333	L & T CR	5.2%	Crack Sealing - AC	\$481
LNSG::175TH ST::100	175TH STREET	WRIGHT STREET	WILLIAM STREET	9,139	L & T CR	3.2%	Crack Sealing - AC	\$291
LNSG::175TH ST::100	175TH STREET	WRIGHT STREET	WILLIAM STREET	9,139	L & T CR	0.6%	Crack Sealing - AC	\$57
LNSG::175TH ST::100	175TH STREET	WRIGHT STREET	WILLIAM STREET	9,139	ALLIGATOR CR	0.1%	Patching - AC Deep	\$236
LNSG::175TH ST::110	175TH STREET	WILLIAM STREET	BERNADINE STREET	9,229	L & T CR	1.0%	Crack Sealing - AC	\$94

Pavement ID	Road Name	From	To	Area	Distress Type	Density	Maint. Activity	Cost
LNSG::175TH ST::110	175TH STREET	WILLIAM STREET	BERNADINE STREET	9,229	L & T CR	1.7%	Crack Sealing - AC	\$158
LNSG::175TH ST::110	175TH STREET	WILLIAM STREET	BERNADINE STREET	9,229	ALLIGATOR CR	0.1%	Patching - AC Deep	\$247
LNSG::175TH ST::110	175TH STREET	WILLIAM STREET	BERNADINE STREET	9,229	RUTTING	0.1%	Patching - AC Shallow	\$27
LNSG::176TH PL::50	176TH PLACE	HOLLIDAY TERRACE	TORRENCE AVENUE	18,559	ALLIGATOR CR	0.8%	Crack Sealing - AC	\$58
LNSG::176TH PL::50	176TH PLACE	HOLLIDAY TERRACE	TORRENCE AVENUE	18,559	L & T CR	0.7%	Crack Sealing - AC	\$133
LNSG::176TH PL::50	176TH PLACE	HOLLIDAY TERRACE	TORRENCE AVENUE	18,559	L & T CR	1.2%	Crack Sealing - AC	\$229
LNSG::176TH PL::50	176TH PLACE	HOLLIDAY TERRACE	TORRENCE AVENUE	18,559	ALLIGATOR CR	0.3%	Patching - AC Deep	\$890
LNSG::176TH PL::50	176TH PLACE	HOLLIDAY TERRACE	TORRENCE AVENUE	18,559	RUTTING	0.4%	Patching - AC Shallow	\$424
LNSG::176TH ST::20	176TH STREET	LORENZ AVENUE	GREENBAY AVENUE	10,055	L & T CR	2.2%	Crack Sealing - AC	\$222
LNSG::176TH ST::20	176TH STREET	LORENZ AVENUE	GREENBAY AVENUE	10,055	L & T CR	2.1%	Crack Sealing - AC	\$216
LNSG::176TH ST::30	176TH STREET	GREENBAY AVENUE	BURNHAM AVENUE	10,203	L & T CR	2.7%	Crack Sealing - AC	\$275
LNSG::176TH ST::30	176TH STREET	GREENBAY AVENUE	BURNHAM AVENUE	10,203	L & T CR	3.2%	Crack Sealing - AC	\$324
LNSG::177TH ST::10	177TH STREET	CHAPPEL AVENUE	PAXTON AVENUE	26,552	L & T CR	0.5%	Crack Sealing - AC	\$138
LNSG::177TH ST::10	177TH STREET	CHAPPEL AVENUE	PAXTON AVENUE	26,552	ALLIGATOR CR	0.3%	Crack Sealing - AC	\$41
LNSG::177TH ST::10	177TH STREET	CHAPPEL AVENUE	PAXTON AVENUE	26,552	L & T CR	1.9%	Crack Sealing - AC	\$503
LNSG::177TH ST::10	177TH STREET	CHAPPEL AVENUE	PAXTON AVENUE	26,552	RUTTING	0.1%	Patching - AC Shallow	\$93
LNSG::177TH ST::40	177TH STREET	OAKLEY AVENUE	ESCANABA AVENUE	9,537	ALLIGATOR CR	0.7%	Crack Sealing - AC	\$31
LNSG::177TH ST::40	177TH STREET	OAKLEY AVENUE	ESCANABA AVENUE	9,537	L & T CR	0.7%	Crack Sealing - AC	\$66
LNSG::177TH ST::50	177TH STREET	ESCANABA AVENUE	EXCHANGE AVENUE	9,243	L & T CR	0.8%	Crack Sealing - AC	\$78
LNSG::177TH ST::50	177TH STREET	ESCANABA AVENUE	EXCHANGE AVENUE	9,243	EDGE CR	1.2%	Crack Sealing - AC	\$108
LNSG::177TH ST::50	177TH STREET	ESCANABA AVENUE	EXCHANGE AVENUE	9,243	ALLIGATOR CR	0.1%	Crack Sealing - AC	\$9
LNSG::177TH ST::50	177TH STREET	ESCANABA AVENUE	EXCHANGE AVENUE	9,243	ALLIGATOR CR	3.0%	Patching - AC Deep	\$3,834
LNSG::177TH ST::50	177TH STREET	ESCANABA AVENUE	EXCHANGE AVENUE	9,243	RUTTING	0.7%	Patching - AC Shallow	\$330
LNSG::177TH ST::80	177TH STREET	BURNHAM AVENUE	WALTER STREET	9,312	BLOCK CR	13.3%	Crack Sealing - AC	\$378
LNSG::177TH ST::80	177TH STREET	BURNHAM AVENUE	WALTER STREET	9,312	L & T CR	1.3%	Crack Sealing - AC	\$117
LNSG::177TH ST::150	177TH STREET	WENTWORTH AVENUE	MAPLE STREET	24,065	BLOCK CR	5.9%	Crack Sealing - AC	\$430
LNSG::177TH ST::150	177TH STREET	WENTWORTH AVENUE	MAPLE STREET	24,065	L & T CR	2.2%	Crack Sealing - AC	\$526
LNSG::177TH ST::150	177TH STREET	WENTWORTH AVENUE	MAPLE STREET	24,065	L & T CR	0.1%	Crack Sealing - AC	\$21
LNSG::178TH PL::10	178TH PLACE	WALTER STREET	END	6,403	L & T CR	0.4%	Crack Sealing - AC	\$22
LNSG::178TH PL::10	178TH PLACE	WALTER STREET	END	6,403	L & T CR	1.0%	Crack Sealing - AC	\$65
LNSG::178TH ST::10	178TH STREET	LOCUST STREET	TORRENCE AVENUE	36,338	ALLIGATOR CR	0.7%	Crack Sealing - AC	\$93
LNSG::178TH ST::10	178TH STREET	LOCUST STREET	TORRENCE AVENUE	36,338	L & T CR	1.2%	Crack Sealing - AC	\$417
LNSG::178TH ST::10	178TH STREET	LOCUST STREET	TORRENCE AVENUE	36,338	RUTTING	0.0%	Patching - AC Shallow	\$51
LNSG::178TH ST::40	178TH STREET	ARCADIA AVENUE	OAKLEY AVENUE	18,659	L & T CR	2.3%	Crack Sealing - AC	\$422
LNSG::178TH ST::40	178TH STREET	ARCADIA AVENUE	OAKLEY AVENUE	18,659	RUTTING	0.0%	Patching - AC Shallow	\$18
LNSG::178TH ST::50	178TH STREET	OAKLEY AVENUE	ESCANABA AVENUE	9,541	L & T CR	1.8%	Crack Sealing - AC	\$172
LNSG::178TH ST::60	178TH STREET	ESCANABA AVENUE	EXCHANGE AVENUE	9,261	L & T CR	1.2%	Crack Sealing - AC	\$113
LNSG::178TH ST::60	178TH STREET	ESCANABA AVENUE	EXCHANGE AVENUE	9,261	L & T CR	1.4%	Crack Sealing - AC	\$133
LNSG::178TH ST::60	178TH STREET	ESCANABA AVENUE	EXCHANGE AVENUE	9,261	RUTTING	0.0%	Patching - AC Shallow	\$17
LNSG::178TH ST::70	178TH STREET	EXCHANGE AVENUE	COMMERCIAL STREET	9,304	L & T CR	1.0%	Crack Sealing - AC	\$95
LNSG::178TH ST::70	178TH STREET	EXCHANGE AVENUE	COMMERCIAL STREET	9,304	L & T CR	1.9%	Crack Sealing - AC	\$172

Pavement ID	Road Name	From	To	Area	Distress Type	Density	Maint. Activity	Cost
LNSG::178TH ST::70	178TH STREET	EXCHANGE AVENUE	COMMERCIAL STREET	9,304	ALLIGATOR CR	0.6%	Patching - AC Deep	\$959
LNSG::178TH ST::80	178TH STREET	COMMERCIAL STREET	SCHOOL STREET	9,109	ALLIGATOR CR	0.2%	Crack Sealing - AC	\$11
LNSG::178TH ST::80	178TH STREET	COMMERCIAL STREET	SCHOOL STREET	9,109	L & T CR	1.1%	Crack Sealing - AC	\$96
LNSG::178TH ST::80	178TH STREET	COMMERCIAL STREET	SCHOOL STREET	9,109	L & T CR	0.8%	Crack Sealing - AC	\$77
LNSG::178TH ST::90	178TH STREET	SCHOOL STREET	WILDWOOD AVENUE	9,513	L & T CR	0.8%	Crack Sealing - AC	\$75
LNSG::178TH ST::90	178TH STREET	SCHOOL STREET	WILDWOOD AVENUE	9,513	L & T CR	1.8%	Crack Sealing - AC	\$170
LNSG::178TH ST::90	178TH STREET	SCHOOL STREET	WILDWOOD AVENUE	9,513	ALLIGATOR CR	2.0%	Patching - AC Deep	\$2,737
LNSG::178TH ST::100	178TH STREET	WILDWOOD AVENUE	OAKWOOD AVENUE	9,355	L & T CR	1.2%	Crack Sealing - AC	\$113
LNSG::178TH ST::100	178TH STREET	WILDWOOD AVENUE	OAKWOOD AVENUE	9,355	L & T CR	1.2%	Crack Sealing - AC	\$113
LNSG::178TH ST::100	178TH STREET	WILDWOOD AVENUE	OAKWOOD AVENUE	9,355	ALLIGATOR CR	0.8%	Patching - AC Deep	\$1,261
LNSG::178TH ST::100	178TH STREET	WILDWOOD AVENUE	OAKWOOD AVENUE	9,355	RUTTING	0.0%	Patching - AC Shallow	\$17
LNSG::178TH ST::110	178TH STREET	OAKWOOD AVENUE	RIDGEWOOD AVENUE	9,251	ALLIGATOR CR	0.2%	Crack Sealing - AC	\$14
LNSG::178TH ST::110	178TH STREET	OAKWOOD AVENUE	RIDGEWOOD AVENUE	9,251	L & T CR	0.4%	Crack Sealing - AC	\$39
LNSG::178TH ST::110	178TH STREET	OAKWOOD AVENUE	RIDGEWOOD AVENUE	9,251	L & T CR	2.9%	Crack Sealing - AC	\$264
LNSG::178TH ST::120	178TH STREET	RIDGEWOOD AVENUE	CHICAGO AVENUE	9,309	L & T CR	2.0%	Crack Sealing - AC	\$189
LNSG::178TH ST::190	178TH STREET	ROY STREET	COMMUNITY STREET	9,394	L & T CR	1.7%	Crack Sealing - AC	\$155
LNSG::178TH ST::190	178TH STREET	ROY STREET	COMMUNITY STREET	9,394	L & T CR	0.8%	Crack Sealing - AC	\$78
LNSG::178TH ST::190	178TH STREET	ROY STREET	COMMUNITY STREET	9,394	ALLIGATOR CR	2.0%	Crack Sealing - AC	\$76
LNSG::178TH ST::190	178TH STREET	ROY STREET	COMMUNITY STREET	9,394	ALLIGATOR CR	0.7%	Patching - AC Deep	\$1,127
LNSG::178TH ST::220	178TH STREET	WILLIAM STREET	BERNADINE STREET	9,614	L & T CR	1.0%	Crack Sealing - AC	\$97
LNSG::178TH ST::220	178TH STREET	WILLIAM STREET	BERNADINE STREET	9,614	L & T CR	0.2%	Crack Sealing - AC	\$19
LNSG::178TH ST::220	178TH STREET	WILLIAM STREET	BERNADINE STREET	9,614	ALLIGATOR CR	1.6%	Crack Sealing - AC	\$64
LNSG::178TH ST::220	178TH STREET	WILLIAM STREET	BERNADINE STREET	9,614	ALLIGATOR CR	1.2%	Patching - AC Deep	\$1,828
LNSG::178TH ST::230	178TH STREET	BERNADINE STREET	WENTWORTH AVENUE	9,419	L & T CR	1.0%	Crack Sealing - AC	\$97
LNSG::178TH ST::230	178TH STREET	BERNADINE STREET	WENTWORTH AVENUE	9,419	ALLIGATOR CR	0.5%	Crack Sealing - AC	\$22
LNSG::178TH ST::230	178TH STREET	BERNADINE STREET	WENTWORTH AVENUE	9,419	ALLIGATOR CR	2.3%	Patching - AC Deep	\$3,066
LNSG::178TH ST::230	178TH STREET	BERNADINE STREET	WENTWORTH AVENUE	9,419	ALLIGATOR CR	0.3%	Patching - AC Deep	\$547
LNSG::179TH ST::30	179TH STREET	TORRENCE AVENUE	GLEN OAK AVENUE	9,315	ALLIGATOR CR	1.4%	Crack Sealing - AC	\$54
LNSG::179TH ST::30	179TH STREET	TORRENCE AVENUE	GLEN OAK AVENUE	9,315	L & T CR	1.4%	Crack Sealing - AC	\$133
LNSG::179TH ST::30	179TH STREET	TORRENCE AVENUE	GLEN OAK AVENUE	9,315	ALLIGATOR CR	2.8%	Patching - AC Deep	\$3,614
LNSG::179TH ST::40	179TH STREET	GLEN OAK AVENUE	ARCADIA AVENUE	9,418	L & T CR	0.2%	Crack Sealing - AC	\$22
LNSG::179TH ST::40	179TH STREET	GLEN OAK AVENUE	ARCADIA AVENUE	9,418	L & T CR	1.2%	Crack Sealing - AC	\$109
LNSG::179TH ST::40	179TH STREET	GLEN OAK AVENUE	ARCADIA AVENUE	9,418	ALLIGATOR CR	2.1%	Crack Sealing - AC	\$80
LNSG::179TH ST::40	179TH STREET	GLEN OAK AVENUE	ARCADIA AVENUE	9,418	ALLIGATOR CR	0.4%	Patching - AC Deep	\$739
LNSG::179TH ST::50	179TH STREET	ESCANABA AVENUE	EXCHANGE AVENUE	9,280	ALLIGATOR CR	4.7%	Crack Sealing - AC	\$160
LNSG::179TH ST::50	179TH STREET	ESCANABA AVENUE	EXCHANGE AVENUE	9,280	L & T CR	0.4%	Crack Sealing - AC	\$32
LNSG::179TH ST::50	179TH STREET	ESCANABA AVENUE	EXCHANGE AVENUE	9,280	L & T CR	3.4%	Crack Sealing - AC	\$319
LNSG::179TH ST::70	179TH STREET	COMMERCIAL STREET	SCHOOL STREET	9,185	ALLIGATOR CR	1.8%	Crack Sealing - AC	\$67
LNSG::179TH ST::70	179TH STREET	COMMERCIAL STREET	SCHOOL STREET	9,185	L & T CR	0.8%	Crack Sealing - AC	\$76
LNSG::179TH ST::70	179TH STREET	COMMERCIAL STREET	SCHOOL STREET	9,185	ALLIGATOR CR	0.4%	Patching - AC Deep	\$743
LNSG::179TH ST::140	179TH STREET	LORENZ AVENUE	END	8,391	L & T CR	1.4%	Crack Sealing - AC	\$116

Pavement ID	Road Name	From	To	Area	Distress Type	Density	Maint. Activity	Cost
LNSG::179TH ST::140	179TH STREET	LORENZ AVENUE	END	8,391	ALLIGATOR CR	0.4%	Crack Sealing - AC	\$19
LNSG::179TH ST::140	179TH STREET	LORENZ AVENUE	END	8,391	ALLIGATOR CR	0.5%	Patching - AC Deep	\$727
LNSG::180TH PL::20	180TH PLACE	WHITMAN LANE	CLYDE AVENUE	12,314	ALLIGATOR CR	0.4%	Crack Sealing - AC	\$25
LNSG::180TH PL::20	180TH PLACE	WHITMAN LANE	CLYDE AVENUE	12,314	L & T CR	0.5%	Crack Sealing - AC	\$63
LNSG::180TH PL::20	180TH PLACE	WHITMAN LANE	CLYDE AVENUE	12,314	BLOCK CR	46.2%	Crack Sealing - AC	\$1,734
LNSG::180TH PL::20	180TH PLACE	WHITMAN LANE	CLYDE AVENUE	12,314	L & T CR	0.7%	Crack Sealing - AC	\$84
LNSG::180TH PL::30	180TH PLACE	CLYDE AVENUE	PAXTON AVENUE	13,941	L & T CR	0.3%	Crack Sealing - AC	\$38
LNSG::180TH PL::30	180TH PLACE	CLYDE AVENUE	PAXTON AVENUE	13,941	ALLIGATOR CR	0.2%	Patching - AC Deep	\$480
LNSG::180TH ST::20	180TH STREET	CHARLOTTE DRIVE	STONY ISLAND AVENUE	5,062	ALLIGATOR CR	0.8%	Crack Sealing - AC	\$21
LNSG::180TH ST::20	180TH STREET	CHARLOTTE DRIVE	STONY ISLAND AVENUE	5,062	L & T CR	2.1%	Crack Sealing - AC	\$104
LNSG::180TH ST::20	180TH STREET	CHARLOTTE DRIVE	STONY ISLAND AVENUE	5,062	L & T CR	0.4%	Crack Sealing - AC	\$18
LNSG::180TH ST::30	180TH STREET	STONY ISLAND AVENUE	BRITTANY LANE	5,889	L & T CR	0.4%	Crack Sealing - AC	\$20
LNSG::180TH ST::30	180TH STREET	STONY ISLAND AVENUE	BRITTANY LANE	5,889	L & T CR	1.4%	Crack Sealing - AC	\$80
LNSG::180TH ST::40	180TH STREET	BRITTANY LANE	CRYSTAL LANE	8,488	L & T CR	1.8%	Crack Sealing - AC	\$155
LNSG::180TH ST::40	180TH STREET	BRITTANY LANE	CRYSTAL LANE	8,488	L & T CR	0.5%	Crack Sealing - AC	\$39
LNSG::180TH ST::150	180TH STREET	OAKWOOD AVENUE	RIDGEWOOD AVENUE	9,335	L & T CR	0.6%	Crack Sealing - AC	\$58
LNSG::180TH ST::150	180TH STREET	OAKWOOD AVENUE	RIDGEWOOD AVENUE	9,335	L & T CR	0.6%	Crack Sealing - AC	\$57
LNSG::180TH ST::150	180TH STREET	OAKWOOD AVENUE	RIDGEWOOD AVENUE	9,335	ALLIGATOR CR	1.9%	Crack Sealing - AC	\$71
LNSG::180TH ST::150	180TH STREET	OAKWOOD AVENUE	RIDGEWOOD AVENUE	9,335	ALLIGATOR CR	2.4%	Patching - AC Deep	\$3,144
LNSG::180TH ST::160	180TH STREET	RIDGEWOOD AVENUE	CHICAGO AVENUE	9,247	ALLIGATOR CR	0.4%	Crack Sealing - AC	\$19
LNSG::180TH ST::160	180TH STREET	RIDGEWOOD AVENUE	CHICAGO AVENUE	9,247	L & T CR	2.5%	Crack Sealing - AC	\$230
LNSG::180TH ST::160	180TH STREET	RIDGEWOOD AVENUE	CHICAGO AVENUE	9,247	ALLIGATOR CR	0.2%	Patching - AC Deep	\$452
LNSG::180TH ST::160	180TH STREET	RIDGEWOOD AVENUE	CHICAGO AVENUE	9,247	RUTTING	0.1%	Patching - AC Shallow	\$31
LNSG::180TH ST::170	180TH STREET	CHICAGO AVENUE	PARK AVENUE	9,398	L & T CR	2.5%	Crack Sealing - AC	\$230
LNSG::180TH ST::170	180TH STREET	CHICAGO AVENUE	PARK AVENUE	9,398	ALLIGATOR CR	0.3%	Crack Sealing - AC	\$15
LNSG::180TH ST::170	180TH STREET	CHICAGO AVENUE	PARK AVENUE	9,398	ALLIGATOR CR	3.3%	Patching - AC Deep	\$4,283
LNSG::180TH ST::180	180TH STREET	PARK AVENUE	LORENZ AVENUE	9,139	ALLIGATOR CR	1.8%	Crack Sealing - AC	\$67
LNSG::180TH ST::180	180TH STREET	PARK AVENUE	LORENZ AVENUE	9,139	L & T CR	1.3%	Crack Sealing - AC	\$114
LNSG::180TH ST::180	180TH STREET	PARK AVENUE	LORENZ AVENUE	9,139	L & T CR	0.8%	Crack Sealing - AC	\$75
LNSG::180TH ST::180	180TH STREET	PARK AVENUE	LORENZ AVENUE	9,139	ALLIGATOR CR	0.5%	Patching - AC Deep	\$859
LNSG::180TH ST::190	180TH STREET	LORENZ AVENUE	BURNHAM AVENUE	18,751	BLOCK CR	24.6%	Crack Sealing - AC	\$1,408
LNSG::180TH ST::190	180TH STREET	LORENZ AVENUE	BURNHAM AVENUE	18,751	L & T CR	1.0%	Crack Sealing - AC	\$192
LNSG::180TH ST::190	180TH STREET	LORENZ AVENUE	BURNHAM AVENUE	18,751	ALLIGATOR CR	1.5%	Patching - AC Deep	\$3,895
LNSG::180TH ST::190	180TH STREET	LORENZ AVENUE	BURNHAM AVENUE	18,751	RUTTING	0.1%	Patching - AC Shallow	\$63
LNSG::181ST PL::10	181ST PLACE	DONATUS DRIVE	HARPER STREET	3,298	L & T CR	0.4%	Crack Sealing - AC	\$11
LNSG::181ST PL::10	181ST PLACE	DONATUS DRIVE	HARPER STREET	3,298	L & T CR	2.0%	Crack Sealing - AC	\$67
LNSG::181ST PL::10	181ST PLACE	DONATUS DRIVE	HARPER STREET	3,298	ALLIGATOR CR	0.4%	Crack Sealing - AC	\$10
LNSG::181ST PL::20	181ST PLACE	HARPER STREET	CHARLOTTE DRIVE	4,171	L & T CR	0.7%	Crack Sealing - AC	\$29
LNSG::181ST PL::20	181ST PLACE	HARPER STREET	CHARLOTTE DRIVE	4,171	L & T CR	0.4%	Crack Sealing - AC	\$14
LNSG::181ST PL::20	181ST PLACE	HARPER STREET	CHARLOTTE DRIVE	4,171	ALLIGATOR CR	0.2%	Patching - AC Deep	\$290
LNSG::181ST PL::30	181ST PLACE	ROY STREET	RANDOLPH STREET	7,982	L & T CR	1.0%	Crack Sealing - AC	\$83

Pavement ID	Road Name	From	To	Area	Distress Type	Density	Maint. Activity	Cost
LNSG::181ST ST::10	181ST STREET	DORCHESTER AVENUE	DONATUS DRIVE	4,899	ALLIGATOR CR	0.3%	Crack Sealing - AC	\$11
LNSG::181ST ST::10	181ST STREET	DORCHESTER AVENUE	DONATUS DRIVE	4,899	L & T CR	0.7%	Crack Sealing - AC	\$34
LNSG::181ST ST::10	181ST STREET	DORCHESTER AVENUE	DONATUS DRIVE	4,899	ALLIGATOR CR	1.2%	Patching - AC Deep	\$1,009
LNSG::181ST ST::50	181ST STREET	EXCHANGE AVENUE	COMMERCIAL STREET	9,273	L & T CR	1.9%	Crack Sealing - AC	\$172
LNSG::181ST ST::50	181ST STREET	EXCHANGE AVENUE	COMMERCIAL STREET	9,273	ALLIGATOR CR	0.4%	Crack Sealing - AC	\$20
LNSG::181ST ST::50	181ST STREET	EXCHANGE AVENUE	COMMERCIAL STREET	9,273	ALLIGATOR CR	1.5%	Patching - AC Deep	\$2,106
LNSG::181ST ST::50	181ST STREET	EXCHANGE AVENUE	COMMERCIAL STREET	9,273	RUTTING	0.4%	Patching - AC Shallow	\$207
LNSG::181ST ST::60	181ST STREET	COMMERCIAL STREET	SCHOOL STREET	9,458	L & T CR	0.7%	Crack Sealing - AC	\$64
LNSG::181ST ST::60	181ST STREET	COMMERCIAL STREET	SCHOOL STREET	9,458	L & T CR	2.7%	Crack Sealing - AC	\$258
LNSG::181ST ST::60	181ST STREET	COMMERCIAL STREET	SCHOOL STREET	9,458	ALLIGATOR CR	0.4%	Crack Sealing - AC	\$21
LNSG::181ST ST::120	181ST STREET	PARK AVENUE	LORENZ AVENUE	9,190	L & T CR	2.7%	Crack Sealing - AC	\$248
LNSG::181ST ST::130	181ST STREET	LORENZ AVENUE	BURNHAM AVENUE	18,698	L & T CR	0.7%	Crack Sealing - AC	\$126
LNSG::181ST ST::130	181ST STREET	LORENZ AVENUE	BURNHAM AVENUE	18,698	L & T CR	1.8%	Crack Sealing - AC	\$338
LNSG::181ST ST::130	181ST STREET	LORENZ AVENUE	BURNHAM AVENUE	18,698	ALLIGATOR CR	0.1%	Patching - AC Deep	\$517
LNSG::182ND PL::50	182ND PLACE	OAKLEY AVENUE	END	7,116	L & T CR	0.7%	Crack Sealing - AC	\$49
LNSG::182ND PL::60	182ND PLACE	CHICAGO AVENUE	OAK STREET	11,758	L & T CR	0.8%	Crack Sealing - AC	\$88
LNSG::182ND PL::60	182ND PLACE	CHICAGO AVENUE	OAK STREET	11,758	ALLIGATOR CR	3.9%	Patching - AC Deep	\$6,032
LNSG::182ND PL::60	182ND PLACE	CHICAGO AVENUE	OAK STREET	11,758	RUTTING	0.0%	Patching - AC Shallow	\$27
LNSG::182ND ST::10	182ND STREET	START	JASON LANE	11,873	L & T CR	1.5%	Crack Sealing - AC	\$183
LNSG::182ND ST::10	182ND STREET	START	JASON LANE	11,873	ALLIGATOR CR	0.1%	Crack Sealing - AC	\$11
LNSG::182ND ST::10	182ND STREET	START	JASON LANE	11,873	ALLIGATOR CR	1.4%	Patching - AC Deep	\$2,464
LNSG::182ND ST::10	182ND STREET	START	JASON LANE	11,873	RUTTING	0.1%	Patching - AC Shallow	\$50
LNSG::182ND ST::20	182ND STREET	JASON LANE	CLYDE AVENUE	17,704	L & T CR	2.8%	Crack Sealing - AC	\$500
LNSG::182ND ST::20	182ND STREET	JASON LANE	CLYDE AVENUE	17,704	L & T CR	0.5%	Crack Sealing - AC	\$81
LNSG::182ND ST::30	182ND STREET	CLYDE AVENUE	PAXTON AVENUE	12,029	L & T CR	2.9%	Crack Sealing - AC	\$347
LNSG::182ND ST::30	182ND STREET	CLYDE AVENUE	PAXTON AVENUE	12,029	ALLIGATOR CR	0.2%	Crack Sealing - AC	\$12
LNSG::182ND ST::40	182ND STREET	PAXTON AVENUE	OLDE FARM ROAD	8,355	L & T CR	3.4%	Crack Sealing - AC	\$287
LNSG::182ND ST::50	182ND STREET	OLDE FARM ROAD	LOCUST STREET	30,882	L & T CR	1.1%	Crack Sealing - AC	\$344
LNSG::182ND ST::50	182ND STREET	OLDE FARM ROAD	LOCUST STREET	30,882	L & T CR	0.4%	Crack Sealing - AC	\$115
LNSG::182ND ST::60	182ND STREET	LOCUST STREET	182ND STREET	4,952	L & T CR	0.4%	Crack Sealing - AC	\$17
LNSG::182ND ST::60	182ND STREET	LOCUST STREET	182ND STREET	4,952	RUTTING	0.4%	Patching - AC Deep	\$235
LNSG::182ND ST::60	182ND STREET	LOCUST STREET	182ND STREET	4,952	RUTTING	0.5%	Patching - AC Shallow	\$126
LNSG::182ND ST::70	182ND STREET	182ND STREET	VIOLET ROAD	20,043	L & T CR	3.0%	Crack Sealing - AC	\$591
LNSG::182ND ST::70	182ND STREET	182ND STREET	VIOLET ROAD	20,043	ALLIGATOR CR	0.8%	Crack Sealing - AC	\$62
LNSG::182ND ST::70	182ND STREET	182ND STREET	VIOLET ROAD	20,043	L & T CR	1.0%	Crack Sealing - AC	\$191
LNSG::182ND ST::70	182ND STREET	182ND STREET	VIOLET ROAD	20,043	ALLIGATOR CR	0.2%	Patching - AC Deep	\$676
LNSG::182ND ST::80	182ND STREET	VIOLET ROAD	TORRENCE AVENUE	14,903	RUTTING	0.1%	Patching - AC Shallow	\$81
LNSG::183RD PL::60	183RD PLACE	LOCUST STREET	ROSE AVENUE	8,818	L & T CR	3.8%	Crack Sealing - AC	\$336
LNSG::183RD PL::60	183RD PLACE	LOCUST STREET	ROSE AVENUE	8,818	ALLIGATOR CR	1.4%	Crack Sealing - AC	\$52
LNSG::183RD PL::60	183RD PLACE	LOCUST STREET	ROSE AVENUE	8,818	L & T CR	1.0%	Crack Sealing - AC	\$90
LNSG::183RD PL::60	183RD PLACE	LOCUST STREET	ROSE AVENUE	8,818	ALLIGATOR CR	1.1%	Patching - AC Deep	\$1,484

Pavement ID	Road Name	From	To	Area	Distress Type	Density	Maint. Activity	Cost
LNSG::183RD PL::90	183RD PLACE	OAKLEY AVENUE	SAILOR LANE	7,713	L & T CR	1.4%	Crack Sealing - AC	\$107
LNSG::183RD PL::90	183RD PLACE	OAKLEY AVENUE	SAILOR LANE	7,713	ALLIGATOR CR	3.9%	Crack Sealing - AC	\$115
LNSG::183RD ST::10	183RD STREET	HOLLAND ROAD	END	5,512	ALLIGATOR CR	0.6%	Crack Sealing - AC	\$17
LNSG::183RD ST::10	183RD STREET	HOLLAND ROAD	END	5,512	L & T CR	3.8%	Crack Sealing - AC	\$210
LNSG::183RD ST::50	183RD STREET	TORRENCE AVENUE	FRED STREET	18,927	ALLIGATOR CR	0.6%	Crack Sealing - AC	\$49
LNSG::183RD ST::50	183RD STREET	TORRENCE AVENUE	FRED STREET	18,927	L & T CR	2.7%	Crack Sealing - AC	\$518
LNSG::183RD ST::50	183RD STREET	TORRENCE AVENUE	FRED STREET	18,927	L & T CR	1.8%	Crack Sealing - AC	\$340
LNSG::183RD ST::50	183RD STREET	TORRENCE AVENUE	FRED STREET	18,927	ALLIGATOR CR	0.4%	Patching - AC Deep	\$1,245
LNSG::183RD ST::50	183RD STREET	TORRENCE AVENUE	FRED STREET	18,927	RUTTING	0.0%	Patching - AC Deep	\$21
LNSG::183RD ST::50	183RD STREET	TORRENCE AVENUE	FRED STREET	18,927	RUTTING	0.0%	Patching - AC Shallow	\$21
LNSG::183RD ST::60	183RD STREET	FRED STREET	OAKLEY AVENUE	18,428	L & T CR	0.5%	Crack Sealing - AC	\$86
LNSG::183RD ST::60	183RD STREET	FRED STREET	OAKLEY AVENUE	18,428	L & T CR	4.5%	Crack Sealing - AC	\$828
LNSG::183RD ST::70	183RD STREET	OAKLEY AVENUE	SAILOR LANE	7,665	L & T CR	1.0%	Crack Sealing - AC	\$80
LNSG::183RD ST::70	183RD STREET	OAKLEY AVENUE	SAILOR LANE	7,665	ALLIGATOR CR	0.1%	Crack Sealing - AC	\$8
LNSG::183RD ST::70	183RD STREET	OAKLEY AVENUE	SAILOR LANE	7,665	RUTTING	0.2%	Patching - AC Shallow	\$65
LNSG::185TH ST::110	185TH STREET	GLEN OAK AVENUE	ARCADIA AVENUE	9,308	L & T CR	4.3%	Crack Sealing - AC	\$397
LNSG::185TH ST::110	185TH STREET	GLEN OAK AVENUE	ARCADIA AVENUE	9,308	ALLIGATOR CR	2.1%	Crack Sealing - AC	\$77
LNSG::185TH ST::110	185TH STREET	GLEN OAK AVENUE	ARCADIA AVENUE	9,308	ALLIGATOR CR	1.4%	Patching - AC Deep	\$1,967
LNSG::186TH ST::20	186TH STREET	DANTE AVENUE	FOREST VIEW LANE	4,288	ALLIGATOR CR	1.7%	Crack Sealing - AC	\$34
LNSG::186TH ST::20	186TH STREET	DANTE AVENUE	FOREST VIEW LANE	4,288	L & T CR	1.0%	Crack Sealing - AC	\$44
LNSG::186TH ST::30	186TH STREET	STONY ISLAND AVENUE	DANTE AVENUE	27,667	ALLIGATOR CR	0.3%	Crack Sealing - AC	\$33
LNSG::186TH ST::30	186TH STREET	STONY ISLAND AVENUE	DANTE AVENUE	27,667	L & T CR	0.1%	Crack Sealing - AC	\$38
LNSG::186TH ST::30	186TH STREET	STONY ISLAND AVENUE	DANTE AVENUE	27,667	L & T CR	2.3%	Crack Sealing - AC	\$623
LNSG::186TH ST::40	186TH STREET	WENTWORTH AVENUE	END	20,252	RUTTING	0.1%	Patching - AC Deep	\$118
LNSG::186TH ST::40	186TH STREET	WENTWORTH AVENUE	END	20,252	RUTTING	0.6%	Patching - AC Shallow	\$718
LNSG::187TH ST::40	187TH STREET	HENRY STREET	SHERMAN STREET	9,380	RUTTING	0.4%	Patching - AC Shallow	\$194
LNSG::187TH ST::50	187TH STREET	SHERMAN STREET	WILLIAM STREET	9,140	BLOCK CR	8.9%	Crack Sealing - AC	\$249
LNSG::189TH ST::10	189TH STREET	RIDGEWOOD AVENUE	WILDWOOD AVENUE	33,403	BLOCK CR	25.2%	Crack Sealing - AC	\$2,562
LNSG::189TH ST::10	189TH STREET	RIDGEWOOD AVENUE	WILDWOOD AVENUE	33,403	L & T CR	0.3%	Crack Sealing - AC	\$101
LNSG::189TH ST::10	189TH STREET	RIDGEWOOD AVENUE	WILDWOOD AVENUE	33,403	L & T CR	2.9%	Crack Sealing - AC	\$967
LNSG::189TH ST::10	189TH STREET	RIDGEWOOD AVENUE	WILDWOOD AVENUE	33,403	ALLIGATOR CR	0.3%	Crack Sealing - AC	\$42
LNSG::189TH ST::10	189TH STREET	RIDGEWOOD AVENUE	WILDWOOD AVENUE	33,403	EDGE CR	0.2%	Crack Sealing - AC	\$76
LNSG::189TH ST::10	189TH STREET	RIDGEWOOD AVENUE	WILDWOOD AVENUE	33,403	ALLIGATOR CR	0.9%	Patching - AC Deep	\$4,003
LNSG::190TH PL::20	190TH PLACE	OAKWOOD AVENUE	END	25,624	L & T CR	0.1%	Crack Sealing - AC	\$22
LNSG::190TH PL::20	190TH PLACE	OAKWOOD AVENUE	END	25,624	ALLIGATOR CR	0.3%	Crack Sealing - AC	\$32
LNSG::190TH PL::20	190TH PLACE	OAKWOOD AVENUE	END	25,624	L & T CR	0.9%	Crack Sealing - AC	\$221
LNSG::190TH PL::20	190TH PLACE	OAKWOOD AVENUE	END	25,624	BLOCK CR	7.4%	Crack Sealing - AC	\$574
LNSG::190TH PL::20	190TH PLACE	OAKWOOD AVENUE	END	25,624	ALLIGATOR CR	2.4%	Patching - AC Deep	\$7,760
LNSG::191ST ST::10	191ST STREET	OAKWOOD AVENUE	WILDWOOD AVENUE	19,461	L & T CR	2.1%	Crack Sealing - AC	\$411
LNSG::191ST ST::10	191ST STREET	OAKWOOD AVENUE	WILDWOOD AVENUE	19,461	ALLIGATOR CR	0.7%	Crack Sealing - AC	\$56
LNSG::191ST ST::20	191ST STREET	PARK AVENUE	OAKWOOD AVENUE	21,547	ALLIGATOR CR	0.2%	Crack Sealing - AC	\$24

Pavement ID	Road Name	From	To	Area	Distress Type	Density	Maint. Activity	Cost
LNSG::191ST ST::20	191ST STREET	PARK AVENUE	OAKWOOD AVENUE	21,547	L & T CR	2.3%	Crack Sealing - AC	\$492
LNSG::191ST ST::20	191ST STREET	PARK AVENUE	OAKWOOD AVENUE	21,547	RUTTING	0.1%	Patching - AC Shallow	\$67
LNSG::191ST ST::30	191ST STREET	GREENBAY AVENUE	PARK AVENUE	23,131	L & T CR	3.0%	Crack Sealing - AC	\$684
LNSG::191ST ST::30	191ST STREET	GREENBAY AVENUE	PARK AVENUE	23,131	ALLIGATOR CR	1.0%	Crack Sealing - AC	\$90
LNSG::191ST ST::30	191ST STREET	GREENBAY AVENUE	PARK AVENUE	23,131	L & T CR	0.3%	Crack Sealing - AC	\$59
LNSG::191ST ST::30	191ST STREET	GREENBAY AVENUE	PARK AVENUE	23,131	ALLIGATOR CR	2.9%	Patching - AC Deep	\$8,559
LNSG::191ST ST::40	191ST STREET	191ST STREET	191ST STREET	4,358	ALLIGATOR CR	1.8%	Crack Sealing - AC	\$36
LNSG::191ST ST::40	191ST STREET	191ST STREET	191ST STREET	4,358	EDGE CR	0.5%	Crack Sealing - AC	\$20
LNSG::191ST ST::40	191ST STREET	191ST STREET	191ST STREET	4,358	L & T CR	0.7%	Crack Sealing - AC	\$30
LNSG::191ST ST::40	191ST STREET	191ST STREET	191ST STREET	4,358	L & T CR	1.7%	Crack Sealing - AC	\$74
LNSG::191ST ST::50	191ST STREET	BURNHAM AVENUE	GREENBAY AVENUE	6,118	L & T CR	0.7%	Crack Sealing - AC	\$42
LNSG::AD ST::70	ADA STREET	ILLINOIS STREET	RIDGE ROAD	26,009	L & T CR	3.4%	Crack Sealing - AC	\$893
LNSG::AD ST::70	ADA STREET	ILLINOIS STREET	RIDGE ROAD	26,009	ALLIGATOR CR	0.0%	Crack Sealing - AC	\$8
LNSG::AD ST::70	ADA STREET	ILLINOIS STREET	RIDGE ROAD	26,009	RUTTING	0.0%	Patching - AC Shallow	\$39
LNSG::DMS ST::10	ADAMS STREET	BURNHAM AVENUE	WALTER STREET	9,250	L & T CR	0.2%	Crack Sealing - AC	\$19
LNSG::DMS ST::40	ADAMS STREET	COMMUNITY STREET	HENRY STREET	9,189	ALLIGATOR CR	4.0%	Patching - AC Deep	\$4,903
LNSG::RPRT DR::10	AIRPORT DRIVE	GLENWOOD LANSING ROA	END	36,800	L & T CR	2.9%	Crack Sealing - AC	\$1,056
LNSG::RPRT DR::10	AIRPORT DRIVE	GLENWOOD LANSING ROA	END	36,800	L & T CR	0.2%	Crack Sealing - AC	\$75
LNSG::RPRT DR::10	AIRPORT DRIVE	GLENWOOD LANSING ROA	END	36,800	ALLIGATOR CR	0.5%	Crack Sealing - AC	\$73
LNSG::RPRT DR::10	AIRPORT DRIVE	GLENWOOD LANSING ROA	END	36,800	ALLIGATOR CR	0.6%	Patching - AC Deep	\$3,314
LNSG::RPRT DR::10	AIRPORT DRIVE	GLENWOOD LANSING ROA	END	36,800	RUTTING	0.1%	Patching - AC Shallow	\$103
LNSG::ANN ST::10	ANN STREET	GREENBAY AVENUE	BURNHAM AVENUE	9,506	L & T CR	2.6%	Crack Sealing - AC	\$251
LNSG::ANN ST::10	ANN STREET	GREENBAY AVENUE	BURNHAM AVENUE	9,506	ALLIGATOR CR	3.2%	Patching - AC Deep	\$4,154
LNSG::ANN ST::30	ANN STREET	WALTER STREET	ROY STREET	10,305	L & T CR	0.6%	Crack Sealing - AC	\$63
LNSG::ANN ST::30	ANN STREET	WALTER STREET	ROY STREET	10,305	ALLIGATOR CR	0.4%	Patching - AC Deep	\$846
LNSG::ANN ST::40	ANN STREET	ROY STREET	ADA STREET	10,508	RUTTING	0.3%	Patching - AC Shallow	\$151
LNSG::ANN ST::50	ANN STREET	ADA STREET	GRANT STREET	10,357	RUTTING	0.1%	Patching - AC Shallow	\$59
LNSG::RCD AVE::20	ARCADIA AVENUE	185TH STREET	184TH STREET	18,431	L & T CR	4.1%	Crack Sealing - AC	\$759
LNSG::RCD AVE::20	ARCADIA AVENUE	185TH STREET	184TH STREET	18,431	L & T CR	0.1%	Crack Sealing - AC	\$21
LNSG::RCD AVE::20	ARCADIA AVENUE	185TH STREET	184TH STREET	18,431	ALLIGATOR CR	2.2%	Crack Sealing - AC	\$151
LNSG::RCD AVE::20	ARCADIA AVENUE	185TH STREET	184TH STREET	18,431	PATCH/UT CUT	0.2%	Patching - AC Deep	\$701
LNSG::RCD AVE::30	ARCADIA AVENUE	THORNTON LANSING ROA	END	8,730	ALLIGATOR CR	0.3%	Crack Sealing - AC	\$16
LNSG::RCD AVE::30	ARCADIA AVENUE	THORNTON LANSING ROA	END	8,730	L & T CR	0.7%	Crack Sealing - AC	\$61
LNSG::RCD AVE::40	ARCADIA AVENUE	THORNTON LANSING ROA	180TH STREET	6,095	L & T CR	1.7%	Crack Sealing - AC	\$103
LNSG::RCD AVE::40	ARCADIA AVENUE	THORNTON LANSING ROA	180TH STREET	6,095	ALLIGATOR CR	0.5%	Crack Sealing - AC	\$18
LNSG::RCD AVE::40	ARCADIA AVENUE	THORNTON LANSING ROA	180TH STREET	6,095	ALLIGATOR CR	0.4%	Patching - AC Deep	\$491
LNSG::RCD AVE::70	ARCADIA AVENUE	178TH STREET	END	18,068	L & T CR	0.7%	Crack Sealing - AC	\$122
LNSG::RCD AVE::70	ARCADIA AVENUE	178TH STREET	END	18,068	ALLIGATOR CR	1.3%	Crack Sealing - AC	\$89
LNSG::RCD AVE::70	ARCADIA AVENUE	178TH STREET	END	18,068	L & T CR	1.1%	Crack Sealing - AC	\$205
LNSG::RCD AVE::70	ARCADIA AVENUE	178TH STREET	END	18,068	ALLIGATOR CR	1.5%	Patching - AC Deep	\$3,665
LNSG::BRNDN ST::40	BERNADINE STREET	188TH STREET	SHERMAN STREET	20,702	RUTTING	0.4%	Patching - AC Shallow	\$412

Pavement ID	Road Name	From	To	Area	Distress Type	Density	Maint. Activity	Cost
LNSG::BRNDN ST::70	BERNADINE STREET	178TH STREET	177TH STREET	18,648	ALLIGATOR CR	0.9%	Crack Sealing - AC	\$71
LNSG::BRNDN ST::70	BERNADINE STREET	178TH STREET	177TH STREET	18,648	L & T CR	1.9%	Crack Sealing - AC	\$348
LNSG::BRNDN ST::80	BERNADINE STREET	177TH STREET	176TH STREET	17,992	L & T CR	2.6%	Crack Sealing - AC	\$466
LNSG::BRNDN ST::80	BERNADINE STREET	177TH STREET	176TH STREET	17,992	ALLIGATOR CR	0.4%	Crack Sealing - AC	\$35
LNSG::BRNDN ST::80	BERNADINE STREET	177TH STREET	176TH STREET	17,992	ALLIGATOR CR	0.1%	Patching - AC Deep	\$448
LNSG::BRNDN ST::90	BERNADINE STREET	176TH STREET	175TH STREET	18,931	L & T CR	0.2%	Crack Sealing - AC	\$33
LNSG::BRNDN ST::90	BERNADINE STREET	176TH STREET	175TH STREET	18,931	L & T CR	2.9%	Crack Sealing - AC	\$550
LNSG::BRNDN ST::90	BERNADINE STREET	176TH STREET	175TH STREET	18,931	ALLIGATOR CR	3.4%	Crack Sealing - AC	\$226
LNSG::BRNDN ST::90	BERNADINE STREET	176TH STREET	175TH STREET	18,931	ALLIGATOR CR	0.2%	Patching - AC Deep	\$652
LNSG::BRNDN ST::100	BERNADINE STREET	BERNICE ROAD	BERNICE AVENUE	11,719	L & T CR	5.2%	Crack Sealing - AC	\$609
LNSG::BRNDN ST::100	BERNADINE STREET	BERNICE ROAD	BERNICE AVENUE	11,719	BLOCK CR	9.1%	Crack Sealing - AC	\$324
LNSG::BRNDN ST::130	BERNADINE STREET	172ND PLACE	BERNADINE STREET	4,040	BLOCK CR	21.5%	Crack Sealing - AC	\$264
LNSG::BRNDN ST::130	BERNADINE STREET	172ND PLACE	BERNADINE STREET	4,040	L & T CR	2.4%	Crack Sealing - AC	\$96
LNSG::BRNDN ST::130	BERNADINE STREET	172ND PLACE	BERNADINE STREET	4,040	RUTTING	0.0%	Patching - AC Deep	\$15
LNSG::BRNDN ST::130	BERNADINE STREET	172ND PLACE	BERNADINE STREET	4,040	RUTTING	0.1%	Patching - AC Shallow	\$15
LNSG::BRNC AVE::10	BERNICE AVENUE	OAK AVENUE	RAILROAD AVENUE	54,841	L & T CR	4.4%	Crack Sealing - AC	\$2,383
LNSG::BRNC AVE::10	BERNICE AVENUE	OAK AVENUE	RAILROAD AVENUE	54,841	BLOCK CR	1.9%	Crack Sealing - AC	\$311
LNSG::BRNC AVE::10	BERNICE AVENUE	OAK AVENUE	RAILROAD AVENUE	54,841	L & T CR	0.1%	Crack Sealing - AC	\$78
LNSG::BRNC AVE::10	BERNICE AVENUE	OAK AVENUE	RAILROAD AVENUE	54,841	ALLIGATOR CR	0.0%	Patching - AC Deep	\$338
LNSG::BRNC AVE::10	BERNICE AVENUE	OAK AVENUE	RAILROAD AVENUE	54,841	RUTTING	0.0%	Patching - AC Deep	\$17
LNSG::BRNC AVE::30	BERNICE AVENUE	TON AVENUE	CHICAGO AVENUE	12,642	L & T CR	3.6%	Crack Sealing - AC	\$453
LNSG::BRNC AVE::30	BERNICE AVENUE	TON AVENUE	CHICAGO AVENUE	12,642	L & T CR	1.4%	Crack Sealing - AC	\$170
LNSG::BRNC AVE::30	BERNICE AVENUE	TON AVENUE	CHICAGO AVENUE	12,642	RUTTING	0.1%	Patching - AC Shallow	\$86
LNSG::BRNC AVE::40	BERNICE AVENUE	CHICAGO AVENUE	LORENZ AVENUE	17,131	L & T CR	0.2%	Crack Sealing - AC	\$38
LNSG::BRNC AVE::40	BERNICE AVENUE	CHICAGO AVENUE	LORENZ AVENUE	17,131	L & T CR	5.6%	Crack Sealing - AC	\$966
LNSG::BRNC AVE::40	BERNICE AVENUE	CHICAGO AVENUE	LORENZ AVENUE	17,131	RUTTING	0.0%	Patching - AC Shallow	\$31
LNSG::BRNC AVE::50	BERNICE AVENUE	LORENZ AVENUE	GREENBAY AVENUE	10,145	L & T CR	3.4%	Crack Sealing - AC	\$341
LNSG::BRNC AVE::70	BERNICE AVENUE	BURNHAM AVENUE	WALTER STREET	9,350	L & T CR	1.7%	Crack Sealing - AC	\$163
LNSG::BRNC AVE::70	BERNICE AVENUE	BURNHAM AVENUE	WALTER STREET	9,350	L & T CR	0.9%	Patching - AC Shallow	\$1,428
LNSG::BRNC AVE::80	BERNICE AVENUE	WALTER STREET	ROY STREET	9,305	L & T CR	3.5%	Crack Sealing - AC	\$328
LNSG::BRNC AVE::80	BERNICE AVENUE	WALTER STREET	ROY STREET	9,305	ALLIGATOR CR	0.6%	Patching - AC Deep	\$1,034
LNSG::BRNC AVE::80	BERNICE AVENUE	WALTER STREET	ROY STREET	9,305	RUTTING	0.0%	Patching - AC Shallow	\$17
LNSG::BRNC AVE::90	BERNICE AVENUE	ROY STREET	COMMUNITY STREET	9,250	BLOCK CR	8.1%	Crack Sealing - AC	\$227
LNSG::BRNC AVE::90	BERNICE AVENUE	ROY STREET	COMMUNITY STREET	9,250	L & T CR	2.7%	Crack Sealing - AC	\$250
LNSG::BRNC AVE::90	BERNICE AVENUE	ROY STREET	COMMUNITY STREET	9,250	RUTTING	0.0%	Patching - AC Shallow	\$15
LNSG::BRNC AVE::90	BERNICE AVENUE	ROY STREET	COMMUNITY STREET	9,250	L & T CR	0.4%	Patching - AC Shallow	\$681
LNSG::BRNC AVE::100	BERNICE AVENUE	COMMUNITY STREET	HENRY STREET	9,239	L & T CR	4.4%	Crack Sealing - AC	\$402
LNSG::BRNC AVE::110	BERNICE AVENUE	HENRY STREET	BERNADINE STREET	26,690	L & T CR	6.0%	Crack Sealing - AC	\$1,592
LNSG::BRNC AVE::110	BERNICE AVENUE	HENRY STREET	BERNADINE STREET	26,690	L & T CR	0.3%	Crack Sealing - AC	\$82
LNSG::BRNC AVE::110	BERNICE AVENUE	HENRY STREET	BERNADINE STREET	26,690	RUTTING	0.0%	Patching - AC Deep	\$33
LNSG::BRNC AVE::110	BERNICE AVENUE	HENRY STREET	BERNADINE STREET	26,690	RUTTING	0.1%	Patching - AC Shallow	\$67

Pavement ID	Road Name	From	To	Area	Distress Type	Density	Maint. Activity	Cost
LNSG::BRNC AVE::130	BERNICE AVENUE	BERNICE AVENUE	174TH COURT	10,351	L & T CR	4.6%	Crack Sealing - AC	\$479
LNSG::BRNC RD::30	BERNICE ROAD	173RD STREET	BENSLEY AVENUE	35,560	ALLIGATOR CR	0.4%	Crack Sealing - AC	\$63
LNSG::BRNC RD::30	BERNICE ROAD	173RD STREET	BENSLEY AVENUE	35,560	L & T CR	0.9%	Crack Sealing - AC	\$305
LNSG::BRNC RD::30	BERNICE ROAD	173RD STREET	BENSLEY AVENUE	35,560	L & T CR	1.0%	Crack Sealing - AC	\$362
LNSG::BRNC RD::30	BERNICE ROAD	173RD STREET	BENSLEY AVENUE	35,560	ALLIGATOR CR	0.4%	Patching - AC Deep	\$1,907
LNSG::BRNC RD::30	BERNICE ROAD	173RD STREET	BENSLEY AVENUE	35,560	RUTTING	0.0%	Patching - AC Shallow	\$35
LNSG::BRNC RD::40	BERNICE ROAD	OAK AVENUE	END	11,890	BLOCK CR	21.8%	Crack Sealing - AC	\$789
LNSG::BRNC RD::40	BERNICE ROAD	OAK AVENUE	END	11,890	L & T CR	4.9%	Crack Sealing - AC	\$586
LNSG::BRNC RD::60	BERNICE ROAD	OAK AVENUE	BERNICE ROAD	1,639	L & T CR	5.4%	Crack Sealing - AC	\$89
LNSG::BRNC RD::60	BERNICE ROAD	OAK AVENUE	BERNICE ROAD	1,639	L & T CR	0.7%	Crack Sealing - AC	\$11
LNSG::BRNC RD::70	BERNICE ROAD	BERNICE ROAD	RAILROAD AVENUE	54,082	L & T CR	2.3%	Crack Sealing - AC	\$1,225
LNSG::BRNC RD::70	BERNICE ROAD	BERNICE ROAD	RAILROAD AVENUE	54,082	L & T CR	2.9%	Crack Sealing - AC	\$1,564
LNSG::BRNC RD::70	BERNICE ROAD	BERNICE ROAD	RAILROAD AVENUE	54,082	ALLIGATOR CR	0.7%	Crack Sealing - AC	\$135
LNSG::BRNC RD::70	BERNICE ROAD	BERNICE ROAD	RAILROAD AVENUE	54,082	ALLIGATOR CR	0.4%	Patching - AC Deep	\$2,927
LNSG::BRNC RD::70	BERNICE ROAD	BERNICE ROAD	RAILROAD AVENUE	54,082	ALLIGATOR CR	0.9%	Patching - AC Deep	\$6,340
LNSG::BRNC RD::70	BERNICE ROAD	BERNICE ROAD	RAILROAD AVENUE	54,082	RUTTING	0.0%	Patching - AC Deep	\$16
LNSG::BRNC RD::70	BERNICE ROAD	BERNICE ROAD	RAILROAD AVENUE	54,082	RUTTING	0.0%	Patching - AC Shallow	\$47
LNSG::BRNC RD::130	BERNICE ROAD	LORENZ AVENUE	LORENZ AVENUE	1,896	BLOCK CR	31.7%	Crack Sealing - AC	\$183
LNSG::BRNC RD::180	BERNICE ROAD	ROY STREET	COMMUNITY STREET	9,261	L & T CR	3.1%	Crack Sealing - AC	\$285
LNSG::BRNC RD::180	BERNICE ROAD	ROY STREET	COMMUNITY STREET	9,261	L & T CR	0.4%	Crack Sealing - AC	\$38
LNSG::BB MLKS DR::10	BOB MALKAS DRIVE	BURNHAM AVENUE	END	25,359	EDGE CR	0.1%	Crack Sealing - AC	\$26
LNSG::BB MLKS DR::10	BOB MALKAS DRIVE	BURNHAM AVENUE	END	25,359	L & T CR	2.9%	Crack Sealing - AC	\$740
LNSG::BB MLKS DR::10	BOB MALKAS DRIVE	BURNHAM AVENUE	END	25,359	L & T CR	0.5%	Crack Sealing - AC	\$130
LNSG::BB MLKS DR::10	BOB MALKAS DRIVE	BURNHAM AVENUE	END	25,359	ALLIGATOR CR	1.3%	Crack Sealing - AC	\$120
LNSG::BB MLKS DR::10	BOB MALKAS DRIVE	BURNHAM AVENUE	END	25,359	ALLIGATOR CR	1.1%	Patching - AC Deep	\$3,924
LNSG::BCK RD::10	BOCK ROAD	THORNTON LANSING ROA	END	8,928	L & T CR	0.2%	Crack Sealing - AC	\$19
LNSG::BCK RD::10	BOCK ROAD	THORNTON LANSING ROA	END	8,928	EDGE CR	0.3%	Crack Sealing - AC	\$23
LNSG::BRITNY LN::20	BRITTANY LANE	181ST STREET	180TH STREET	17,974	L & T CR	0.9%	Crack Sealing - AC	\$166
LNSG::BRITNY LN::20	BRITTANY LANE	181ST STREET	180TH STREET	17,974	BLOCK CR	11.4%	Crack Sealing - AC	\$626
LNSG::BRITNY LN::20	BRITTANY LANE	181ST STREET	180TH STREET	17,974	L & T CR	0.4%	Crack Sealing - AC	\$62
LNSG::CNTRL ST::20	CENTRAL STREET	MERTLE COURT	WENTWORTH AVENUE	8,834	BLOCK CR	77.1%	Crack Sealing - AC	\$2,076
LNSG::CNTRL ST::20	CENTRAL STREET	MERTLE COURT	WENTWORTH AVENUE	8,834	ALLIGATOR CR	0.2%	Crack Sealing - AC	\$12
LNSG::CNTRL ST::20	CENTRAL STREET	MERTLE COURT	WENTWORTH AVENUE	8,834	L & T CR	0.2%	Crack Sealing - AC	\$19
LNSG::CHPPL AVE::10	CHAPPEL AVENUE	THORNTON LANSING ROA	177TH STREET	46,556	L & T CR	0.8%	Crack Sealing - AC	\$373
LNSG::CHPPL AVE::10	CHAPPEL AVENUE	THORNTON LANSING ROA	177TH STREET	46,556	ALLIGATOR CR	0.4%	Crack Sealing - AC	\$78
LNSG::CHPPL AVE::10	CHAPPEL AVENUE	THORNTON LANSING ROA	177TH STREET	46,556	RUTTING	0.0%	Patching - AC Shallow	\$22
LNSG::CHPPL AVE::20	CHAPPEL AVENUE	177TH STREET	FRONTAGE ROAD	21,680	L & T CR	0.4%	Crack Sealing - AC	\$81
LNSG::CHRLTT DR::10	CHARLOTTE DRIVE	181ST PLACE	180TH STREET	28,055	ALLIGATOR CR	0.4%	Crack Sealing - AC	\$47
LNSG::CHRLTT DR::10	CHARLOTTE DRIVE	181ST PLACE	180TH STREET	28,055	L & T CR	2.2%	Crack Sealing - AC	\$628
LNSG::CHRLTT DR::10	CHARLOTTE DRIVE	181ST PLACE	180TH STREET	28,055	L & T CR	0.5%	Crack Sealing - AC	\$152
LNSG::CHRLTT DR::10	CHARLOTTE DRIVE	181ST PLACE	180TH STREET	28,055	ALLIGATOR CR	1.5%	Patching - AC Deep	\$5,406

Pavement ID	Road Name	From	To	Area	Distress Type	Density	Maint. Activity	Cost
LNSG::CHRLTT DR::10	CHARLOTTE DRIVE	181ST PLACE	180TH STREET	28,055	L & T CR	0.1%	Patching - AC Shallow	\$395
LNSG::CHCG AVE::80	CHICAGO AVENUE	RIDGE ROAD	181ST STREET	18,550	ALLIGATOR CR	0.7%	Crack Sealing - AC	\$56
LNSG::CHCG AVE::80	CHICAGO AVENUE	RIDGE ROAD	181ST STREET	18,550	L & T CR	3.4%	Crack Sealing - AC	\$633
LNSG::CHCG AVE::80	CHICAGO AVENUE	RIDGE ROAD	181ST STREET	18,550	L & T CR	0.1%	Crack Sealing - AC	\$21
LNSG::CHCG AVE::80	CHICAGO AVENUE	RIDGE ROAD	181ST STREET	18,550	ALLIGATOR CR	0.4%	Patching - AC Deep	\$1,148
LNSG::CHCG AVE::80	CHICAGO AVENUE	RIDGE ROAD	181ST STREET	18,550	SLIPPAGE CR	0.3%	Patching - AC Shallow	\$450
LNSG::CHCG AVE::90	CHICAGO AVENUE	181ST STREET	180TH STREET	18,596	L & T CR	3.3%	Crack Sealing - AC	\$613
LNSG::CHCG AVE::90	CHICAGO AVENUE	181ST STREET	180TH STREET	18,596	ALLIGATOR CR	0.8%	Crack Sealing - AC	\$62
LNSG::CHCG AVE::90	CHICAGO AVENUE	181ST STREET	180TH STREET	18,596	ALLIGATOR CR	0.1%	Patching - AC Deep	\$547
LNSG::CHCG AVE::110	CHICAGO AVENUE	179TH STREET	178TH STREET	18,636	L & T CR	0.2%	Crack Sealing - AC	\$43
LNSG::CHCG AVE::110	CHICAGO AVENUE	179TH STREET	178TH STREET	18,636	ALLIGATOR CR	1.0%	Crack Sealing - AC	\$76
LNSG::CHCG AVE::110	CHICAGO AVENUE	179TH STREET	178TH STREET	18,636	L & T CR	1.2%	Crack Sealing - AC	\$215
LNSG::CHCG AVE::110	CHICAGO AVENUE	179TH STREET	178TH STREET	18,636	ALLIGATOR CR	0.1%	Patching - AC Deep	\$534
LNSG::CHCG AVE::120	CHICAGO AVENUE	178TH STREET	PARK BOULEVARD	13,601	L & T CR	0.7%	Crack Sealing - AC	\$95
LNSG::CHCG AVE::120	CHICAGO AVENUE	178TH STREET	PARK BOULEVARD	13,601	L & T CR	5.0%	Crack Sealing - AC	\$686
LNSG::CHCG AVE::130	CHICAGO AVENUE	PARK BOULEVARD	SPRING LAKE DRIVE	14,208	L & T CR	5.9%	Crack Sealing - AC	\$835
LNSG::CHCG AVE::130	CHICAGO AVENUE	PARK BOULEVARD	SPRING LAKE DRIVE	14,208	BLOCK CR	13.0%	Crack Sealing - AC	\$564
LNSG::CHCG AVE::140	CHICAGO AVENUE	SPRING LAKE DRIVE	175TH STREET	27,355	L & T CR	3.9%	Crack Sealing - AC	\$1,067
LNSG::CHCG AVE::140	CHICAGO AVENUE	SPRING LAKE DRIVE	175TH STREET	27,355	BLOCK CR	0.8%	Crack Sealing - AC	\$67
LNSG::CHCG AVE::140	CHICAGO AVENUE	SPRING LAKE DRIVE	175TH STREET	27,355	L & T CR	0.3%	Crack Sealing - AC	\$75
LNSG::CHCG AVE::140	CHICAGO AVENUE	SPRING LAKE DRIVE	175TH STREET	27,355	ALLIGATOR CR	0.0%	Patching - AC Deep	\$315
LNSG::CHCG AVE::150	CHICAGO AVENUE	BERNICE ROAD	BERNICE AVENUE	10,588	L & T CR	3.1%	Crack Sealing - AC	\$330
LNSG::CHCG AVE::150	CHICAGO AVENUE	BERNICE ROAD	BERNICE AVENUE	10,588	BLOCK CR	18.5%	Crack Sealing - AC	\$595
LNSG::CHCG AVE::150	CHICAGO AVENUE	BERNICE ROAD	BERNICE AVENUE	10,588	L & T CR	0.7%	Crack Sealing - AC	\$71
LNSG::CHCG AVE::170	CHICAGO AVENUE	BERNICE ROAD	173RD STREET	4,442	BLOCK CR	1.4%	Crack Sealing - AC	\$18
LNSG::CHCG AVE::170	CHICAGO AVENUE	BERNICE ROAD	173RD STREET	4,442	L & T CR	4.8%	Crack Sealing - AC	\$211
LNSG::CHCG AVE::180	CHICAGO AVENUE	173RD PLACE	FLANIGAN DRIVE	2,107	BLOCK CR	37.0%	Crack Sealing - AC	\$237
LNSG::CMMRCL ST::10	COMMERCIAL STREET	181ST STREET	180TH STREET	16,141	L & T CR	0.3%	Crack Sealing - AC	\$54
LNSG::CMMRCL ST::10	COMMERCIAL STREET	181ST STREET	180TH STREET	16,141	L & T CR	2.9%	Crack Sealing - AC	\$467
LNSG::CMMRCL ST::20	COMMERCIAL STREET	180TH STREET	179TH STREET	18,443	L & T CR	0.2%	Crack Sealing - AC	\$41
LNSG::CMMRCL ST::20	COMMERCIAL STREET	180TH STREET	179TH STREET	18,443	ALLIGATOR CR	0.4%	Crack Sealing - AC	\$37
LNSG::CMMRCL ST::20	COMMERCIAL STREET	180TH STREET	179TH STREET	18,443	L & T CR	2.1%	Crack Sealing - AC	\$395
LNSG::CMMRCL ST::30	COMMERCIAL STREET	179TH STREET	178TH STREET	18,253	L & T CR	1.8%	Crack Sealing - AC	\$329
LNSG::CMMRCL ST::30	COMMERCIAL STREET	179TH STREET	178TH STREET	18,253	ALLIGATOR CR	1.3%	Crack Sealing - AC	\$93
LNSG::CMMRCL ST::30	COMMERCIAL STREET	179TH STREET	178TH STREET	18,253	ALLIGATOR CR	9.9%	Patching - AC Deep	\$21,860
LNSG::CMMNTY ST::20	COMMUNITY STREET	ADAMS STREET	178TH STREET	19,412	L & T CR	0.6%	Crack Sealing - AC	\$115
LNSG::CMMNTY ST::20	COMMUNITY STREET	ADAMS STREET	178TH STREET	19,412	ALLIGATOR CR	0.7%	Crack Sealing - AC	\$57
LNSG::CMMNTY ST::20	COMMUNITY STREET	ADAMS STREET	178TH STREET	19,412	ALLIGATOR CR	0.1%	Patching - AC Deep	\$514
LNSG::CMMNTY ST::20	COMMUNITY STREET	ADAMS STREET	178TH STREET	19,412	RUTTING	0.0%	Patching - AC Shallow	\$19
LNSG::CMMNTY ST::30	COMMUNITY STREET	178TH STREET	177TH STREET	18,508	L & T CR	0.2%	Crack Sealing - AC	\$43
LNSG::CMMNTY ST::30	COMMUNITY STREET	178TH STREET	177TH STREET	18,508	L & T CR	3.6%	Crack Sealing - AC	\$674

Pavement ID	Road Name	From	To	Area	Distress Type	Density	Maint. Activity	Cost
LNSG::CMMNTY ST::30	COMMUNITY STREET	178TH STREET	177TH STREET	18,508	ALLIGATOR CR	1.0%	Crack Sealing - AC	\$72
LNSG::CMMNTY ST::30	COMMUNITY STREET	178TH STREET	177TH STREET	18,508	ALLIGATOR CR	2.9%	Patching - AC Deep	\$7,064
LNSG::CMMNTY ST::30	COMMUNITY STREET	178TH STREET	177TH STREET	18,508	RUTTING	0.0%	Patching - AC Shallow	\$21
LNSG::CMMNTY ST::40	COMMUNITY STREET	177TH STREET	176TH STREET	18,543	ALLIGATOR CR	0.5%	Crack Sealing - AC	\$41
LNSG::CMMNTY ST::40	COMMUNITY STREET	177TH STREET	176TH STREET	18,543	L & T CR	2.4%	Crack Sealing - AC	\$446
LNSG::CMMNTY ST::40	COMMUNITY STREET	177TH STREET	176TH STREET	18,543	ALLIGATOR CR	1.2%	Patching - AC Deep	\$3,039
LNSG::CMMNTY ST::40	COMMUNITY STREET	177TH STREET	176TH STREET	18,543	POTHOLE	0.0%	Patching - AC Deep	\$157
LNSG::CMMNTY ST::40	COMMUNITY STREET	177TH STREET	176TH STREET	18,543	RUTTING	0.2%	Patching - AC Shallow	\$154
LNSG::CMMNTY ST::50	COMMUNITY STREET	176TH STREET	175TH STREET	18,637	L & T CR	3.8%	Crack Sealing - AC	\$701
LNSG::CMMNTY ST::50	COMMUNITY STREET	176TH STREET	175TH STREET	18,637	L & T CR	0.5%	Crack Sealing - AC	\$84
LNSG::CMMNTY ST::50	COMMUNITY STREET	176TH STREET	175TH STREET	18,637	ALLIGATOR CR	1.2%	Crack Sealing - AC	\$88
LNSG::CMMNTY ST::50	COMMUNITY STREET	176TH STREET	175TH STREET	18,637	ALLIGATOR CR	1.6%	Patching - AC Deep	\$4,047
LNSG::CMMNTY ST::50	COMMUNITY STREET	176TH STREET	175TH STREET	18,637	RUTTING	0.0%	Patching - AC Shallow	\$18
LNSG::CRYSTL LN::20	CRYSTAL LANE	181ST STREET	180TH STREET	18,109	L & T CR	1.2%	Crack Sealing - AC	\$212
LNSG::DNTS DR::10	DONATUS DRIVE	181ST PLACE	181ST STREET	16,240	ALLIGATOR CR	0.3%	Crack Sealing - AC	\$22
LNSG::DNTS DR::10	DONATUS DRIVE	181ST PLACE	181ST STREET	16,240	L & T CR	2.1%	Crack Sealing - AC	\$336
LNSG::DNTS DR::10	DONATUS DRIVE	181ST PLACE	181ST STREET	16,240	L & T CR	0.5%	Crack Sealing - AC	\$84
LNSG::DNTS DR::10	DONATUS DRIVE	181ST PLACE	181ST STREET	16,240	ALLIGATOR CR	1.2%	Patching - AC Deep	\$2,892
LNSG::DNTS DR::20	DONATUS DRIVE	181ST STREET	180TH STREET	12,174	L & T CR	2.4%	Crack Sealing - AC	\$293
LNSG::DNTS DR::20	DONATUS DRIVE	181ST STREET	180TH STREET	12,174	L & T CR	1.4%	Crack Sealing - AC	\$169
LNSG::DNTS DR::20	DONATUS DRIVE	181ST STREET	180TH STREET	12,174	ALLIGATOR CR	0.8%	Crack Sealing - AC	\$41
LNSG::DRCHST AVE::10	DORCHESTER AVENUE	182ND STREET	181ST STREET	21,084	RUTTING	0.0%	Patching - AC Shallow	\$39
LNSG::DRCHST AVE::20	DORCHESTER AVENUE	181ST STREET	WEST FRONTAGE ROAD	32,150	RUTTING	0.0%	Patching - AC Shallow	\$31
LNSG::SCNB AVE::30	ESCANABA AVENUE	180TH STREET	179TH STREET	18,399	ALLIGATOR CR	1.0%	Crack Sealing - AC	\$73
LNSG::SCNB AVE::30	ESCANABA AVENUE	180TH STREET	179TH STREET	18,399	L & T CR	0.2%	Crack Sealing - AC	\$41
LNSG::SCNB AVE::30	ESCANABA AVENUE	180TH STREET	179TH STREET	18,399	L & T CR	2.2%	Crack Sealing - AC	\$395
LNSG::SCNB AVE::30	ESCANABA AVENUE	180TH STREET	179TH STREET	18,399	ALLIGATOR CR	0.7%	Patching - AC Deep	\$1,835
LNSG::SCNB AVE::30	ESCANABA AVENUE	180TH STREET	179TH STREET	18,399	RUTTING	0.1%	Patching - AC Shallow	\$86
LNSG::XCHNG AVE::30	EXCHANGE AVENUE	181ST STREET	180TH STREET	16,237	ALLIGATOR CR	0.2%	Crack Sealing - AC	\$20
LNSG::XCHNG AVE::30	EXCHANGE AVENUE	181ST STREET	180TH STREET	16,237	L & T CR	2.9%	Crack Sealing - AC	\$474
LNSG::XCHNG AVE::30	EXCHANGE AVENUE	181ST STREET	180TH STREET	16,237	RUTTING	0.0%	Patching - AC Shallow	\$17
LNSG::XCHNG AVE::40	EXCHANGE AVENUE	180TH STREET	179TH STREET	18,421	L & T CR	2.5%	Crack Sealing - AC	\$457
LNSG::XCHNG AVE::40	EXCHANGE AVENUE	180TH STREET	179TH STREET	18,421	L & T CR	0.4%	Crack Sealing - AC	\$64
LNSG::XCHNG AVE::40	EXCHANGE AVENUE	180TH STREET	179TH STREET	18,421	ALLIGATOR CR	0.8%	Crack Sealing - AC	\$59
LNSG::XCHNG AVE::40	EXCHANGE AVENUE	180TH STREET	179TH STREET	18,421	ALLIGATOR CR	0.9%	Patching - AC Deep	\$2,531
LNSG::XCHNG AVE::70	EXCHANGE AVENUE	170TH STREET	TORRENCE AVENUE	110,190	RUTTING	0.0%	Patching - AC Shallow	\$56
LNSG::FR STN3 DR::10	FIRE STATION 3 DRIVE	BURNHAM AVENUE	END	3,385	JT SEAL DMG	100.0%	Joint Seal (Localized)	\$589
LNSG::FRD ST::10	FRED STREET	183RD STREET	END	4,924	L & T CR	1.7%	Crack Sealing - AC	\$85
LNSG::FRD ST::10	FRED STREET	183RD STREET	END	4,924	ALLIGATOR CR	0.2%	Patching - AC Deep	\$288
LNSG::FRD ST::20	FRED STREET	183RD STREET	RIDGE ROAD	11,534	BLOCK CR	81.9%	Crack Sealing - AC	\$2,880
LNSG::FRD ST::20	FRED STREET	183RD STREET	RIDGE ROAD	11,534	L & T CR	0.2%	Crack Sealing - AC	\$23

Pavement ID	Road Name	From	To	Area	Distress Type	Density	Maint. Activity	Cost
LNSG::FRNTG RD::10	FRONTAGE ROAD	VOLBRECHT ROAD	CHAPPEL AVENUE	49,297	L & T CR	0.7%	Crack Sealing - AC	\$350
LNSG::FRNTG RD::10	FRONTAGE ROAD	VOLBRECHT ROAD	CHAPPEL AVENUE	49,297	ALLIGATOR CR	0.7%	Crack Sealing - AC	\$133
LNSG::FRNTG RD::10	FRONTAGE ROAD	VOLBRECHT ROAD	CHAPPEL AVENUE	49,297	L & T CR	4.6%	Crack Sealing - AC	\$2,289
LNSG::FRNTG RD::10	FRONTAGE ROAD	VOLBRECHT ROAD	CHAPPEL AVENUE	49,297	EDGE CR	0.7%	Crack Sealing - AC	\$326
LNSG::FRNTG RD::10	FRONTAGE ROAD	VOLBRECHT ROAD	CHAPPEL AVENUE	49,297	ALLIGATOR CR	0.1%	Patching - AC Deep	\$1,205
LNSG::FRNTG RD::20	FRONTAGE ROAD	CHAPPEL AVENUE	PAXTON AVENUE	27,270	L & T CR	4.5%	Crack Sealing - AC	\$1,223
LNSG::FRNTG RD::20	FRONTAGE ROAD	CHAPPEL AVENUE	PAXTON AVENUE	27,270	L & T CR	0.4%	Crack Sealing - AC	\$96
LNSG::FRNTG RD::20	FRONTAGE ROAD	CHAPPEL AVENUE	PAXTON AVENUE	27,270	ALLIGATOR CR	0.5%	Crack Sealing - AC	\$52
LNSG::FRNTG RD::30	FRONTAGE ROAD	PAXTON AVENUE	END	22,461	L & T CR	1.5%	Crack Sealing - AC	\$328
LNSG::FRNTG RD::30	FRONTAGE ROAD	PAXTON AVENUE	END	22,461	L & T CR	1.6%	Crack Sealing - AC	\$362
LNSG::FRNTG RD::30	FRONTAGE ROAD	PAXTON AVENUE	END	22,461	ALLIGATOR CR	0.1%	Crack Sealing - AC	\$12
LNSG::GLN OK AVE::40	GLEN OAK AVENUE	THORNTON LANSING ROA	179TH STREET	24,704	BLOCK CR	21.5%	Crack Sealing - AC	\$1,616
LNSG::GLN OK AVE::40	GLEN OAK AVENUE	THORNTON LANSING ROA	179TH STREET	24,704	L & T CR	3.4%	Crack Sealing - AC	\$844
LNSG::GLN OK AVE::40	GLEN OAK AVENUE	THORNTON LANSING ROA	179TH STREET	24,704	L & T CR	0.3%	Crack Sealing - AC	\$84
LNSG::GLN OK AVE::40	GLEN OAK AVENUE	THORNTON LANSING ROA	179TH STREET	24,704	ALLIGATOR CR	1.1%	Patching - AC Deep	\$3,724
LNSG::GLN OK AVE::50	GLEN OAK AVENUE	179TH STREET	178TH STREET	18,277	L & T CR	2.3%	Crack Sealing - AC	\$419
LNSG::GLN OK AVE::50	GLEN OAK AVENUE	179TH STREET	178TH STREET	18,277	ALLIGATOR CR	0.5%	Crack Sealing - AC	\$42
LNSG::GLN OK AVE::50	GLEN OAK AVENUE	179TH STREET	178TH STREET	18,277	RUTTING	0.0%	Patching - AC Shallow	\$18
LNSG::GRNT ST::30	GRANT STREET	ANN STREET	ILLINOIS STREET	15,213	ALLIGATOR CR	0.2%	Crack Sealing - AC	\$15
LNSG::GRNT ST::40	GRANT STREET	ILLINOIS STREET	STEWART AVENUE	19,352	ALLIGATOR CR	0.5%	Crack Sealing - AC	\$42
LNSG::GRNT ST::40	GRANT STREET	ILLINOIS STREET	STEWART AVENUE	19,352	L & T CR	3.5%	Crack Sealing - AC	\$685
LNSG::GRNBY AVE::70	GREENBAY AVENUE	188TH PLACE	188TH STREET	8,954	ALLIGATOR CR	1.0%	Crack Sealing - AC	\$41
LNSG::GRNBY AVE::70	GREENBAY AVENUE	188TH PLACE	188TH STREET	8,954	L & T CR	3.6%	Crack Sealing - AC	\$323
LNSG::GRNBY AVE::110	GREENBAY AVENUE	176TH STREET	175TH STREET	18,355	ALLIGATOR CR	0.3%	Crack Sealing - AC	\$29
LNSG::GRNBY AVE::110	GREENBAY AVENUE	176TH STREET	175TH STREET	18,355	L & T CR	2.5%	Crack Sealing - AC	\$458
LNSG::GRNBY AVE::110	GREENBAY AVENUE	176TH STREET	175TH STREET	18,355	ALLIGATOR CR	0.1%	Patching - AC Deep	\$552
LNSG::HRPR AVE::20	HARPER AVENUE	THORNTON LANSING ROA	END	4,631	ALLIGATOR CR	1.7%	Crack Sealing - AC	\$36
LNSG::HRPR AVE::20	HARPER AVENUE	THORNTON LANSING ROA	END	4,631	L & T CR	2.7%	Crack Sealing - AC	\$126
LNSG::HRPR ST::10	HARPER STREET	182ND STREET	181ST PLACE	4,952	L & T CR	0.7%	Crack Sealing - AC	\$34
LNSG::HRPR ST::10	HARPER STREET	182ND STREET	181ST PLACE	4,952	L & T CR	1.7%	Crack Sealing - AC	\$85
LNSG::HRPR ST::10	HARPER STREET	182ND STREET	181ST PLACE	4,952	ALLIGATOR CR	0.6%	Crack Sealing - AC	\$17
LNSG::HNRV ST::30	HENRY STREET	OTTO STREET	186TH STREET	14,296	RUTTING	0.0%	Patching - AC Deep	\$17
LNSG::HNRV ST::30	HENRY STREET	OTTO STREET	186TH STREET	14,296	RUTTING	0.6%	Patching - AC Shallow	\$488
LNSG::HNRV ST::35	HENRY STREET	RIDGE ROAD	LAKE STREET	8,731	ALLIGATOR CR	0.4%	Crack Sealing - AC	\$18
LNSG::HNRV ST::35	HENRY STREET	RIDGE ROAD	LAKE STREET	8,731	L & T CR	1.8%	Crack Sealing - AC	\$154
LNSG::HNRV ST::35	HENRY STREET	RIDGE ROAD	LAKE STREET	8,731	L & T CR	0.4%	Crack Sealing - AC	\$38
LNSG::HNRV ST::40	HENRY STREET	LAKE STREET	RANDOLPH STREET	9,876	L & T CR	0.2%	Crack Sealing - AC	\$19
LNSG::HNRV ST::40	HENRY STREET	LAKE STREET	RANDOLPH STREET	9,876	RUTTING	0.1%	Patching - AC Deep	\$78
LNSG::HNRV ST::40	HENRY STREET	LAKE STREET	RANDOLPH STREET	9,876	RUTTING	0.0%	Patching - AC Shallow	\$17
LNSG::HNRV ST::50	HENRY STREET	RANDOLPH STREET	WASHINGTON STREET	9,685	L & T CR	0.4%	Crack Sealing - AC	\$38
LNSG::HNRV ST::50	HENRY STREET	RANDOLPH STREET	WASHINGTON STREET	9,685	ALLIGATOR CR	0.8%	Crack Sealing - AC	\$34

Pavement ID	Road Name	From	To	Area	Distress Type	Density	Maint. Activity	Cost
LNSG::HNRV ST::60	HENRY STREET	WASHINGTON STREET	MADISON STREET	9,885	L & T CR	0.4%	Crack Sealing - AC	\$39
LNSG::HNRV ST::60	HENRY STREET	WASHINGTON STREET	MADISON STREET	9,885	ALLIGATOR CR	0.3%	Crack Sealing - AC	\$16
LNSG::HNRV ST::60	HENRY STREET	WASHINGTON STREET	MADISON STREET	9,885	RUTTING	0.0%	Patching - AC Deep	\$16
LNSG::HNRV ST::60	HENRY STREET	WASHINGTON STREET	MADISON STREET	9,885	RUTTING	0.1%	Patching - AC Shallow	\$47
LNSG::HNRV ST::100	HENRY STREET	178TH STREET	177TH STREET	18,184	ALLIGATOR CR	1.0%	Crack Sealing - AC	\$72
LNSG::HNRV ST::100	HENRY STREET	178TH STREET	177TH STREET	18,184	L & T CR	3.7%	Crack Sealing - AC	\$669
LNSG::HNRV ST::100	HENRY STREET	178TH STREET	177TH STREET	18,184	L & T CR	0.7%	Crack Sealing - AC	\$124
LNSG::HNRV ST::100	HENRY STREET	178TH STREET	177TH STREET	18,184	ALLIGATOR CR	2.0%	Patching - AC Deep	\$4,894
LNSG::HNRV ST::100	HENRY STREET	178TH STREET	177TH STREET	18,184	RUTTING	0.1%	Patching - AC Shallow	\$51
LNSG::HNRV ST::120	HENRY STREET	176TH STREET	175TH STREET	18,732	L & T CR	0.2%	Crack Sealing - AC	\$45
LNSG::HNRV ST::120	HENRY STREET	176TH STREET	175TH STREET	18,732	L & T CR	3.7%	Crack Sealing - AC	\$683
LNSG::HNRV ST::120	HENRY STREET	176TH STREET	175TH STREET	18,732	ALLIGATOR CR	0.4%	Crack Sealing - AC	\$31
LNSG::HNRV ST::120	HENRY STREET	176TH STREET	175TH STREET	18,732	ALLIGATOR CR	1.1%	Patching - AC Deep	\$3,027
LNSG::HNRV ST::120	HENRY STREET	176TH STREET	175TH STREET	18,732	RUTTING	0.0%	Patching - AC Shallow	\$19
LNSG::HNRV ST::120	HENRY STREET	176TH STREET	175TH STREET	18,732	BLOCK CR	8.9%	Patching - AC Shallow	\$9,150
LNSG::HNRV ST::130	HENRY STREET	BERNICE ROAD	BERNICE AVENUE	11,544	BLOCK CR	0.7%	Crack Sealing - AC	\$25
LNSG::HNRV ST::130	HENRY STREET	BERNICE ROAD	BERNICE AVENUE	11,544	L & T CR	4.3%	Crack Sealing - AC	\$501
LNSG::HLLDY TERR::10	HOLLIDAY TERRACE	176TH PLACE	END	20,671	ALLIGATOR CR	1.3%	Crack Sealing - AC	\$106
LNSG::HLLDY TERR::10	HOLLIDAY TERRACE	176TH PLACE	END	20,671	L & T CR	0.7%	Crack Sealing - AC	\$142
LNSG::HLLDY TERR::10	HOLLIDAY TERRACE	176TH PLACE	END	20,671	ALLIGATOR CR	0.5%	Patching - AC Deep	\$1,512
LNSG::LLNS ST::30	ILLINOIS STREET	WALTER STREET	ROY STREET	10,404	RUTTING	0.0%	Patching - AC Shallow	\$16
LNSG::LLNS ST::40	ILLINOIS STREET	ROY STREET	ADA STREET	10,462	RUTTING	0.0%	Patching - AC Shallow	\$17
LNSG::LLNS ST::60	ILLINOIS STREET	GRANT STREET	SHERMAN STREET	9,160	L & T CR	4.6%	Crack Sealing - AC	\$419
LNSG::LLNS ST::60	ILLINOIS STREET	GRANT STREET	SHERMAN STREET	9,160	ALLIGATOR CR	0.2%	Crack Sealing - AC	\$10
LNSG::LLNS ST::60	ILLINOIS STREET	GRANT STREET	SHERMAN STREET	9,160	RUTTING	0.0%	Patching - AC Deep	\$17
LNSG::LLNS ST::70	ILLINOIS STREET	SHERMAN STREET	STEWART AVENUE	6,269	L & T CR	2.0%	Crack Sealing - AC	\$128
LNSG::LK ST::20	LAKE STREET	WILLIAM STREET	WENTWORTH AVENUE	19,939	BLOCK CR	10.0%	Crack Sealing - AC	\$608
LNSG::LK ST::20	LAKE STREET	WILLIAM STREET	WENTWORTH AVENUE	19,939	L & T CR	1.3%	Crack Sealing - AC	\$250
LNSG::LK ST::20	LAKE STREET	WILLIAM STREET	WENTWORTH AVENUE	19,939	ALLIGATOR CR	0.2%	Patching - AC Deep	\$833
LNSG::LK ST::20	LAKE STREET	WILLIAM STREET	WENTWORTH AVENUE	19,939	RUTTING	0.1%	Patching - AC Shallow	\$57
LNSG::LK ST::40	LAKE STREET	LANGE STREET	MAPLE STREET	12,456	BLOCK CR	0.7%	Crack Sealing - AC	\$26
LNSG::LK ST::40	LAKE STREET	LANGE STREET	MAPLE STREET	12,456	L & T CR	0.9%	Crack Sealing - AC	\$107
LNSG::LNG ST::40	LANGE STREET	TERRACE DRIVE	LANGE STREET	15,170	L & T CR	1.8%	Crack Sealing - AC	\$266
LNSG::LNG ST::40	LANGE STREET	TERRACE DRIVE	LANGE STREET	15,170	ALLIGATOR CR	0.5%	Crack Sealing - AC	\$37
LNSG::LNG ST::40	LANGE STREET	TERRACE DRIVE	LANGE STREET	15,170	ALLIGATOR CR	0.8%	Patching - AC Deep	\$1,862
LNSG::LNG ST::40	LANGE STREET	TERRACE DRIVE	LANGE STREET	15,170	RUTTING	0.0%	Patching - AC Shallow	\$26
LNSG::LNG ST::60	LANGE STREET	RIDGE ROAD	TERRACE DRIVE	25,205	RUTTING	0.0%	Patching - AC Shallow	\$59
LNSG::LNG ST::70	LANGE STREET	RIDGE ROAD	LAKE STREET	15,374	ALLIGATOR CR	0.6%	Crack Sealing - AC	\$43
LNSG::LNG ST::70	LANGE STREET	RIDGE ROAD	LAKE STREET	15,374	L & T CR	5.1%	Crack Sealing - AC	\$786
LNSG::LNG ST::70	LANGE STREET	RIDGE ROAD	LAKE STREET	15,374	L & T CR	0.4%	Crack Sealing - AC	\$53
LNSG::LNG ST::70	LANGE STREET	RIDGE ROAD	LAKE STREET	15,374	ALLIGATOR CR	0.6%	Patching - AC Deep	\$1,543

Pavement ID	Road Name	From	To	Area	Distress Type	Density	Maint. Activity	Cost
LNSG::LNG ST::80	LANGE STREET	LAKE STREET	RANDOLPH STREET	9,741	L & T CR	3.5%	Crack Sealing - AC	\$342
LNSG::LCST ST::100	LOCUST STREET	177TH STREET	178TH STREET	19,814	ALLIGATOR CR	1.5%	Crack Sealing - AC	\$111
LNSG::LCST ST::100	LOCUST STREET	177TH STREET	178TH STREET	19,814	L & T CR	1.0%	Crack Sealing - AC	\$204
LNSG::LCST ST::110	LOCUST STREET	176TH PLACE	177TH STREET	9,463	L & T CR	1.2%	Crack Sealing - AC	\$116
LNSG::LCST ST::110	LOCUST STREET	176TH PLACE	177TH STREET	9,463	ALLIGATOR CR	2.8%	Crack Sealing - AC	\$101
LNSG::LCST ST::110	LOCUST STREET	176TH PLACE	177TH STREET	9,463	RUTTING	0.1%	Patching - AC Shallow	\$36
LNSG::LRNZ AVE::20	LORENZ AVENUE	RIDGE ROAD	181ST STREET	18,053	L & T CR	0.5%	Crack Sealing - AC	\$83
LNSG::LRNZ AVE::20	LORENZ AVENUE	RIDGE ROAD	181ST STREET	18,053	ALLIGATOR CR	0.9%	Crack Sealing - AC	\$64
LNSG::LRNZ AVE::20	LORENZ AVENUE	RIDGE ROAD	181ST STREET	18,053	L & T CR	2.8%	Crack Sealing - AC	\$503
LNSG::LRNZ AVE::20	LORENZ AVENUE	RIDGE ROAD	181ST STREET	18,053	ALLIGATOR CR	0.1%	Patching - AC Deep	\$423
LNSG::LRNZ AVE::20	LORENZ AVENUE	RIDGE ROAD	181ST STREET	18,053	RUTTING	0.0%	Patching - AC Shallow	\$19
LNSG::LRNZ AVE::30	LORENZ AVENUE	181ST STREET	180TH STREET	18,699	L & T CR	2.9%	Crack Sealing - AC	\$550
LNSG::LRNZ AVE::30	LORENZ AVENUE	181ST STREET	180TH STREET	18,699	ALLIGATOR CR	0.6%	Crack Sealing - AC	\$51
LNSG::LRNZ AVE::40	LORENZ AVENUE	180TH STREET	179TH STREET	18,338	L & T CR	2.0%	Crack Sealing - AC	\$370
LNSG::LRNZ AVE::40	LORENZ AVENUE	180TH STREET	179TH STREET	18,338	ALLIGATOR CR	1.6%	Patching - AC Deep	\$3,934
LNSG::LRNZ AVE::50	LORENZ AVENUE	179TH STREET	178TH STREET	19,394	L & T CR	0.2%	Crack Sealing - AC	\$44
LNSG::LRNZ AVE::50	LORENZ AVENUE	179TH STREET	178TH STREET	19,394	L & T CR	3.1%	Crack Sealing - AC	\$593
LNSG::LRNZ AVE::50	LORENZ AVENUE	179TH STREET	178TH STREET	19,394	ALLIGATOR CR	2.1%	Patching - AC Deep	\$5,372
LNSG::LRNZ AVE::90	LORENZ AVENUE	173RD STREET	172ND STREET	18,504	BLOCK CR	22.1%	Crack Sealing - AC	\$1,244
LNSG::LRNZ AVE::90	LORENZ AVENUE	173RD STREET	172ND STREET	18,504	L & T CR	4.9%	Crack Sealing - AC	\$899
LNSG::LRNZ AVE::110	LORENZ AVENUE	171ST STREET	170TH STREET	18,377	BLOCK CR	8.8%	Crack Sealing - AC	\$493
LNSG::LRNZ AVE::110	LORENZ AVENUE	171ST STREET	170TH STREET	18,377	L & T CR	4.0%	Crack Sealing - AC	\$732
LNSG::MPL AVE::40	MAPLE AVENUE	177TH STREET	176TH PLACE	10,457	L & T CR	2.9%	Crack Sealing - AC	\$303
LNSG::MPL AVE::40	MAPLE AVENUE	177TH STREET	176TH PLACE	10,457	EDGE CR	0.7%	Crack Sealing - AC	\$71
LNSG::MPL AVE::40	MAPLE AVENUE	177TH STREET	176TH PLACE	10,457	ALLIGATOR CR	1.4%	Crack Sealing - AC	\$59
LNSG::MPL AVE::50	MAPLE AVENUE	177TH PLACE	177TH STREET	9,190	L & T CR	3.6%	Crack Sealing - AC	\$327
LNSG::MPL AVE::50	MAPLE AVENUE	177TH PLACE	177TH STREET	9,190	ALLIGATOR CR	1.1%	Crack Sealing - AC	\$45
LNSG::MPL AVE::50	MAPLE AVENUE	177TH PLACE	177TH STREET	9,190	EDGE CR	0.5%	Crack Sealing - AC	\$45
LNSG::MPL AVE::50	MAPLE AVENUE	177TH PLACE	177TH STREET	9,190	ALLIGATOR CR	0.8%	Patching - AC Deep	\$1,285
LNSG::MPL AVE::60	MAPLE AVENUE	178TH STREET	177TH PLACE	9,291	L & T CR	1.8%	Crack Sealing - AC	\$171
LNSG::MPL AVE::60	MAPLE AVENUE	178TH STREET	177TH PLACE	9,291	ALLIGATOR CR	1.0%	Crack Sealing - AC	\$40
LNSG::MPL AVE::60	MAPLE AVENUE	178TH STREET	177TH PLACE	9,291	L & T CR	0.6%	Crack Sealing - AC	\$57
LNSG::MPL AVE::60	MAPLE AVENUE	178TH STREET	177TH PLACE	9,291	RUTTING	0.0%	Patching - AC Shallow	\$17
LNSG::MPL ST::20	MAPLE STREET	TERRACE DRIVE	ILLI INDI DRIVE	24,447	ALLIGATOR CR	0.8%	Crack Sealing - AC	\$77
LNSG::MPL ST::20	MAPLE STREET	TERRACE DRIVE	ILLI INDI DRIVE	24,447	L & T CR	0.6%	Crack Sealing - AC	\$145
LNSG::MPL ST::20	MAPLE STREET	TERRACE DRIVE	ILLI INDI DRIVE	24,447	L & T CR	2.6%	Crack Sealing - AC	\$644
LNSG::MPL ST::20	MAPLE STREET	TERRACE DRIVE	ILLI INDI DRIVE	24,447	ALLIGATOR CR	2.8%	Patching - AC Deep	\$8,673
LNSG::MPL ST::40	MAPLE STREET	TERRACE DRIVE	TERRACE DRIVE	979	L & T CR	2.7%	Crack Sealing - AC	\$27
LNSG::MPL ST::40	MAPLE STREET	TERRACE DRIVE	TERRACE DRIVE	979	ALLIGATOR CR	0.4%	Crack Sealing - AC	\$5
LNSG::MPL ST::50	MAPLE STREET	TERRACE DRIVE	TERRACE DRIVE	2,019	ALLIGATOR CR	1.0%	Crack Sealing - AC	\$13
LNSG::MPL ST::50	MAPLE STREET	TERRACE DRIVE	TERRACE DRIVE	2,019	L & T CR	1.4%	Crack Sealing - AC	\$28

Pavement ID	Road Name	From	To	Area	Distress Type	Density	Maint. Activity	Cost
LNSG::MPL ST::60	MAPLE STREET	TERRACE DRIVE	TERRACE DRIVE	1,134	L & T CR	0.4%	Crack Sealing - AC	\$4
LNSG::MPL ST::60	MAPLE STREET	TERRACE DRIVE	TERRACE DRIVE	1,134	ALLIGATOR CR	1.0%	Patching - AC Deep	\$322
LNSG::MPL ST::80	MAPLE STREET	RIDGE ROAD	LAKE STREET	17,453	L & T CR	4.5%	Crack Sealing - AC	\$787
LNSG::MPL ST::80	MAPLE STREET	RIDGE ROAD	LAKE STREET	17,453	ALLIGATOR CR	0.3%	Crack Sealing - AC	\$26
LNSG::MPL ST::80	MAPLE STREET	RIDGE ROAD	LAKE STREET	17,453	ALLIGATOR CR	1.1%	Patching - AC Deep	\$2,727
LNSG::MPL ST::90	MAPLE STREET	LAKE STREET	RANDOLPH STREET	10,197	L & T CR	4.8%	Crack Sealing - AC	\$493
LNSG::MPL ST::90	MAPLE STREET	LAKE STREET	RANDOLPH STREET	10,197	ALLIGATOR CR	1.1%	Crack Sealing - AC	\$49
LNSG::MPL ST::90	MAPLE STREET	LAKE STREET	RANDOLPH STREET	10,197	ALLIGATOR CR	0.8%	Patching - AC Deep	\$1,299
LNSG::MRTL CT::10	MERTLE COURT	CENTRAL STREET	WENTWORTH AVENUE	13,713	L & T CR	0.4%	Crack Sealing - AC	\$47
LNSG::MRTL CT::10	MERTLE COURT	CENTRAL STREET	WENTWORTH AVENUE	13,713	ALLIGATOR CR	0.8%	Crack Sealing - AC	\$46
LNSG::MRTL CT::10	MERTLE COURT	CENTRAL STREET	WENTWORTH AVENUE	13,713	L & T CR	5.3%	Crack Sealing - AC	\$727
LNSG::MRTL CT::10	MERTLE COURT	CENTRAL STREET	WENTWORTH AVENUE	13,713	ALLIGATOR CR	0.3%	Patching - AC Deep	\$841
LNSG::MRTL CT::10	MERTLE COURT	CENTRAL STREET	WENTWORTH AVENUE	13,713	RUTTING	0.0%	Patching - AC Shallow	\$20
LNSG::MLLR DR::20	MILLER DRIVE	ANN STREET	SCHULTZ DRIVE N	10,553	ALLIGATOR CR	0.2%	Crack Sealing - AC	\$13
LNSG::MLLR DR::20	MILLER DRIVE	ANN STREET	SCHULTZ DRIVE N	10,553	L & T CR	0.5%	Crack Sealing - AC	\$49
LNSG::MLLR DR::20	MILLER DRIVE	ANN STREET	SCHULTZ DRIVE N	10,553	ALLIGATOR CR	1.2%	Patching - AC Deep	\$1,883
LNSG::MNR ST::20	MONROE STREET	WILLIAM STREET	WENTWORTH AVENUE	19,580	L & T CR	5.4%	Crack Sealing - AC	\$1,056
LNSG::MNR ST::20	MONROE STREET	WILLIAM STREET	WENTWORTH AVENUE	19,580	RUTTING	0.0%	Patching - AC Shallow	\$22
LNSG::OK AVE::80	OAK AVENUE	BERNICE ROAD	BERNICE AVENUE	13,060	L & T CR	4.7%	Crack Sealing - AC	\$619
LNSG::OK AVE::100	OAK AVENUE	OAK AVENUE	173RD STREET	1,706	L & T CR	0.7%	Crack Sealing - AC	\$11
LNSG::OK AVE::100	OAK AVENUE	OAK AVENUE	173RD STREET	1,706	L & T CR	0.7%	Crack Sealing - AC	\$12
LNSG::OK AVE::100	OAK AVENUE	OAK AVENUE	173RD STREET	1,706	ALLIGATOR CR	1.1%	Crack Sealing - AC	\$13
LNSG::OK AVE::110	OAK AVENUE	173RD STREET	172ND STREET	18,316	ALLIGATOR CR	2.8%	Crack Sealing - AC	\$188
LNSG::OK AVE::110	OAK AVENUE	173RD STREET	172ND STREET	18,316	L & T CR	0.1%	Crack Sealing - AC	\$21
LNSG::OK ST::20	OAK STREET	182ND PLACE	END	6,183	L & T CR	2.0%	Crack Sealing - AC	\$126
LNSG::OK ST::20	OAK STREET	182ND PLACE	END	6,183	RUTTING	0.1%	Patching - AC Shallow	\$31
LNSG::OK ST::30	OAK STREET	182ND PLACE	OAK STREET	3,056	L & T CR	1.4%	Crack Sealing - AC	\$42
LNSG::OK ST::30	OAK STREET	182ND PLACE	OAK STREET	3,056	L & T CR	0.4%	Crack Sealing - AC	\$11
LNSG::OK ST::30	OAK STREET	182ND PLACE	OAK STREET	3,056	ALLIGATOR CR	0.8%	Patching - AC Deep	\$532
LNSG::OK ST::30	OAK STREET	182ND PLACE	OAK STREET	3,056	RUTTING	0.8%	Patching - AC Shallow	\$129
LNSG::OK ST::40	OAK STREET	OAK STREET	RIDGE ROAD	7,641	ALLIGATOR CR	2.9%	Crack Sealing - AC	\$86
LNSG::OK ST::40	OAK STREET	OAK STREET	RIDGE ROAD	7,641	L & T CR	0.7%	Crack Sealing - AC	\$51
LNSG::OK ST::40	OAK STREET	OAK STREET	RIDGE ROAD	7,641	L & T CR	2.1%	Crack Sealing - AC	\$159
LNSG::KLY AVE::30	OAKLEY AVENUE	183RD PLACE	END	5,601	L & T CR	4.1%	Crack Sealing - AC	\$228
LNSG::KLY AVE::40	OAKLEY AVENUE	183RD STREET	183RD PLACE	9,280	L & T CR	3.1%	Crack Sealing - AC	\$287
LNSG::KLY AVE::40	OAKLEY AVENUE	183RD STREET	183RD PLACE	9,280	ALLIGATOR CR	0.5%	Crack Sealing - AC	\$23
LNSG::KLY AVE::40	OAKLEY AVENUE	183RD STREET	183RD PLACE	9,280	L & T CR	0.8%	Crack Sealing - AC	\$75
LNSG::KLY AVE::40	OAKLEY AVENUE	183RD STREET	183RD PLACE	9,280	ALLIGATOR CR	0.7%	Patching - AC Deep	\$1,085
LNSG::KLY AVE::40	OAKLEY AVENUE	183RD STREET	183RD PLACE	9,280	RUTTING	0.1%	Patching - AC Shallow	\$47
LNSG::KLY AVE::50	OAKLEY AVENUE	183RD STREET	183RD STREET	7,034	ALLIGATOR CR	3.3%	Crack Sealing - AC	\$91
LNSG::KLY AVE::50	OAKLEY AVENUE	183RD STREET	183RD STREET	7,034	L & T CR	2.7%	Crack Sealing - AC	\$192

Pavement ID	Road Name	From	To	Area	Distress Type	Density	Maint. Activity	Cost
LNSG::KLY AVE::60	OAKLEY AVENUE	183RD STREET	182ND PLACE	1,739	ALLIGATOR CR	0.4%	Crack Sealing - AC	\$6
LNSG::KLY AVE::70	OAKLEY AVENUE	182ND PLACE	RIDGE ROAD	9,798	ALLIGATOR CR	1.0%	Crack Sealing - AC	\$43
LNSG::KLY AVE::70	OAKLEY AVENUE	182ND PLACE	RIDGE ROAD	9,798	L & T CR	1.2%	Crack Sealing - AC	\$117
LNSG::KLY AVE::70	OAKLEY AVENUE	182ND PLACE	RIDGE ROAD	9,798	ALLIGATOR CR	1.8%	Patching - AC Deep	\$2,548
LNSG::KWD AVE::100	OAKWOOD AVENUE	188TH PLACE	188TH STREET	7,142	ALLIGATOR CR	0.5%	Crack Sealing - AC	\$20
LNSG::KWD AVE::100	OAKWOOD AVENUE	188TH PLACE	188TH STREET	7,142	L & T CR	2.1%	Crack Sealing - AC	\$147
LNSG::KWD AVE::100	OAKWOOD AVENUE	188TH PLACE	188TH STREET	7,142	L & T CR	0.7%	Crack Sealing - AC	\$49
LNSG::KWD AVE::100	OAKWOOD AVENUE	188TH PLACE	188TH STREET	7,142	ALLIGATOR CR	0.3%	Patching - AC Deep	\$427
LNSG::KWD AVE::140	OAKWOOD AVENUE	185TH STREET	184TH STREET	18,554	RUTTING	0.9%	Patching - AC Shallow	\$869
LNSG::KWD AVE::200	OAKWOOD AVENUE	179TH STREET	178TH STREET	18,302	ALLIGATOR CR	0.1%	Crack Sealing - AC	\$8
LNSG::KWD AVE::200	OAKWOOD AVENUE	179TH STREET	178TH STREET	18,302	L & T CR	3.4%	Crack Sealing - AC	\$623
LNSG::KWD AVE::200	OAKWOOD AVENUE	179TH STREET	178TH STREET	18,302	L & T CR	0.2%	Crack Sealing - AC	\$42
LNSG::KWD AVE::200	OAKWOOD AVENUE	179TH STREET	178TH STREET	18,302	ALLIGATOR CR	1.0%	Patching - AC Deep	\$2,698
LNSG::KWD AVE::240	OAKWOOD AVENUE	171ST STREET	170TH STREET	18,341	BLOCK CR	3.2%	Crack Sealing - AC	\$180
LNSG::KWD AVE::240	OAKWOOD AVENUE	171ST STREET	170TH STREET	18,341	L & T CR	5.8%	Crack Sealing - AC	\$1,062
LNSG::KWD AVE::240	OAKWOOD AVENUE	171ST STREET	170TH STREET	18,341	RUTTING	0.1%	Patching - AC Shallow	\$54
LNSG::PRK AVE::20	PARK AVENUE	194TH PLACE	195TH COURT	8,898	BLOCK CR	6.6%	Patching - AC Shallow	\$3,247
LNSG::PRK AVE::110	PARK AVENUE	179TH STREET	178TH STREET	18,544	L & T CR	0.3%	Crack Sealing - AC	\$63
LNSG::PRK AVE::110	PARK AVENUE	179TH STREET	178TH STREET	18,544	L & T CR	2.5%	Crack Sealing - AC	\$464
LNSG::PRK AVE::110	PARK AVENUE	179TH STREET	178TH STREET	18,544	ALLIGATOR CR	1.1%	Crack Sealing - AC	\$83
LNSG::PXTN AVE::10	PAXTON AVENUE	START	END	7,710	ALLIGATOR CR	1.1%	Crack Sealing - AC	\$39
LNSG::PXTN AVE::30	PAXTON AVENUE	177TH PLACE	177TH STREET	11,725	ALLIGATOR CR	0.7%	Crack Sealing - AC	\$36
LNSG::PXTN AVE::30	PAXTON AVENUE	177TH PLACE	177TH STREET	11,725	L & T CR	0.4%	Crack Sealing - AC	\$41
LNSG::PXTN AVE::30	PAXTON AVENUE	177TH PLACE	177TH STREET	11,725	ALLIGATOR CR	3.0%	Patching - AC Deep	\$4,763
LNSG::RNDLPH ST::07	RANDOLPH STREET	181ST PLACE	HENRY STREET	11,330	L & T CR	0.2%	Crack Sealing - AC	\$20
LNSG::RNDLPH ST::10	RANDOLPH STREET	HENRY STREET	WILLIAM STREET	17,554	L & T CR	4.0%	Crack Sealing - AC	\$693
LNSG::RNDLPH ST::20	RANDOLPH STREET	WILLIAM STREET	WENTWORTH AVENUE	19,849	L & T CR	3.6%	Crack Sealing - AC	\$718
LNSG::RNDLPH ST::20	RANDOLPH STREET	WILLIAM STREET	WENTWORTH AVENUE	19,849	RUTTING	0.1%	Patching - AC Deep	\$120
LNSG::RNDLPH ST::20	RANDOLPH STREET	WILLIAM STREET	WENTWORTH AVENUE	19,849	RUTTING	0.0%	Patching - AC Shallow	\$20
LNSG::RNDLPH ST::40	RANDOLPH STREET	LANGE STREET	MAPLE STREET	12,341	L & T CR	5.6%	Crack Sealing - AC	\$687
LNSG::RNDLPH ST::40	RANDOLPH STREET	LANGE STREET	MAPLE STREET	12,341	ALLIGATOR CR	0.4%	Patching - AC Deep	\$962
LNSG::RDG RD::30	RIDGE ROAD	FRED STREET	GLEN TERRACE	4,376	ALLIGATOR CR	0.1%	Crack Sealing - AC	\$5
LNSG::RDG RD::30	RIDGE ROAD	FRED STREET	GLEN TERRACE	4,376	BLOCK CR	25.6%	Crack Sealing - AC	\$342
LNSG::RDG RD::30	RIDGE ROAD	FRED STREET	GLEN TERRACE	4,376	L & T CR	0.4%	Crack Sealing - AC	\$15
LNSG::RDG RD::30	RIDGE ROAD	FRED STREET	GLEN TERRACE	4,376	L & T CR	0.7%	Crack Sealing - AC	\$30
LNSG::RDG RD::30	RIDGE ROAD	FRED STREET	GLEN TERRACE	4,376	ALLIGATOR CR	0.7%	Patching - AC Deep	\$652
LNSG::RDG RD::80	RIDGE ROAD	SCHOOL STREET	WILDWOOD AVENUE	12,809	L & T CR	1.3%	Crack Sealing - AC	\$166
LNSG::RDG RD::80	RIDGE ROAD	SCHOOL STREET	WILDWOOD AVENUE	12,809	L & T CR	2.9%	Crack Sealing - AC	\$369
LNSG::RDG RD::80	RIDGE ROAD	SCHOOL STREET	WILDWOOD AVENUE	12,809	ALLIGATOR CR	1.2%	Crack Sealing - AC	\$63
LNSG::RDG RD::80	RIDGE ROAD	SCHOOL STREET	WILDWOOD AVENUE	12,809	ALLIGATOR CR	1.1%	Patching - AC Deep	\$2,095
LNSG::RDG RD::80	RIDGE ROAD	SCHOOL STREET	WILDWOOD AVENUE	12,809	RUTTING	0.1%	Patching - AC Shallow	\$34

Pavement ID	Road Name	From	To	Area	Distress Type	Density	Maint. Activity	Cost
LNSG::RDG RD::100	RIDGE ROAD	OAKWOOD AVENUE	RIDGEWOOD AVENUE	13,465	BLOCK CR	31.0%	Crack Sealing - AC	\$1,271
LNSG::RDG RD::100	RIDGE ROAD	OAKWOOD AVENUE	RIDGEWOOD AVENUE	13,465	L & T CR	0.8%	Crack Sealing - AC	\$111
LNSG::RDG RD::100	RIDGE ROAD	OAKWOOD AVENUE	RIDGEWOOD AVENUE	13,465	ALLIGATOR CR	0.9%	Crack Sealing - AC	\$52
LNSG::RDG RD::100	RIDGE ROAD	OAKWOOD AVENUE	RIDGEWOOD AVENUE	13,465	L & T CR	1.6%	Crack Sealing - AC	\$220
LNSG::RDG RD::100	RIDGE ROAD	OAKWOOD AVENUE	RIDGEWOOD AVENUE	13,465	ALLIGATOR CR	0.7%	Patching - AC Deep	\$1,424
LNSG::RDG RD::100	RIDGE ROAD	OAKWOOD AVENUE	RIDGEWOOD AVENUE	13,465	RUTTING	0.2%	Patching - AC Shallow	\$123
LNSG::RDG RD::110	RIDGE ROAD	RIDGEWOOD AVENUE	CHICAGO AVENUE	13,145	L & T CR	2.9%	Crack Sealing - AC	\$375
LNSG::RDG RD::110	RIDGE ROAD	RIDGEWOOD AVENUE	CHICAGO AVENUE	13,145	L & T CR	1.6%	Crack Sealing - AC	\$205
LNSG::RDG RD::110	RIDGE ROAD	RIDGEWOOD AVENUE	CHICAGO AVENUE	13,145	ALLIGATOR CR	0.4%	Crack Sealing - AC	\$26
LNSG::RDG RD::110	RIDGE ROAD	RIDGEWOOD AVENUE	CHICAGO AVENUE	13,145	ALLIGATOR CR	2.5%	Patching - AC Deep	\$4,419
LNSG::RDG RD::110	RIDGE ROAD	RIDGEWOOD AVENUE	CHICAGO AVENUE	13,145	RUTTING	0.0%	Patching - AC Shallow	\$28
LNSG::RDG RD::120	RIDGE ROAD	CHICAGO AVENUE	PARK AVENUE	13,461	ALLIGATOR CR	2.3%	Crack Sealing - AC	\$117
LNSG::RDG RD::120	RIDGE ROAD	CHICAGO AVENUE	PARK AVENUE	13,461	L & T CR	1.4%	Crack Sealing - AC	\$191
LNSG::RDG RD::120	RIDGE ROAD	CHICAGO AVENUE	PARK AVENUE	13,461	L & T CR	2.8%	Crack Sealing - AC	\$377
LNSG::RDG RD::120	RIDGE ROAD	CHICAGO AVENUE	PARK AVENUE	13,461	ALLIGATOR CR	0.8%	Patching - AC Deep	\$1,674
LNSG::RDG RD::130	RIDGE ROAD	PARK AVENUE	OAK STREET	3,114	L & T CR	1.7%	Crack Sealing - AC	\$53
LNSG::RDG RD::130	RIDGE ROAD	PARK AVENUE	OAK STREET	3,114	L & T CR	0.5%	Crack Sealing - AC	\$16
LNSG::RDG RD::130	RIDGE ROAD	PARK AVENUE	OAK STREET	3,114	ALLIGATOR CR	0.4%	Crack Sealing - AC	\$10
LNSG::RDG RD::130	RIDGE ROAD	PARK AVENUE	OAK STREET	3,114	ALLIGATOR CR	0.5%	Patching - AC Deep	\$367
LNSG::RDG RD::160	RIDGE ROAD	BURNHAM AVENUE	WALTER STREET	20,436	ALLIGATOR CR	0.8%	Crack Sealing - AC	\$69
LNSG::RDG RD::160	RIDGE ROAD	BURNHAM AVENUE	WALTER STREET	20,436	L & T CR	0.4%	Crack Sealing - AC	\$74
LNSG::RDG RD::160	RIDGE ROAD	BURNHAM AVENUE	WALTER STREET	20,436	L & T CR	3.1%	Crack Sealing - AC	\$637
LNSG::RDG RD::160	RIDGE ROAD	BURNHAM AVENUE	WALTER STREET	20,436	EDGE CR	0.8%	Crack Sealing - AC	\$153
LNSG::RDG RD::160	RIDGE ROAD	BURNHAM AVENUE	WALTER STREET	20,436	RUTTING	0.0%	Patching - AC Shallow	\$21
LNSG::RDG RD::230	RIDGE ROAD	WILLIAM STREET	WEST STREET	2,983	L & T CR	1.2%	Crack Sealing - AC	\$36
LNSG::RDG RD::230	RIDGE ROAD	WILLIAM STREET	WEST STREET	2,983	L & T CR	0.7%	Crack Sealing - AC	\$20
LNSG::RDG RD::230	RIDGE ROAD	WILLIAM STREET	WEST STREET	2,983	ALLIGATOR CR	1.1%	Patching - AC Deep	\$641
LNSG::RDG RD::250	RIDGE ROAD	WENTWORTH AVENUE	LANGE STREET	19,029	L & T CR	1.3%	Crack Sealing - AC	\$243
LNSG::RDG RD::250	RIDGE ROAD	WENTWORTH AVENUE	LANGE STREET	19,029	ALLIGATOR CR	0.4%	Crack Sealing - AC	\$34
LNSG::RDG RD::250	RIDGE ROAD	WENTWORTH AVENUE	LANGE STREET	19,029	L & T CR	1.1%	Crack Sealing - AC	\$208
LNSG::RDG RD::250	RIDGE ROAD	WENTWORTH AVENUE	LANGE STREET	19,029	ALLIGATOR CR	0.8%	Patching - AC Deep	\$2,365
LNSG::RDG RD::250	RIDGE ROAD	WENTWORTH AVENUE	LANGE STREET	19,029	RUTTING	0.0%	Patching - AC Deep	\$15
LNSG::RDG RD::250	RIDGE ROAD	WENTWORTH AVENUE	LANGE STREET	19,029	RUTTING	0.1%	Patching - AC Shallow	\$140
LNSG::RDG RD::260	RIDGE ROAD	LANGE STREET	MAPLE STREET	20,621	ALLIGATOR CR	0.7%	Crack Sealing - AC	\$56
LNSG::RDG RD::260	RIDGE ROAD	LANGE STREET	MAPLE STREET	20,621	L & T CR	1.1%	Crack Sealing - AC	\$231
LNSG::RDG RD::260	RIDGE ROAD	LANGE STREET	MAPLE STREET	20,621	L & T CR	1.9%	Crack Sealing - AC	\$391
LNSG::RDG RD::260	RIDGE ROAD	LANGE STREET	MAPLE STREET	20,621	EDGE CR	0.1%	Crack Sealing - AC	\$18
LNSG::RDG RD::260	RIDGE ROAD	LANGE STREET	MAPLE STREET	20,621	ALLIGATOR CR	0.7%	Patching - AC Deep	\$2,199
LNSG::RDG RD::260	RIDGE ROAD	LANGE STREET	MAPLE STREET	20,621	RUTTING	0.0%	Patching - AC Shallow	\$46
LNSG::RDGWD AVE::30	RIDGEWOOD AVENUE	189TH STREET	188TH PLACE	6,518	BLOCK CR	85.8%	Crack Sealing - AC	\$1,705
LNSG::RDGWD AVE::40	RIDGEWOOD AVENUE	186TH STREET	185TH STREET	18,373	RUTTING	0.3%	Patching - AC Shallow	\$289

Pavement ID	Road Name	From	To	Area	Distress Type	Density	Maint. Activity	Cost
LNSG::RDGWD AVE::100	RIDGEWOOD AVENUE	180TH STREET	179TH STREET	18,531	L & T CR	1.8%	Crack Sealing - AC	\$335
LNSG::RDGWD AVE::100	RIDGEWOOD AVENUE	180TH STREET	179TH STREET	18,531	L & T CR	0.1%	Crack Sealing - AC	\$21
LNSG::RDGWD AVE::100	RIDGEWOOD AVENUE	180TH STREET	179TH STREET	18,531	ALLIGATOR CR	0.6%	Crack Sealing - AC	\$49
LNSG::RDGWD AVE::110	RIDGEWOOD AVENUE	179TH STREET	178TH STREET	18,574	ALLIGATOR CR	0.3%	Crack Sealing - AC	\$26
LNSG::RDGWD AVE::110	RIDGEWOOD AVENUE	179TH STREET	178TH STREET	18,574	L & T CR	2.0%	Crack Sealing - AC	\$371
LNSG::RS AVE::40	ROSE AVENUE	THORNTON LANSING ROA	END	38,736	L & T CR	2.7%	Crack Sealing - AC	\$1,044
LNSG::RS AVE::40	ROSE AVENUE	THORNTON LANSING ROA	END	38,736	ALLIGATOR CR	0.4%	Crack Sealing - AC	\$57
LNSG::RS AVE::40	ROSE AVENUE	THORNTON LANSING ROA	END	38,736	RUTTING	0.0%	Patching - AC Shallow	\$36
LNSG::RY ST::50	ROY STREET	RIDGE ROAD	181ST PLACE	13,412	L & T CR	2.7%	Crack Sealing - AC	\$362
LNSG::RY ST::60	ROY STREET	181ST PLACE	181ST STREET	6,260	L & T CR	0.7%	Crack Sealing - AC	\$43
LNSG::RY ST::110	ROY STREET	178TH STREET	177TH STREET	18,526	BLOCK CR	8.4%	Crack Sealing - AC	\$472
LNSG::RY ST::110	ROY STREET	178TH STREET	177TH STREET	18,526	L & T CR	5.2%	Crack Sealing - AC	\$961
LNSG::RY ST::120	ROY STREET	177TH STREET	176TH STREET	18,564	L & T CR	5.7%	Crack Sealing - AC	\$1,050
LNSG::RY ST::120	ROY STREET	177TH STREET	176TH STREET	18,564	RUTTING	0.0%	Patching - AC Shallow	\$18
LNSG::RY ST::130	ROY STREET	176TH STREET	175TH STREET	18,423	BLOCK CR	17.2%	Crack Sealing - AC	\$963
LNSG::RY ST::130	ROY STREET	176TH STREET	175TH STREET	18,423	L & T CR	4.5%	Crack Sealing - AC	\$834
LNSG::RY ST::140	ROY STREET	BERNICE ROAD	BERNICE AVENUE	11,547	L & T CR	5.8%	Crack Sealing - AC	\$664
LNSG::RY ST::140	ROY STREET	BERNICE ROAD	BERNICE AVENUE	11,547	ALLIGATOR CR	0.2%	Patching - AC Deep	\$487
LNSG::SLR LN::10	SAILOR LANE	183RD PLACE	183RD STREET	8,515	ALLIGATOR CR	0.6%	Crack Sealing - AC	\$27
LNSG::SLR LN::10	SAILOR LANE	183RD PLACE	183RD STREET	8,515	L & T CR	0.7%	Crack Sealing - AC	\$59
LNSG::SLR LN::10	SAILOR LANE	183RD PLACE	183RD STREET	8,515	L & T CR	3.1%	Crack Sealing - AC	\$265
LNSG::SCHL ST::40	SCHOOL STREET	183RD STREET	RIDGE ROAD	18,477	RUTTING	0.1%	Patching - AC Shallow	\$85
LNSG::SCHL ST::60	SCHOOL STREET	INDIANA AVENUE	181ST STREET	9,517	L & T CR	0.2%	Crack Sealing - AC	\$19
LNSG::SCHL ST::60	SCHOOL STREET	INDIANA AVENUE	181ST STREET	9,517	L & T CR	4.6%	Crack Sealing - AC	\$437
LNSG::SCHL ST::60	SCHOOL STREET	INDIANA AVENUE	181ST STREET	9,517	ALLIGATOR CR	0.9%	Crack Sealing - AC	\$39
LNSG::SCHL ST::60	SCHOOL STREET	INDIANA AVENUE	181ST STREET	9,517	ALLIGATOR CR	1.5%	Patching - AC Deep	\$2,085
LNSG::SCHL ST::70	SCHOOL STREET	181ST STREET	181ST STREET	2,419	L & T CR	0.4%	Crack Sealing - AC	\$8
LNSG::SCHL ST::70	SCHOOL STREET	181ST STREET	181ST STREET	2,419	ALLIGATOR CR	0.4%	Crack Sealing - AC	\$8
LNSG::SCHL ST::70	SCHOOL STREET	181ST STREET	181ST STREET	2,419	ALLIGATOR CR	0.8%	Patching - AC Deep	\$458
LNSG::SCHL ST::80	SCHOOL STREET	181ST STREET	180TH STREET	16,297	L & T CR	2.1%	Crack Sealing - AC	\$342
LNSG::SCHL ST::80	SCHOOL STREET	181ST STREET	180TH STREET	16,297	ALLIGATOR CR	0.1%	Crack Sealing - AC	\$13
LNSG::SCHL ST::80	SCHOOL STREET	181ST STREET	180TH STREET	16,297	ALLIGATOR CR	0.7%	Patching - AC Deep	\$1,759
LNSG::SCHL ST::100	SCHOOL STREET	179TH STREET	178TH STREET	18,202	L & T CR	2.7%	Crack Sealing - AC	\$495
LNSG::SCHL ST::100	SCHOOL STREET	179TH STREET	178TH STREET	18,202	ALLIGATOR CR	0.9%	Crack Sealing - AC	\$66
LNSG::SCHL ST::100	SCHOOL STREET	179TH STREET	178TH STREET	18,202	L & T CR	0.8%	Crack Sealing - AC	\$145
LNSG::SCHL ST::100	SCHOOL STREET	179TH STREET	178TH STREET	18,202	ALLIGATOR CR	1.2%	Patching - AC Deep	\$3,133
LNSG::SCHLZ DR N::40	SCHULTZ DRIVE	WALTER STREET	ROY STREET	10,663	ALLIGATOR CR	0.1%	Crack Sealing - AC	\$11
LNSG::SCHLZ DR N::40	SCHULTZ DRIVE	WALTER STREET	ROY STREET	10,663	ALLIGATOR CR	0.3%	Patching - AC Deep	\$714
LNSG::SCHLZ DR N::40	SCHULTZ DRIVE	WALTER STREET	ROY STREET	10,663	RUTTING	0.1%	Patching - AC Shallow	\$31
LNSG::SCHLZ DR N::50	SCHULTZ DRIVE	ROY STREET	ADA STREET	8,815	RUTTING	0.0%	Patching - AC Shallow	\$17
LNSG::SCHLZ DR N::80	SCHULTZ DRIVE	GRANT STREET	SHERMAN STREET	6,890	ALLIGATOR CR	0.6%	Crack Sealing - AC	\$21

Pavement ID	Road Name	From	To	Area	Distress Type	Density	Maint. Activity	Cost
LNSG::SCHLZ DR N::80	SCHULTZ DRIVE	GRANT STREET	SHERMAN STREET	6,890	ALLIGATOR CR	0.8%	Patching - AC Deep	\$968
LNSG::SCHLZ DR N::80	SCHULTZ DRIVE	GRANT STREET	SHERMAN STREET	6,890	RUTTING	0.2%	Patching - AC Deep	\$131
LNSG::SCHLZ DR N::80	SCHULTZ DRIVE	GRANT STREET	SHERMAN STREET	6,890	RUTTING	0.4%	Patching - AC Shallow	\$152
LNSG::SCHLZ DR N::90	SCHULTZ DRIVE	SHERMAN STREET	SHERMAN STREET	1,851	RUTTING	0.5%	Patching - AC Shallow	\$47
LNSG::SCHLZ DR N::100	SCHULTZ DRIVE	SHERMAN STREET	MILLER DRIVE	9,573	RUTTING	0.1%	Patching - AC Shallow	\$47
LNSG::SCHLZ DR S::30	SCHULTZ DRIVE S	ROY STREET	ADA STREET	9,000	RUTTING	0.0%	Patching - AC Deep	\$16
LNSG::SCHLZ DR S::30	SCHULTZ DRIVE S	ROY STREET	ADA STREET	9,000	RUTTING	0.4%	Patching - AC Shallow	\$202
LNSG::ST LN AVE::50	STATE LINE AVENUE	176TH PLACE	175TH STREET	18,654	L & T CR	0.1%	Crack Sealing - AC	\$23
LNSG::ST LN AVE::50	STATE LINE AVENUE	176TH PLACE	175TH STREET	18,654	L & T CR	1.3%	Crack Sealing - AC	\$233
LNSG::ST LN AVE::50	STATE LINE AVENUE	176TH PLACE	175TH STREET	18,654	ALLIGATOR CR	3.0%	Crack Sealing - AC	\$202
LNSG::ST LN AVE::50	STATE LINE AVENUE	176TH PLACE	175TH STREET	18,654	ALLIGATOR CR	1.8%	Patching - AC Deep	\$4,449
LNSG::ST LN AVE::50	STATE LINE AVENUE	176TH PLACE	175TH STREET	18,654	RUTTING	0.0%	Patching - AC Shallow	\$10
LNSG::STWRT AVE::10	STEWART AVENUE	GRANT STREET	SHERMAN STREET	14,469	L & T CR	1.1%	Crack Sealing - AC	\$161
LNSG::STWRT AVE::10	STEWART AVENUE	GRANT STREET	SHERMAN STREET	14,469	L & T CR	0.2%	Crack Sealing - AC	\$32
LNSG::STWRT AVE::20	STEWART AVENUE	SHERMAN STREET	ILLINOIS STREET	10,078	ALLIGATOR CR	0.2%	Crack Sealing - AC	\$12
LNSG::STN L AVE::50	STONY ISLAND AVENUE	181ST STREET	180TH STREET	15,229	L & T CR	0.4%	Crack Sealing - AC	\$58
LNSG::STN L AVE::50	STONY ISLAND AVENUE	181ST STREET	180TH STREET	15,229	L & T CR	3.6%	Crack Sealing - AC	\$551
LNSG::STN L AVE::50	STONY ISLAND AVENUE	181ST STREET	180TH STREET	15,229	ALLIGATOR CR	0.3%	Patching - AC Deep	\$861
LNSG::STN L AVE::60	STONY ISLAND AVENUE	180TH STREET	180TH STREET	2,653	L & T CR	0.4%	Crack Sealing - AC	\$9
LNSG::STN L AVE::60	STONY ISLAND AVENUE	180TH STREET	180TH STREET	2,653	L & T CR	0.4%	Crack Sealing - AC	\$9
LNSG::STN L AVE::70	STONY ISLAND AVENUE	180TH STREET	THORNTON LANSING ROAD	17,958	L & T CR	0.7%	Crack Sealing - AC	\$124
LNSG::STN L AVE::70	STONY ISLAND AVENUE	180TH STREET	THORNTON LANSING ROAD	17,958	ALLIGATOR CR	0.4%	Crack Sealing - AC	\$34
LNSG::TRRC DR::10	TERRACE DRIVE	WENTWORTH AVENUE	LANGE STREET	13,218	L & T CR	0.5%	Crack Sealing - AC	\$70
LNSG::TRRC DR::10	TERRACE DRIVE	WENTWORTH AVENUE	LANGE STREET	13,218	L & T CR	3.2%	Crack Sealing - AC	\$418
LNSG::TRRC DR::10	TERRACE DRIVE	WENTWORTH AVENUE	LANGE STREET	13,218	EDGE CR	0.1%	Crack Sealing - AC	\$14
LNSG::TRRC DR::10	TERRACE DRIVE	WENTWORTH AVENUE	LANGE STREET	13,218	ALLIGATOR CR	1.3%	Crack Sealing - AC	\$70
LNSG::TRRC DR::10	TERRACE DRIVE	WENTWORTH AVENUE	LANGE STREET	13,218	ALLIGATOR CR	0.2%	Patching - AC Deep	\$639
LNSG::TRRC DR::20	TERRACE DRIVE	LANGE STREET	MAPLE STREET	7,159	ALLIGATOR CR	0.5%	Crack Sealing - AC	\$21
LNSG::TRRC DR::20	TERRACE DRIVE	LANGE STREET	MAPLE STREET	7,159	L & T CR	3.0%	Crack Sealing - AC	\$218
LNSG::TRRC DR::20	TERRACE DRIVE	LANGE STREET	MAPLE STREET	7,159	EDGE CR	0.3%	Crack Sealing - AC	\$21
LNSG::THRTLNS RD::20	THORNTON-LANSING ROAD	GLEN OAK AVENUE	ARCADIA AVENUE	10,148	L & T CR	1.0%	Crack Sealing - AC	\$105
LNSG::THRTLNS RD::20	THORNTON-LANSING ROAD	GLEN OAK AVENUE	ARCADIA AVENUE	10,148	ALLIGATOR CR	0.4%	Crack Sealing - AC	\$19
LNSG::VLT RD::30	VIOLET ROAD	FOREST COURT	184TH STREET	3,652	ALLIGATOR CR	0.4%	Crack Sealing - AC	\$10
LNSG::WLTR ST::60	WALTER STREET	ILLINOIS STREET	RIDGE ROAD	26,448	ALLIGATOR CR	1.1%	Crack Sealing - AC	\$108
LNSG::WLTR ST::60	WALTER STREET	ILLINOIS STREET	RIDGE ROAD	26,448	L & T CR	4.1%	Crack Sealing - AC	\$1,077
LNSG::WSHGTN ST::10	WASHINGTON STREET	HENRY STREET	END	12,690	L & T CR	1.4%	Crack Sealing - AC	\$172
LNSG::WSHGTN ST::10	WASHINGTON STREET	HENRY STREET	END	12,690	PATCH/UT CUT	0.3%	Patching - AC Deep	\$682
LNSG::WSHGTN ST::10	WASHINGTON STREET	HENRY STREET	END	12,690	RUTTING	0.0%	Patching - AC Shallow	\$19
LNSG::WSHGTN ST::20	WASHINGTON STREET	HENRY STREET	WILLIAM STREET	17,596	L & T CR	4.9%	Crack Sealing - AC	\$870
LNSG::WSHGTN ST::20	WASHINGTON STREET	HENRY STREET	WILLIAM STREET	17,596	RUTTING	0.1%	Patching - AC Shallow	\$50
LNSG::WSHGTN ST::60	WASHINGTON STREET	MAPLE STREET	STATE LINE AVENUE	9,540	L & T CR	5.4%	Crack Sealing - AC	\$514

Pavement ID	Road Name	From	To	Area	Distress Type	Density	Maint. Activity	Cost
LNSG::WTFRD DR::10	WATERFORD DRIVE	170TH STREET	WATERFORD DRIVE	46,394	RUTTING	0.4%	Patching - AC Shallow	\$942
LNSG::WWRTH AVE::10	WENTWORTH AVENUE	GLENWOOD LANSING ROA	193RD PLACE	20,151	EDGE CR	0.1%	Crack Sealing - AC	\$19
LNSG::WWRTH AVE::10	WENTWORTH AVENUE	GLENWOOD LANSING ROA	193RD PLACE	20,151	ALLIGATOR CR	0.3%	Crack Sealing - AC	\$29
LNSG::WWRTH AVE::10	WENTWORTH AVENUE	GLENWOOD LANSING ROA	193RD PLACE	20,151	L & T CR	1.0%	Crack Sealing - AC	\$194
LNSG::WWRTH AVE::10	WENTWORTH AVENUE	GLENWOOD LANSING ROA	193RD PLACE	20,151	ALLIGATOR CR	0.5%	Patching - AC Deep	\$1,564
LNSG::WWRTH AVE::20	WENTWORTH AVENUE	193RD PLACE	192ND PLACE	21,060	L & T CR	0.5%	Crack Sealing - AC	\$95
LNSG::WWRTH AVE::20	WENTWORTH AVENUE	193RD PLACE	192ND PLACE	21,060	ALLIGATOR CR	0.1%	Crack Sealing - AC	\$12
LNSG::WWRTH AVE::20	WENTWORTH AVENUE	193RD PLACE	192ND PLACE	21,060	EDGE CR	0.1%	Crack Sealing - AC	\$13
LNSG::WWRTH AVE::20	WENTWORTH AVENUE	193RD PLACE	192ND PLACE	21,060	ALLIGATOR CR	0.0%	Patching - AC Deep	\$239
LNSG::WWRTH AVE::30	WENTWORTH AVENUE	192ND PLACE	192ND STREET	10,828	L & T CR	0.2%	Crack Sealing - AC	\$24
LNSG::WWRTH AVE::30	WENTWORTH AVENUE	192ND PLACE	192ND STREET	10,828	EDGE CR	0.1%	Crack Sealing - AC	\$11
LNSG::WWRTH AVE::30	WENTWORTH AVENUE	192ND PLACE	192ND STREET	10,828	ALLIGATOR CR	0.3%	Crack Sealing - AC	\$20
LNSG::WWRTH AVE::30	WENTWORTH AVENUE	192ND PLACE	192ND STREET	10,828	L & T CR	1.0%	Crack Sealing - AC	\$107
LNSG::WWRTH AVE::30	WENTWORTH AVENUE	192ND PLACE	192ND STREET	10,828	ALLIGATOR CR	0.7%	Patching - AC Deep	\$1,186
LNSG::WWRTH AVE::30	WENTWORTH AVENUE	192ND PLACE	192ND STREET	10,828	RUTTING	0.0%	Patching - AC Shallow	\$10
LNSG::WWRTH AVE::40	WENTWORTH AVENUE	192ND STREET	191ST PLACE	10,459	ALLIGATOR CR	0.7%	Crack Sealing - AC	\$34
LNSG::WWRTH AVE::40	WENTWORTH AVENUE	192ND STREET	191ST PLACE	10,459	L & T CR	0.8%	Crack Sealing - AC	\$83
LNSG::WWRTH AVE::40	WENTWORTH AVENUE	192ND STREET	191ST PLACE	10,459	ALLIGATOR CR	0.8%	Patching - AC Deep	\$1,301
LNSG::WWRTH AVE::50	WENTWORTH AVENUE	191ST PLACE	190TH PLACE	20,919	ALLIGATOR CR	0.1%	Crack Sealing - AC	\$17
LNSG::WWRTH AVE::50	WENTWORTH AVENUE	191ST PLACE	190TH PLACE	20,919	L & T CR	0.1%	Crack Sealing - AC	\$24
LNSG::WWRTH AVE::50	WENTWORTH AVENUE	191ST PLACE	190TH PLACE	20,919	ALLIGATOR CR	0.5%	Patching - AC Deep	\$1,545
LNSG::WWRTH AVE::50	WENTWORTH AVENUE	191ST PLACE	190TH PLACE	20,919	RUTTING	0.0%	Patching - AC Shallow	\$10
LNSG::WWRTH AVE::60	WENTWORTH AVENUE	190TH PLACE	S MANOR DRIVE	10,626	RUTTING	0.0%	Patching - AC Shallow	\$10
LNSG::WWRTH AVE::70	WENTWORTH AVENUE	S MANOR DRIVE	188TH STREET	45,750	L & T CR	0.5%	Crack Sealing - AC	\$216
LNSG::WWRTH AVE::70	WENTWORTH AVENUE	S MANOR DRIVE	188TH STREET	45,750	ALLIGATOR CR	0.4%	Crack Sealing - AC	\$72
LNSG::WWRTH AVE::70	WENTWORTH AVENUE	S MANOR DRIVE	188TH STREET	45,750	RUTTING	0.0%	Patching - AC Deep	\$41
LNSG::WWRTH AVE::70	WENTWORTH AVENUE	S MANOR DRIVE	188TH STREET	45,750	RUTTING	0.1%	Patching - AC Shallow	\$114
LNSG::WWRTH AVE::80	WENTWORTH AVENUE	188TH STREET	187TH STREET	23,569	L & T CR	0.5%	Crack Sealing - AC	\$106
LNSG::WWRTH AVE::80	WENTWORTH AVENUE	188TH STREET	187TH STREET	23,569	ALLIGATOR CR	0.2%	Crack Sealing - AC	\$20
LNSG::WWRTH AVE::90	WENTWORTH AVENUE	187TH STREET	186TH STREET	23,758	RUTTING	0.0%	Patching - AC Shallow	\$29
LNSG::WWRTH AVE::100	WENTWORTH AVENUE	186TH STREET	SCHULTZ DRIVE N	22,114	RUTTING	0.0%	Patching - AC Shallow	\$11
LNSG::WWRTH AVE::160	WENTWORTH AVENUE	CENTRAL STREET	RIDGE ROAD	7,398	ALLIGATOR CR	0.1%	Crack Sealing - AC	\$8
LNSG::WWRTH AVE::160	WENTWORTH AVENUE	CENTRAL STREET	RIDGE ROAD	7,398	L & T CR	1.0%	Crack Sealing - AC	\$75
LNSG::WWRTH AVE::160	WENTWORTH AVENUE	CENTRAL STREET	RIDGE ROAD	7,398	RUTTING	0.3%	Patching - AC Shallow	\$111
LNSG::WWRTH AVE::280	WENTWORTH AVENUE	176TH STREET	MAPLE AVENUE	12,719	L & T CR	1.5%	Crack Sealing - AC	\$193
LNSG::WWRTH AVE::280	WENTWORTH AVENUE	176TH STREET	MAPLE AVENUE	12,719	ALLIGATOR CR	0.6%	Crack Sealing - AC	\$33
LNSG::WWRTH AVE::280	WENTWORTH AVENUE	176TH STREET	MAPLE AVENUE	12,719	L & T CR	0.7%	Crack Sealing - AC	\$87
LNSG::WWRTH AVE::280	WENTWORTH AVENUE	176TH STREET	MAPLE AVENUE	12,719	ALLIGATOR CR	0.9%	Patching - AC Deep	\$1,828
LNSG::WSTFRNT RD::10	WEST FRONTAGE ROAD	186TH STREET	184TH STREET	35,943	RUTTING	0.1%	Patching - AC Shallow	\$95
LNSG::WSTFRNT RD::20	WEST FRONTAGE ROAD	184TH STREET	182ND STREET	37,505	RUTTING	0.1%	Patching - AC Shallow	\$175
LNSG::WSTFRNT RD::30	WEST FRONTAGE ROAD	182ND STREET	DORCHESTER AVENUE	73,157	RUTTING	0.0%	Patching - AC Shallow	\$16

Pavement ID	Road Name	From	To	Area	Distress Type	Density	Maint. Activity	Cost
LNSG::WSTFRNT RD::40	WEST FRONTAGE ROAD	DORCHESTER AVENUE	THORNTON LANSING ROAD	4,619	JT SEAL DMG	100.0%	Joint Seal (Localized)	\$819
LNSG::WSTFRNT RD::50	WEST FRONTAGE ROAD	THORNTON LANSING ROA	END	21,609	JT SEAL DMG	100.0%	Joint Seal (Localized)	\$3,987
LNSG::WST LK RD::10	WEST LAKE ROAD	OAKWOOD AVENUE	WEST LAKE ROAD	10,046	L & T CR	4.3%	Crack Sealing - AC	\$436
LNSG::WST LK RD::10	WEST LAKE ROAD	OAKWOOD AVENUE	WEST LAKE ROAD	10,046	POTHOLE	0.0%	Patching - AC Deep	\$164
LNSG::WST ST::10	WEST STREET	CENTRAL STREET	WENTWORTH AVENUE	29,963	L & T CR	2.2%	Crack Sealing - AC	\$649
LNSG::WST ST::10	WEST STREET	CENTRAL STREET	WENTWORTH AVENUE	29,963	L & T CR	0.4%	Crack Sealing - AC	\$121
LNSG::WST ST::20	WEST STREET	RIDGE ROAD	CENTRAL STREET	8,982	L & T CR	4.2%	Crack Sealing - AC	\$380
LNSG::WST ST::20	WEST STREET	RIDGE ROAD	CENTRAL STREET	8,982	ALLIGATOR CR	0.7%	Crack Sealing - AC	\$29
LNSG::WST ST::20	WEST STREET	RIDGE ROAD	CENTRAL STREET	8,982	L & T CR	0.2%	Crack Sealing - AC	\$21
LNSG::WST ST::20	WEST STREET	RIDGE ROAD	CENTRAL STREET	8,982	ALLIGATOR CR	0.4%	Patching - AC Deep	\$663
LNSG::WST ST::20	WEST STREET	RIDGE ROAD	CENTRAL STREET	8,982	RUTTING	0.0%	Patching - AC Shallow	\$18
LNSG::WLDWD AVE::100	WILDWOOD AVENUE	189TH PLACE	189TH STREET	13,689	ALLIGATOR CR	1.4%	Crack Sealing - AC	\$77
LNSG::WLDWD AVE::100	WILDWOOD AVENUE	189TH PLACE	189TH STREET	13,689	L & T CR	1.4%	Crack Sealing - AC	\$190
LNSG::WLDWD AVE::100	WILDWOOD AVENUE	189TH PLACE	189TH STREET	13,689	L & T CR	4.6%	Crack Sealing - AC	\$634
LNSG::WLDWD AVE::100	WILDWOOD AVENUE	189TH PLACE	189TH STREET	13,689	ALLIGATOR CR	0.2%	Patching - AC Deep	\$513
LNSG::WLDWD AVE::100	WILDWOOD AVENUE	189TH PLACE	189TH STREET	13,689	RUTTING	0.0%	Patching - AC Deep	\$16
LNSG::WLDWD AVE::100	WILDWOOD AVENUE	189TH PLACE	189TH STREET	13,689	RUTTING	0.1%	Patching - AC Shallow	\$80
LNSG::WLDWD AVE::170	WILDWOOD AVENUE	181ST STREET	END	10,611	L & T CR	3.2%	Crack Sealing - AC	\$334
LNSG::WLDWD AVE::170	WILDWOOD AVENUE	181ST STREET	END	10,611	L & T CR	0.2%	Crack Sealing - AC	\$23
LNSG::WLDWD AVE::180	WILDWOOD AVENUE	181ST STREET	180TH STREET	18,686	L & T CR	2.3%	Crack Sealing - AC	\$420
LNSG::WLDWD AVE::180	WILDWOOD AVENUE	181ST STREET	180TH STREET	18,686	ALLIGATOR CR	0.2%	Crack Sealing - AC	\$18
LNSG::WLDWD AVE::190	WILDWOOD AVENUE	180TH STREET	179TH STREET	18,487	L & T CR	2.7%	Crack Sealing - AC	\$499
LNSG::WLDWD AVE::190	WILDWOOD AVENUE	180TH STREET	179TH STREET	18,487	ALLIGATOR CR	0.2%	Crack Sealing - AC	\$19
LNSG::WLDWD AVE::190	WILDWOOD AVENUE	180TH STREET	179TH STREET	18,487	ALLIGATOR CR	0.4%	Patching - AC Deep	\$1,222
LNSG::WLDWD AVE::200	WILDWOOD AVENUE	179TH STREET	178TH STREET	18,149	ALLIGATOR CR	0.7%	Crack Sealing - AC	\$51
LNSG::WLDWD AVE::200	WILDWOOD AVENUE	179TH STREET	178TH STREET	18,149	L & T CR	0.2%	Crack Sealing - AC	\$43
LNSG::WLDWD AVE::200	WILDWOOD AVENUE	179TH STREET	178TH STREET	18,149	L & T CR	2.5%	Crack Sealing - AC	\$447
LNSG::WLDWD AVE::200	WILDWOOD AVENUE	179TH STREET	178TH STREET	18,149	ALLIGATOR CR	3.7%	Patching - AC Deep	\$8,586
LNSG::WLDWD AVE::200	WILDWOOD AVENUE	179TH STREET	178TH STREET	18,149	RUTTING	0.0%	Patching - AC Shallow	\$18
LNSG::WLLM ST::30	WILLIAM STREET	187TH STREET	186TH STREET	19,056	BLOCK CR	0.9%	Crack Sealing - AC	\$51
LNSG::WLLM ST::30	WILLIAM STREET	187TH STREET	186TH STREET	19,056	RUTTING	0.1%	Patching - AC Shallow	\$91
LNSG::WLLM ST::40	WILLIAM STREET	RIDGE ROAD	LAKE STREET	9,877	L & T CR	0.4%	Crack Sealing - AC	\$41
LNSG::WLLM ST::40	WILLIAM STREET	RIDGE ROAD	LAKE STREET	9,877	ALLIGATOR CR	1.0%	Patching - AC Deep	\$1,538
LNSG::WLLM ST::50	WILLIAM STREET	LAKE STREET	RANDOLPH STREET	9,831	EDGE CR	0.6%	Crack Sealing - AC	\$54
LNSG::WLLM ST::50	WILLIAM STREET	LAKE STREET	RANDOLPH STREET	9,831	BLOCK CR	2.3%	Crack Sealing - AC	\$67
LNSG::WLLM ST::50	WILLIAM STREET	LAKE STREET	RANDOLPH STREET	9,831	L & T CR	1.4%	Crack Sealing - AC	\$134
LNSG::WLLM ST::50	WILLIAM STREET	LAKE STREET	RANDOLPH STREET	9,831	ALLIGATOR CR	0.9%	Patching - AC Deep	\$1,443
LNSG::WLLM ST::110	WILLIAM STREET	JACKSON STREET	178TH STREET	9,932	L & T CR	0.6%	Crack Sealing - AC	\$58
LNSG::WLLM ST::110	WILLIAM STREET	JACKSON STREET	178TH STREET	9,932	ALLIGATOR CR	0.9%	Patching - AC Deep	\$1,488
LNSG::WLLM ST::120	WILLIAM STREET	178TH STREET	177TH STREET	18,634	ALLIGATOR CR	0.1%	Crack Sealing - AC	\$9
LNSG::WLLM ST::120	WILLIAM STREET	178TH STREET	177TH STREET	18,634	RUTTING	0.0%	Patching - AC Deep	\$18

Pavement ID	Road Name	From	To	Area	Distress Type	Density	Maint. Activity	Cost
LNSG::WLLM ST::120	WILLIAM STREET	178TH STREET	177TH STREET	18,634	RUTTING	0.2%	Patching - AC Shallow	\$187
LNSG::WLLM ST::160	WILLIAM STREET	173RD STREET	172ND PLACE	9,528	BLOCK CR	24.7%	Crack Sealing - AC	\$717
LNSG::WLLM ST::160	WILLIAM STREET	173RD STREET	172ND PLACE	9,528	L & T CR	3.3%	Crack Sealing - AC	\$318
LNSG::WLLW LN::10	WILLOW LANE	ILLI INDI DRIVE	END	13,771	ALLIGATOR CR	0.9%	Crack Sealing - AC	\$54
LNSG::WLLW LN::10	WILLOW LANE	ILLI INDI DRIVE	END	13,771	L & T CR	1.1%	Crack Sealing - AC	\$152
LNSG::WLLW LN::10	WILLOW LANE	ILLI INDI DRIVE	END	13,771	ALLIGATOR CR	4.6%	Patching - AC Deep	\$8,044
LNSG::WRGHT ST::10	WRIGHT STREET	177TH STREET	END	7,389	L & T CR	2.7%	Crack Sealing - AC	\$199
LNSG::WRGHT ST::20	WRIGHT STREET	177TH STREET	176TH STREET	18,137	L & T CR	2.0%	Crack Sealing - AC	\$366
LNSG::WRGHT ST::30	WRIGHT STREET	176TH STREET	175TH STREET	18,639	L & T CR	2.1%	Crack Sealing - AC	\$398

APPENDIX F – PAVEMENT INVENTORY AND CONDITION TABULAR DATA

Pavement ID	Road Name	From	To	Surface	Rank	Length (FT)	Width (FT)	Area (SF)	PCI	IRI
LNSG::169TH CT::10	169TH COURT	170TH STREET	END	Asphalt	S	1,004	28	28,104	37	379
LNSG::169TH CT::20	169TH COURT	170TH PLACE	170TH STREET	Asphalt	S	317	28	8,864	61	344
LNSG::169TH CT::30	169TH COURT	171ST STREET	170TH PLACE	Asphalt	S	323	28	9,041	40	359
LNSG::169TH CT::40	169TH COURT	WENTWORTH AVENUE	171ST STREET	Asphalt	S	355	28	9,953	20	387
LNSG::170TH PL::10	170TH PLACE	WILLIAM STREET	169TH COURT	Asphalt	S	413	28	11,571	61	223
LNSG::170TH ST::10	170TH STREET	COMMUNITY STREET	HENRY STREET	Asphalt	S	332	28	9,286	53	325
LNSG::170TH ST::20	170TH STREET	GRANT STREET	169TH COURT	Asphalt	S	681	28	19,065	33	364
LNSG::171ST ST::10	171ST STREET	OAKWOOD AVENUE	END	Asphalt	S	170	28	4,771	61	264
LNSG::171ST ST::100	171ST STREET	ROY STREET	COMMUNITY STREET	Asphalt	S	242	28	6,767	8	366
LNSG::171ST ST::110	171ST STREET	COMMUNITY STREET	HENRY STREET	Asphalt	S	314	28	8,781	13	305
LNSG::171ST ST::120	171ST STREET	HENRY STREET	GRANT STREET	Asphalt	S	312	28	8,739	29	335
LNSG::171ST ST::130	171ST STREET	GRANT STREET	WILLIAM STREET	Asphalt	S	322	28	9,010	14	421
LNSG::171ST ST::140	171ST STREET	WILLIAM STREET	BERNADINE STREET	Asphalt	S	415	28	11,621	12	361
LNSG::171ST ST::150	171ST STREET	BERNADINE STREET	169TH COURT	Asphalt	S	169	28	4,729	46	584
LNSG::171ST ST::20	171ST STREET	OAKWOOD AVENUE	RIDGEWOOD AVENUE	Asphalt	S	332	28	9,285	51	280
LNSG::171ST ST::30	171ST STREET	RIDGEWOOD AVENUE	CHICAGO AVENUE	Asphalt	S	326	28	9,132	51	333
LNSG::171ST ST::40	171ST STREET	CHICAGO AVENUE	PARK AVENUE	Asphalt	S	337	28	9,430	44	410
LNSG::171ST ST::50	171ST STREET	PARK AVENUE	LORENZ AVENUE	Asphalt	S	332	28	9,308	42	398
LNSG::171ST ST::60	171ST STREET	LORENZ AVENUE	GREENBAY AVENUE	Asphalt	S	332	28	9,287	51	297
LNSG::171ST ST::70	171ST STREET	GREENBAY AVENUE	BURNHAM AVENUE	Asphalt	S	314	28	8,782	38	483
LNSG::171ST ST::80	171ST STREET	BURNHAM AVENUE	WALTER STREET	Asphalt	S	349	28	9,769	51	388
LNSG::171ST ST::90	171ST STREET	WALTER STREET	ROY STREET	Asphalt	S	325	28	9,112	54	290
LNSG::172ND PL::10	172ND PLACE	HENRY STREET	GRANT STREET	Asphalt	S	177	28	4,963	64	198
LNSG::172ND PL::20	172ND PLACE	GRANT STREET	WILLIAM STREET	Asphalt	S	316	28	8,859	55	176
LNSG::172ND PL::30	172ND PLACE	WILLIAM STREET	BERNADINE STREET	Asphalt	S	391	28	10,960	43	236
LNSG::172ND ST::10	172ND STREET	TORRENCE AVENUE	BENSLEY AVENUE	Asphalt	S	963	28	26,975	69	240
LNSG::172ND ST::100	172ND STREET	BURNHAM AVENUE	WALTER STREET	Asphalt	S	343	28	9,612	46	329
LNSG::172ND ST::110	172ND STREET	WALTER STREET	ROY STREET	Asphalt	S	327	28	9,164	48	408
LNSG::172ND ST::120	172ND STREET	ROY STREET	COMMUNITY STREET	Asphalt	S	332	28	9,288	41	352
LNSG::172ND ST::130	172ND STREET	COMMUNITY STREET	HENRY STREET	Asphalt	S	321	28	8,982	59	268
LNSG::172ND ST::140	172ND STREET	WENTWORTH AVENUE	END	Asphalt	S	474	28	13,259	38	288
LNSG::172ND ST::20	172ND STREET	TORRENCE AVENUE	OAK AVENUE	Asphalt	S	1,013	28	28,372	92	142
LNSG::172ND ST::30	172ND STREET	OAKWOOD AVENUE	END	Asphalt	S	178	28	4,971	52	369
LNSG::172ND ST::40	172ND STREET	OAKWOOD AVENUE	RIDGEWOOD AVENUE	Asphalt	S	330	28	9,246	46	242
LNSG::172ND ST::50	172ND STREET	RIDGEWOOD AVENUE	CHICAGO AVENUE	Asphalt	S	324	28	9,077	47	390
LNSG::172ND ST::60	172ND STREET	CHICAGO AVENUE	PARK AVENUE	Asphalt	S	340	28	9,515	52	241
LNSG::172ND ST::70	172ND STREET	PARK AVENUE	LORENZ AVENUE	Asphalt	S	331	28	9,274	53	351
LNSG::172ND ST::80	172ND STREET	LORENZ AVENUE	GREENBAY AVENUE	Asphalt	S	332	28	9,302	69	277
LNSG::172ND ST::90	172ND STREET	GREENBAY AVENUE	BURNHAM AVENUE	Asphalt	S	318	28	8,904	61	451
LNSG::173RD PL::10	173RD PLACE	OAKWOOD AVENUE	WEST LAKE ROAD	Asphalt	S	355	28	9,928	66	247
LNSG::173RD PL::20	173RD PLACE	OAKWOOD AVENUE	CHICAGO AVENUE	Asphalt	S	771	28	21,574	20	286
LNSG::173RD PL::30	173RD PLACE	HENRY STREET	WILLIAM STREET	Asphalt	S	666	28	18,646	37	224
LNSG::173RD PL::40	173RD PLACE	WILLIAM STREET	BERNADINE STREET	Asphalt	S	330	28	9,238	32	188

Pavement ID	Road Name	From	To	Surface	Rank	Length (FT)	Width (FT)	Area (SF)	PCI	IRI
LNSG::173RD ST::10	173RD STREET	BERNICE ROAD	BENSLEY AVENUE	Asphalt	S	653	28	18,282	87	150
LNSG::173RD ST::100	173RD STREET	ROY STREET	COMMUNITY STREET	Asphalt	S	331	28	9,275	50	307
LNSG::173RD ST::110	173RD STREET	COMMUNITY STREET	HENRY STREET	Asphalt	S	325	28	9,087	45	199
LNSG::173RD ST::120	173RD STREET	HENRY STREET	HENRY STREET	Asphalt	S	212	28	5,943	45	373
LNSG::173RD ST::130	173RD STREET	HENRY STREET	WILLIAM STREET	Asphalt	S	490	28	13,732	56	200
LNSG::173RD ST::140	173RD STREET	WILLIAM STREET	WILLIAM STREET	Asphalt	S	174	28	4,870	52	421
LNSG::173RD ST::150	173RD STREET	WILLIAM STREET	BERNADINE STREET	Asphalt	S	219	28	6,137	41	259
LNSG::173RD ST::160	173RD STREET	BERNADINE STREET	BERNADINE STREET	Asphalt	S	133	28	3,735	62	551
LNSG::173RD ST::170	173RD STREET	BERNADINE STREET	WENTWORTH AVENUE	Asphalt	S	316	28	8,842	47	230
LNSG::173RD ST::20	173RD STREET	TORRENCE AVENUE	BERNICE ROAD	Asphalt	S	314	28	8,804	94	235
LNSG::173RD ST::30	173RD STREET	TORRENCE AVENUE	OAK AVENUE	Asphalt	P	1,023	28	28,652	93	133
LNSG::173RD ST::40	173RD STREET	CHICAGO AVENUE	PARK AVENUE	Asphalt	S	340	28	9,520	28	372
LNSG::173RD ST::50	173RD STREET	PARK AVENUE	LORENZ AVENUE	Asphalt	S	330	28	9,246	37	381
LNSG::173RD ST::60	173RD STREET	LORENZ AVENUE	GREENBAY AVENUE	Asphalt	S	332	28	9,309	34	469
LNSG::173RD ST::70	173RD STREET	GREENBAY AVENUE	BURNHAM AVENUE	Asphalt	S	325	28	9,108	32	382
LNSG::173RD ST::80	173RD STREET	BURNHAM AVENUE	WALTER STREET	Asphalt	S	335	28	9,372	27	302
LNSG::173RD ST::90	173RD STREET	WALTER STREET	ROY STREET	Asphalt	S	329	28	9,218	35	381
LNSG::175TH ST::10	175TH STREET	176TH STREET	CHICAGO AVENUE	Asphalt	S	3,279	28	91,808	80	182
LNSG::175TH ST::100	175TH STREET	WRIGHT STREET	WILLIAM STREET	Asphalt	S	326	28	9,139	78	198
LNSG::175TH ST::110	175TH STREET	WILLIAM STREET	BERNADINE STREET	Asphalt	S	330	28	9,229	78	234
LNSG::175TH ST::120	175TH STREET	WENTWORTH AVENUE	STATE LINE AVENUE	Asphalt	S	1,081	28	30,262	54	164
LNSG::175TH ST::20	175TH STREET	CHICAGO AVENUE	LORENZ AVENUE	Asphalt	S	610	28	17,073	77	181
LNSG::175TH ST::30	175TH STREET	LORENZ AVENUE	GREENBAY AVENUE	Asphalt	S	354	28	9,911	79	180
LNSG::175TH ST::40	175TH STREET	GREENBAY AVENUE	BURNHAM AVENUE	Asphalt	S	362	28	10,137	76	329
LNSG::175TH ST::50	175TH STREET	BURNHAM AVENUE	WALTER STREET	Asphalt	S	340	28	9,507	70	299
LNSG::175TH ST::60	175TH STREET	WALTER STREET	ROY STREET	Asphalt	S	329	28	9,201	50	300
LNSG::175TH ST::70	175TH STREET	ROY STREET	COMMUNITY STREET	Asphalt	S	334	28	9,349	42	286
LNSG::175TH ST::80	175TH STREET	COMMUNITY STREET	HENRY STREET	Asphalt	S	326	28	9,131	51	248
LNSG::175TH ST::90	175TH STREET	HENRY STREET	WRIGHT STREET	Asphalt	S	333	28	9,333	74	275
LNSG::176TH PL::10	176TH PLACE	PAXTON AVENUE	ROSEWOOD DRIVE	Asphalt	S	385	28	10,785	91	261
LNSG::176TH PL::20	176TH PLACE	ROSEWOOD DRIVE	176TH PLACE	Asphalt	S	636	28	17,803	94	174
LNSG::176TH PL::30	176TH PLACE	176TH PLACE	LOCUST STREET	Asphalt	S	310	28	8,674	95	222
LNSG::176TH PL::40	176TH PLACE	LOCUST STREET	HOLLIDAY TERRACE	Asphalt	S	655	28	18,352	94	149
LNSG::176TH PL::50	176TH PLACE	HOLLIDAY TERRACE	TORRENCE AVENUE	Asphalt	S	663	28	18,559	69	266
LNSG::176TH PL::60	176TH PLACE	WENTWORTH AVENUE	MAPLE STREET	Asphalt	S	863	28	24,161	26	420
LNSG::176TH PL::70	176TH PLACE	MAPLE AVENUE	STATE LINE AVENUE	Asphalt	S	328	28	9,171	44	333
LNSG::176TH ST::10	176TH STREET	TORRENCE AVENUE	175TH STREET	Asphalt	S	1,162	28	32,546	59	178
LNSG::176TH ST::100	176TH STREET	WRIGHT STREET	WILLIAM STREET	Asphalt	S	329	28	9,199	34	534
LNSG::176TH ST::110	176TH STREET	WILLIAM STREET	BERNADINE STREET	Asphalt	S	329	28	9,216	42	852
LNSG::176TH ST::120	176TH STREET	BERNADINE STREET	WENTWORTH AVENUE	Asphalt	S	337	28	9,431	61	381
LNSG::176TH ST::130	176TH STREET	WENTWORTH AVENUE	WENTWORTH AVENUE	Asphalt	S	79	28	2,207	100	912
LNSG::176TH ST::20	176TH STREET	LORENZ AVENUE	GREENBAY AVENUE	Asphalt	S	359	28	10,055	80	334
LNSG::176TH ST::30	176TH STREET	GREENBAY AVENUE	BURNHAM AVENUE	Asphalt	S	364	28	10,203	79	344

Pavement ID	Road Name	From	To	Surface	Rank	Length (FT)	Width (FT)	Area (SF)	PCI	IRI
LNSG::176TH ST::40	176TH STREET	BURNHAM AVENUE	WALTER STREET	Asphalt	S	336	28	9,410	100	534
LNSG::176TH ST::50	176TH STREET	WALTER STREET	ROY STREET	Asphalt	S	331	28	9,262	100	449
LNSG::176TH ST::60	176TH STREET	ROY STREET	COMMUNITY STREET	Asphalt	S	334	28	9,363	100	338
LNSG::176TH ST::70	176TH STREET	COMMUNITY STREET	HENRY STREET	Asphalt	S	326	28	9,121	100	557
LNSG::176TH ST::80	176TH STREET	HENRY STREET	END	Gravel	T	112	28	3,125	Gravel	Gravel
LNSG::176TH ST::90	176TH STREET	START	WRIGHT STREET	Gravel	T	83	28	2,337	Gravel	Gravel
LNSG::177TH PL::10	177TH PLACE	PAXTON AVENUE	ROSEWOOD DRIVE	Asphalt	S	385	28	10,777	37	339
LNSG::177TH PL::20	177TH PLACE	WENTWORTH AVENUE	MAPLE STREET	Asphalt	S	856	28	23,979	100	240
LNSG::177TH ST::10	177TH STREET	CHAPPEL AVENUE	PAXTON AVENUE	Asphalt	S	948	28	26,552	82	332
LNSG::177TH ST::100	177TH STREET	ROY STREET	COMMUNITY STREET	Asphalt	S	335	28	9,379	100	416
LNSG::177TH ST::110	177TH STREET	COMMUNITY STREET	HENRY STREET	Asphalt	S	325	28	9,111	100	305
LNSG::177TH ST::120	177TH STREET	WRIGHT STREET	WILLIAM STREET	Asphalt	S	331	28	9,261	43	504
LNSG::177TH ST::130	177TH STREET	WILLIAM STREET	BERNADINE STREET	Asphalt	S	329	28	9,207	26	367
LNSG::177TH ST::140	177TH STREET	BERNADINE STREET	WENTWORTH AVENUE	Asphalt	S	337	28	9,425	26	371
LNSG::177TH ST::150	177TH STREET	WENTWORTH AVENUE	MAPLE STREET	Asphalt	S	859	28	24,065	86	316
LNSG::177TH ST::160	177TH STREET	MAPLE AVENUE	STATE LINE AVENUE	Asphalt	S	331	28	9,275	54	373
LNSG::177TH ST::20	177TH STREET	ROSEWOOD DRIVE	END	Asphalt	S	375	28	10,487	51	324
LNSG::177TH ST::30	177TH STREET	LOCUST STREET	TORRENCE AVENUE	Asphalt	S	1,311	28	36,708	31	359
LNSG::177TH ST::40	177TH STREET	OAKLEY AVENUE	ESCANABA AVENUE	Asphalt	S	341	28	9,537	85	280
LNSG::177TH ST::50	177TH STREET	ESCANABA AVENUE	EXCHANGE AVENUE	Asphalt	S	330	28	9,243	68	258
LNSG::177TH ST::60	177TH STREET	EXCHANGE AVENUE	COMMERCIAL STREET	Asphalt	S	333	28	9,315	52	305
LNSG::177TH ST::70	177TH STREET	COMMERCIAL STREET	SCHOOL STREET	Asphalt	S	323	28	9,033	28	379
LNSG::177TH ST::80	177TH STREET	BURNHAM AVENUE	WALTER STREET	Asphalt	S	333	28	9,312	80	352
LNSG::177TH ST::90	177TH STREET	WALTER STREET	ROY STREET	Asphalt	S	333	28	9,325	100	469
LNSG::178TH PL::10	178TH PLACE	WALTER STREET	END	Asphalt	S	229	28	6,403	85	334
LNSG::178TH ST::10	178TH STREET	LOCUST STREET	TORRENCE AVENUE	Asphalt	S	1,298	28	36,338	87	189
LNSG::178TH ST::100	178TH STREET	WILDWOOD AVENUE	OAKWOOD AVENUE	Asphalt	S	334	28	9,355	74	203
LNSG::178TH ST::110	178TH STREET	OAKWOOD AVENUE	RIDGEWOOD AVENUE	Asphalt	S	330	28	9,251	75	237
LNSG::178TH ST::120	178TH STREET	RIDGEWOOD AVENUE	CHICAGO AVENUE	Asphalt	S	332	28	9,309	82	244
LNSG::178TH ST::130	178TH STREET	CHICAGO AVENUE	PARK AVENUE	Asphalt	S	334	28	9,361	59	403
LNSG::178TH ST::140	178TH STREET	PARK AVENUE	LORENZ AVENUE	Asphalt	S	246	28	6,894	62	368
LNSG::178TH ST::150	178TH STREET	LORENZ AVENUE	COLUMBIAN DRIVE	Asphalt	S	199	28	5,583	40	538
LNSG::178TH ST::160	178TH STREET	LORENZ AVENUE	BURNHAM AVENUE	Asphalt	S	545	28	15,262	46	327
LNSG::178TH ST::170	178TH STREET	BURNHAM AVENUE	WALTER STREET	Asphalt	S	329	28	9,218	50	462
LNSG::178TH ST::180	178TH STREET	WALTER STREET	ROY STREET	Asphalt	S	335	28	9,389	59	221
LNSG::178TH ST::190	178TH STREET	ROY STREET	COMMUNITY STREET	Asphalt	S	336	28	9,394	72	206
LNSG::178TH ST::20	178TH STREET	TORRENCE AVENUE	GLEN OAK AVENUE	Asphalt	S	332	28	9,304	58	232
LNSG::178TH ST::200	178TH STREET	COMMUNITY STREET	HENRY STREET	Asphalt	S	325	28	9,107	52	267
LNSG::178TH ST::210	178TH STREET	HENRY STREET	WILLIAM STREET	Asphalt	S	649	28	18,165	57	282
LNSG::178TH ST::220	178TH STREET	WILLIAM STREET	BERNADINE STREET	Asphalt	S	343	28	9,614	69	279
LNSG::178TH ST::230	178TH STREET	BERNADINE STREET	WENTWORTH AVENUE	Asphalt	S	336	28	9,419	67	328
LNSG::178TH ST::240	178TH STREET	WENTWORTH AVENUE	MAPLE STREET	Concrete	S	853	28	23,893	57	493
LNSG::178TH ST::30	178TH STREET	GLEN OAK AVENUE	ARCADIA AVENUE	Asphalt	S	337	28	9,440	52	257

Pavement ID	Road Name	From	To	Surface	Rank	Length (FT)	Width (FT)	Area (SF)	PCI	IRI
LNSG::178TH ST::40	178TH STREET	ARCADIA AVENUE	OAKLEY AVENUE	Asphalt	S	666	28	18,659	90	190
LNSG::178TH ST::50	178TH STREET	OAKLEY AVENUE	ESCANABA AVENUE	Asphalt	S	341	28	9,541	92	149
LNSG::178TH ST::60	178TH STREET	ESCANABA AVENUE	EXCHANGE AVENUE	Asphalt	S	331	28	9,261	85	278
LNSG::178TH ST::70	178TH STREET	EXCHANGE AVENUE	COMMERCIAL STREET	Asphalt	S	332	28	9,304	79	223
LNSG::178TH ST::80	178TH STREET	COMMERCIAL STREET	SCHOOL STREET	Asphalt	S	325	28	9,109	87	270
LNSG::178TH ST::90	178TH STREET	SCHOOL STREET	WILDWOOD AVENUE	Asphalt	S	340	28	9,513	66	303
LNSG::179TH ST::10	179TH STREET	HICKORY STREET	DECKER AVENUE	Asphalt	S	340	28	9,518	49	350
LNSG::179TH ST::100	179TH STREET	OAKWOOD AVENUE	RIDGEWOOD AVENUE	Asphalt	S	332	28	9,291	45	563
LNSG::179TH ST::110	179TH STREET	RIDGEWOOD AVENUE	CHICAGO AVENUE	Asphalt	S	331	28	9,277	53	557
LNSG::179TH ST::120	179TH STREET	CHICAGO AVENUE	PARK AVENUE	Asphalt	S	335	28	9,380	53	393
LNSG::179TH ST::130	179TH STREET	PARK AVENUE	LORENZ AVENUE	Asphalt	S	327	28	9,144	46	452
LNSG::179TH ST::140	179TH STREET	LORENZ AVENUE	END	Asphalt	S	300	28	8,391	71	495
LNSG::179TH ST::20	179TH STREET	DECKER AVENUE	TORRENCE AVENUE	Asphalt	S	354	28	9,916	53	392
LNSG::179TH ST::30	179TH STREET	TORRENCE AVENUE	GLEN OAK AVENUE	Asphalt	S	333	28	9,315	68	477
LNSG::179TH ST::40	179TH STREET	GLEN OAK AVENUE	ARCADIA AVENUE	Asphalt	S	336	28	9,418	76	293
LNSG::179TH ST::50	179TH STREET	ESCANABA AVENUE	EXCHANGE AVENUE	Asphalt	S	331	28	9,280	68	248
LNSG::179TH ST::60	179TH STREET	EXCHANGE AVENUE	COMMERCIAL STREET	Asphalt	S	332	28	9,293	57	353
LNSG::179TH ST::70	179TH STREET	COMMERCIAL STREET	SCHOOL STREET	Asphalt	S	328	28	9,185	76	259
LNSG::179TH ST::80	179TH STREET	SCHOOL STREET	WILDWOOD AVENUE	Asphalt	S	336	28	9,407	28	402
LNSG::179TH ST::90	179TH STREET	WILDWOOD AVENUE	OAKWOOD AVENUE	Asphalt	S	334	28	9,355	53	518
LNSG::180TH PL::10	180TH PLACE	JASON LANE	WHITMAN LANE	Asphalt	S	356	28	9,973	54	329
LNSG::180TH PL::20	180TH PLACE	WHITMAN LANE	CLYDE AVENUE	Asphalt	S	440	28	12,314	69	277
LNSG::180TH PL::30	180TH PLACE	CLYDE AVENUE	PAXTON AVENUE	Asphalt	S	498	28	13,941	89	369
LNSG::180TH ST::10	180TH STREET	DONATUS DRIVE	CHARLOTTE DRIVE	Asphalt	S	287	28	8,042	50	235
LNSG::180TH ST::100	180TH STREET	ESCANABA AVENUE	EXCHANGE AVENUE	Asphalt	S	332	28	9,298	38	202
LNSG::180TH ST::110	180TH STREET	EXCHANGE AVENUE	COMMERCIAL STREET	Asphalt	S	331	28	9,282	100	296
LNSG::180TH ST::120	180TH STREET	COMMERCIAL STREET	SCHOOL STREET	Asphalt	S	333	28	9,325	100	319
LNSG::180TH ST::130	180TH STREET	SCHOOL STREET	WILDWOOD AVENUE	Asphalt	S	330	28	9,237	50	366
LNSG::180TH ST::140	180TH STREET	WILDWOOD AVENUE	OAKWOOD AVENUE	Asphalt	S	334	28	9,357	52	299
LNSG::180TH ST::150	180TH STREET	OAKWOOD AVENUE	RIDGEWOOD AVENUE	Asphalt	S	333	28	9,335	66	259
LNSG::180TH ST::160	180TH STREET	RIDGEWOOD AVENUE	CHICAGO AVENUE	Asphalt	S	330	28	9,247	83	343
LNSG::180TH ST::170	180TH STREET	CHICAGO AVENUE	PARK AVENUE	Asphalt	S	336	28	9,398	75	270
LNSG::180TH ST::180	180TH STREET	PARK AVENUE	LORENZ AVENUE	Asphalt	S	326	28	9,139	73	300
LNSG::180TH ST::190	180TH STREET	LORENZ AVENUE	BURNHAM AVENUE	Asphalt	S	670	28	18,751	75	313
LNSG::180TH ST::20	180TH STREET	CHARLOTTE DRIVE	STONY ISLAND AVENUE	Asphalt	S	181	28	5,062	84	391
LNSG::180TH ST::200	180TH STREET	BURNHAM AVENUE	END	Asphalt	S	188	28	5,270	32	275
LNSG::180TH ST::30	180TH STREET	STONY ISLAND AVENUE	BRITTANY LANE	Asphalt	S	210	28	5,889	91	347
LNSG::180TH ST::40	180TH STREET	BRITTANY LANE	CRYSTAL LANE	Asphalt	S	303	28	8,488	91	175
LNSG::180TH ST::50	180TH STREET	CRYSTAL LANE	RIDGELAND AVENUE	Asphalt	S	488	28	13,668	56	274
LNSG::180TH ST::60	180TH STREET	RIDGELAND AVENUE	JASON LANE	Asphalt	S	483	28	13,516	41	357
LNSG::180TH ST::70	180TH STREET	ARCADIA AVENUE	OAK AVENUE	Asphalt	S	328	28	9,184	40	268
LNSG::180TH ST::80	180TH STREET	OAK AVENUE	OAKLEY AVENUE	Asphalt	S	338	28	9,456	25	278
LNSG::180TH ST::90	180TH STREET	OAKLEY AVENUE	ESCANABA AVENUE	Asphalt	S	335	28	9,367	25	278

Pavement ID	Road Name	From	To	Surface	Rank	Length (FT)	Width (FT)	Area (SF)	PCI	IRI
LNSG::181ST PL::10	181ST PLACE	DONATUS DRIVE	HARPER STREET	Asphalt	S	118	28	3,298	85	377
LNSG::181ST PL::20	181ST PLACE	HARPER STREET	CHARLOTTE DRIVE	Asphalt	S	149	28	4,171	86	427
LNSG::181ST PL::30	181ST PLACE	ROY STREET	RANDOLPH STREET	Asphalt	S	285	28	7,982	89	355
LNSG::181ST ST::10	181ST STREET	DORCHESTER AVENUE	DONATUS DRIVE	Asphalt	S	175	28	4,899	72	275
LNSG::181ST ST::100	181ST STREET	RIDGEWOOD AVENUE	CHICAGO AVENUE	Asphalt	S	329	28	9,216	100	301
LNSG::181ST ST::110	181ST STREET	CHICAGO AVENUE	PARK AVENUE	Asphalt	S	336	28	9,418	100	348
LNSG::181ST ST::120	181ST STREET	PARK AVENUE	LORENZ AVENUE	Asphalt	S	328	28	9,190	91	244
LNSG::181ST ST::130	181ST STREET	LORENZ AVENUE	BURNHAM AVENUE	Asphalt	S	668	28	18,698	82	292
LNSG::181ST ST::140	181ST STREET	BURNHAM AVENUE	ROY STREET	Asphalt	S	654	28	18,325	59	355
LNSG::181ST ST::20	181ST STREET	STONY ISLAND AVENUE	BRITTANY LANE	Asphalt	S	209	28	5,859	24	398
LNSG::181ST ST::30	181ST STREET	BRITTANY LANE	CRYSTAL LANE	Asphalt	S	306	28	8,563	22	279
LNSG::181ST ST::40	181ST STREET	JASON LANE	WHITMAN LANE	Asphalt	S	348	28	9,753	47	268
LNSG::181ST ST::50	181ST STREET	EXCHANGE AVENUE	COMMERCIAL STREET	Asphalt	S	331	28	9,273	71	219
LNSG::181ST ST::60	181ST STREET	COMMERCIAL STREET	SCHOOL STREET	Asphalt	S	338	28	9,458	83	249
LNSG::181ST ST::70	181ST STREET	SCHOOL STREET	WILDWOOD AVENUE	Asphalt	S	323	28	9,055	100	176
LNSG::181ST ST::80	181ST STREET	WILDWOOD AVENUE	OAKWOOD AVENUE	Asphalt	S	334	28	9,360	100	330
LNSG::181ST ST::90	181ST STREET	OAKWOOD AVENUE	RIDGEWOOD AVENUE	Asphalt	S	335	28	9,380	100	279
LNSG::182ND PL::10	182ND PLACE	CLYDE AVENUE	END	Asphalt	S	361	28	10,106	26	487
LNSG::182ND PL::20	182ND PLACE	CLYDE AVENUE	DEJONG LANE	Asphalt	S	225	28	6,292	24	266
LNSG::182ND PL::30	182ND PLACE	LOCUST STREET	ROSE AVENUE	Asphalt	S	388	28	10,872	54	389
LNSG::182ND PL::40	182ND PLACE	ROSE AVENUE	HICKORY STREET	Asphalt	S	265	28	7,432	51	493
LNSG::182ND PL::50	182ND PLACE	OAKLEY AVENUE	END	Asphalt	S	254	28	7,116	94	247
LNSG::182ND PL::60	182ND PLACE	CHICAGO AVENUE	OAK STREET	Asphalt	S	420	28	11,758	71	338
LNSG::182ND PL::70	182ND PLACE	OAK STREET	GREENBAY AVENUE	Asphalt	S	509	28	14,243	100	193
LNSG::182ND ST::10	182ND STREET	START	JASON LANE	Asphalt	P	424	28	11,873	73	194
LNSG::182ND ST::20	182ND STREET	JASON LANE	CLYDE AVENUE	Asphalt	P	632	28	17,704	89	232
LNSG::182ND ST::30	182ND STREET	CLYDE AVENUE	PAXTON AVENUE	Asphalt	P	430	28	12,029	89	227
LNSG::182ND ST::40	182ND STREET	PAXTON AVENUE	OLDE FARM ROAD	Asphalt	P	298	28	8,355	90	194
LNSG::182ND ST::50	182ND STREET	OLDE FARM ROAD	LOCUST STREET	Asphalt	P	1,103	28	30,882	91	153
LNSG::182ND ST::60	182ND STREET	LOCUST STREET	182ND STREET	Asphalt	P	177	28	4,952	73	507
LNSG::182ND ST::70	182ND STREET	182ND STREET	VIOLET ROAD	Asphalt	P	716	28	20,043	81	201
LNSG::182ND ST::80	182ND STREET	VIOLET ROAD	TORRENCE AVENUE	Asphalt	P	373	40	14,903	91	329
LNSG::183RD PL::10	183RD PLACE	COUNTRY LANE	CLYDE AVENUE	Asphalt	S	309	28	8,655	47	411
LNSG::183RD PL::20	183RD PLACE	CLYDE AVENUE	DEJONG LANE	Asphalt	S	323	28	9,044	23	431
LNSG::183RD PL::30	183RD PLACE	DEJONG LANE	OLDE FARM ROAD	Asphalt	S	472	28	13,217	25	333
LNSG::183RD PL::40	183RD PLACE	OLDE FARM ROAD	CARRIAGE LANE	Asphalt	S	311	28	8,697	36	335
LNSG::183RD PL::50	183RD PLACE	CARRIAGE LANE	BOCK AVENUE	Asphalt	S	302	28	8,456	49	412
LNSG::183RD PL::60	183RD PLACE	LOCUST STREET	ROSE AVENUE	Asphalt	S	315	28	8,818	68	377
LNSG::183RD PL::70	183RD PLACE	ROSE AVENUE	HICKORY STREET	Asphalt	S	310	28	8,693	44	353
LNSG::183RD PL::80	183RD PLACE	HICKORY STREET	VIOLET ROAD	Asphalt	S	429	28	12,004	47	357
LNSG::183RD PL::90	183RD PLACE	OAKLEY AVENUE	SAILOR LANE	Asphalt	S	275	28	7,713	74	241
LNSG::183RD ST::10	183RD STREET	HOLLAND ROAD	END	Asphalt	S	197	28	5,512	85	218
LNSG::183RD ST::100	183RD STREET	OAKWOOD AVENUE	RIDGEWOOD AVENUE	Asphalt	S	337	28	9,436	57	653

Pavement ID	Road Name	From	To	Surface	Rank	Length (FT)	Width (FT)	Area (SF)	PCI	IRI
LNSG::183RD ST::110	183RD STREET	RIDGEWOOD AVENUE	CHICAGO AVENUE	Asphalt	S	329	28	9,199	59	529
LNSG::183RD ST::120	183RD STREET	CHICAGO AVENUE	END	Asphalt	S	495	28	13,857	21	316
LNSG::183RD ST::20	183RD STREET	HOLLAND ROAD	COUNTRY LANE	Asphalt	S	308	28	8,632	59	251
LNSG::183RD ST::30	183RD STREET	BOCK AVENUE	END	Asphalt	S	252	28	7,060	38	413
LNSG::183RD ST::40	183RD STREET	LOCUST STREET	BOCK AVENUE	Asphalt	S	298	28	8,334	54	344
LNSG::183RD ST::50	183RD STREET	TORRENCE AVENUE	FRED STREET	Asphalt	S	676	28	18,927	76	279
LNSG::183RD ST::60	183RD STREET	FRED STREET	OAKLEY AVENUE	Asphalt	S	658	28	18,428	85	230
LNSG::183RD ST::70	183RD STREET	OAKLEY AVENUE	SAILOR LANE	Asphalt	S	274	28	7,665	83	418
LNSG::183RD ST::80	183RD STREET	SCHOOL STREET	WILDWOOD AVENUE	Asphalt	S	322	28	9,019	49	580
LNSG::183RD ST::90	183RD STREET	WILDWOOD AVENUE	OAKWOOD AVENUE	Asphalt	S	328	28	9,172	25	601
LNSG::184TH PL::10	184TH PLACE	LOCUST STREET	ROSE AVENUE	Asphalt	S	293	28	8,195	22	480
LNSG::184TH PL::20	184TH PLACE	ROSE AVENUE	VIOLET ROAD	Asphalt	S	787	28	22,029	37	508
LNSG::184TH ST::10	184TH STREET	WEST FRONTAGE ROAD	HARPER AVENUE	Gravel	T	1,022	10	10,217	Gravel	Gravel
LNSG::184TH ST::20	184TH STREET	LOCUST STREET	VIOLET ROAD	Asphalt	S	1,080	28	30,238	49	310
LNSG::184TH ST::30	184TH STREET	TORRENCE AVENUE	GLEN OAK AVENUE	Asphalt	S	337	28	9,429	46	282
LNSG::184TH ST::40	184TH STREET	GLEN OAK AVENUE	ARCADIA AVENUE	Asphalt	S	338	28	9,454	100	194
LNSG::184TH ST::50	184TH STREET	ARCADIA AVENUE	OAK AVENUE	Asphalt	S	311	28	8,707	100	237
LNSG::184TH ST::60	184TH STREET	SCHOOL STREET	WILDWOOD AVENUE	Asphalt	S	324	28	9,071	22	693
LNSG::184TH ST::70	184TH STREET	WILDWOOD AVENUE	OAKWOOD AVENUE	Asphalt	S	324	28	9,072	33	599
LNSG::184TH ST::80	184TH STREET	OAKWOOD AVENUE	RIDGEWOOD AVENUE	Asphalt	S	337	28	9,448	46	553
LNSG::184TH ST::90	184TH STREET	RIDGEWOOD AVENUE	CHICAGO AVENUE	Asphalt	S	328	28	9,198	44	378
LNSG::185TH CT::10	185TH COURT	HICKORY STREET	ROSE AVENUE	Asphalt	S	265	28	7,426	32	427
LNSG::185TH PL::10	185TH PLACE	ROSE AVENUE	LOCUST STREET	Asphalt	S	464	28	12,991	30	552
LNSG::185TH ST::10	185TH STREET	STONY ISLAND AVENUE	HOLLAND ROAD	Asphalt	S	1,586	28	44,401	29	298
LNSG::185TH ST::100	185TH STREET	TORRENCE AVENUE	GLEN OAK AVENUE	Asphalt	S	333	28	9,314	53	334
LNSG::185TH ST::110	185TH STREET	GLEN OAK AVENUE	ARCADIA AVENUE	Asphalt	S	332	28	9,308	68	324
LNSG::185TH ST::120	185TH STREET	ARCADIA AVENUE	OAK AVENUE	Asphalt	S	335	28	9,387	57	288
LNSG::185TH ST::130	185TH STREET	OAK AVENUE	OAKLEY AVENUE	Asphalt	S	325	28	9,090	47	340
LNSG::185TH ST::140	185TH STREET	SCHOOL STREET	WILDWOOD AVENUE	Asphalt	S	326	28	9,124	48	312
LNSG::185TH ST::150	185TH STREET	WILDWOOD AVENUE	OAKWOOD AVENUE	Asphalt	S	320	28	8,972	50	460
LNSG::185TH ST::160	185TH STREET	OAKWOOD AVENUE	RIDGEWOOD AVENUE	Asphalt	S	338	28	9,459	55	578
LNSG::185TH ST::170	185TH STREET	RIDGEWOOD AVENUE	CHICAGO AVENUE	Asphalt	S	328	28	9,196	23	552
LNSG::185TH ST::20	185TH STREET	HOLLAND ROAD	COUNTRY LANE	Asphalt	S	316	28	8,850	49	264
LNSG::185TH ST::30	185TH STREET	COUNTRY LANE	CLYDE AVENUE	Asphalt	S	311	28	8,698	26	411
LNSG::185TH ST::40	185TH STREET	CLYDE AVENUE	DEJONG LANE	Asphalt	S	322	28	9,017	26	356
LNSG::185TH ST::50	185TH STREET	DEJONG LANE	OLDE FARM ROAD	Asphalt	S	673	28	18,833	25	456
LNSG::185TH ST::60	185TH STREET	OLDE FARM ROAD	CARRIAGE LANE	Asphalt	S	308	28	8,628	25	635
LNSG::185TH ST::70	185TH STREET	CARRIAGE LANE	BOCK AVENUE	Asphalt	S	301	28	8,437	23	528
LNSG::185TH ST::80	185TH STREET	ROSE AVENUE	HICKORY STREET	Asphalt	S	420	28	11,755	25	347
LNSG::185TH ST::90	185TH STREET	HICKORY STREET	VIOLET ROAD	Asphalt	S	317	28	8,862	24	372
LNSG::186TH PL::10	186TH PLACE	TORRENCE AVENUE	OAK AVENUE	Asphalt	S	1,009	28	28,257	46	386
LNSG::186TH ST::10	186TH STREET	FOREST VIEW LANE	WEST FRONTAGE ROAD	Asphalt	S	177	28	4,947	53	418
LNSG::186TH ST::20	186TH STREET	DANTE AVENUE	FOREST VIEW LANE	Asphalt	S	153	28	4,288	81	193

Pavement ID	Road Name	From	To	Surface	Rank	Length (FT)	Width (FT)	Area (SF)	PCI	IRI
LNSG::186TH ST::30	186TH STREET	STONY ISLAND AVENUE	DANTE AVENUE	Asphalt	S	988	28	27,667	90	150
LNSG::186TH ST::40	186TH STREET	WENTWORTH AVENUE	END	Asphalt	S	723	28	20,252	82	468
LNSG::187TH PL::10	187TH PLACE	TORRENCE AVENUE	OAK AVENUE	Asphalt	S	1,024	28	28,669	28	332
LNSG::187TH ST::10	187TH STREET	TORRENCE AVENUE	OAK AVENUE	Asphalt	S	1,019	28	28,540	22	369
LNSG::187TH ST::40	187TH STREET	HENRY STREET	SHERMAN STREET	Asphalt	S	335	28	9,380	86	454
LNSG::187TH ST::50	187TH STREET	SHERMAN STREET	WILLIAM STREET	Asphalt	S	326	28	9,140	83	454
LNSG::187TH ST::60	187TH STREET	WILLIAM STREET	BERNADINE STREET	Asphalt	S	340	28	9,510	49	303
LNSG::187TH ST::70	187TH STREET	BERNADINE STREET	WENTWORTH AVENUE	Asphalt	S	319	28	8,927	55	248
LNSG::188TH PL::10	188TH PLACE	RIDGEWOOD AVENUE	OAKWOOD AVENUE	Asphalt	S	571	28	16,001	57	266
LNSG::188TH PL::20	188TH PLACE	OAKWOOD AVENUE	RIDGEWOOD AVENUE	Asphalt	S	305	28	8,554	54	365
LNSG::188TH PL::30	188TH PLACE	GREENBAY AVENUE	LORENZ AVENUE	Asphalt	S	287	28	8,037	100	268
LNSG::188TH ST::10	188TH STREET	TORRENCE AVENUE	OAK AVENUE	Asphalt	S	1,011	28	28,306	23	310
LNSG::188TH ST::100	188TH STREET	BERNADINE STREET	WENTWORTH AVENUE	Asphalt	S	320	28	8,956	46	335
LNSG::188TH ST::20	188TH STREET	WILDWOOD AVENUE	OAKWOOD AVENUE	Asphalt	S	405	28	11,342	54	263
LNSG::188TH ST::30	188TH STREET	CHERRY LANE	PARK AVENUE	Asphalt	S	310	28	8,690	58	267
LNSG::188TH ST::40	188TH STREET	GREENBAY AVENUE	CHERRY LANE	Asphalt	S	516	28	14,458	63	187
LNSG::188TH ST::50	188TH STREET	WALTER STREET	END	Asphalt	S	144	28	4,037	8	205
LNSG::188TH ST::60	188TH STREET	WALTER STREET	HENRY STREET	Asphalt	S	421	28	11,793	100	439
LNSG::188TH ST::70	188TH STREET	HENRY STREET	SHERMAN STREET	Asphalt	S	338	28	9,457	44	288
LNSG::188TH ST::80	188TH STREET	SHERMAN STREET	WILLIAM STREET	Asphalt	S	327	28	9,156	44	309
LNSG::188TH ST::90	188TH STREET	WILLIAM STREET	BERNADINE STREET	Asphalt	S	335	28	9,382	42	261
LNSG::189TH PL::10	189TH PLACE	RIDGEWOOD AVENUE	WILDWOOD AVENUE	Asphalt	S	1,085	28	30,394	56	216
LNSG::189TH ST::10	189TH STREET	RIDGEWOOD AVENUE	WILDWOOD AVENUE	Asphalt	S	1,193	28	33,403	71	194
LNSG::189TH ST::20	189TH STREET	LORENZ AVENUE	PARK AVENUE	Asphalt	S	650	28	18,187	100	204
LNSG::189TH ST::30	189TH STREET	BURNHAM AVENUE	GREENBAY AVENUE	Asphalt	S	234	28	6,552	31	302
LNSG::190TH PL::10	190TH PLACE	OAKWOOD AVENUE	WILDWOOD AVENUE	Asphalt	S	669	28	18,734	44	243
LNSG::190TH PL::20	190TH PLACE	OAKWOOD AVENUE	END	Asphalt	S	915	28	25,624	74	223
LNSG::190TH PL::30	190TH PLACE	SHERMAN STREET	BERNADINE STREET	Asphalt	S	762	28	21,340	28	241
LNSG::190TH PL::40	190TH PLACE	BERNADINE STREET	WENTWORTH AVENUE	Asphalt	S	386	28	10,811	51	182
LNSG::190TH ST::10	190TH STREET	FOREST LANE	WILDWOOD AVENUE	Asphalt	S	459	28	12,851	48	206
LNSG::190TH ST::20	190TH STREET	OAKWOOD AVENUE	FOREST LANE	Asphalt	S	376	28	10,525	62	294
LNSG::190TH ST::30	190TH STREET	RIDGEWOOD AVENUE	OAKWOOD AVENUE	Asphalt	S	301	28	8,419	100	278
LNSG::190TH ST::40	190TH STREET	PARK AVENUE	RIDGEWOOD AVENUE	Asphalt	S	313	28	8,769	100	261
LNSG::190TH ST::50	190TH STREET	GREENBAY AVENUE	PARK AVENUE	Asphalt	S	943	28	26,409	100	197
LNSG::191ST PL::10	191ST PLACE	OAKWOOD AVENUE	WILDWOOD AVENUE	Asphalt	S	695	28	19,451	42	245
LNSG::191ST PL::20	191ST PLACE	PARK AVENUE	OAKWOOD AVENUE	Asphalt	S	764	28	21,381	49	327
LNSG::191ST PL::30	191ST PLACE	GREENBAY AVENUE	PARK AVENUE	Asphalt	S	640	28	17,927	34	257
LNSG::191ST PL::40	191ST PLACE	GREENBAY AVENUE	END	Asphalt	S	129	28	3,624	36	476
LNSG::191ST PL::50	191ST PLACE	SHERMAN STREET	BERNADINE STREET	Asphalt	S	780	28	21,847	50	241
LNSG::191ST PL::60	191ST PLACE	BERNADINE STREET	WENTWORTH AVENUE	Asphalt	S	425	28	11,900	24	317
LNSG::191ST ST::10	191ST STREET	OAKWOOD AVENUE	WILDWOOD AVENUE	Asphalt	S	695	28	19,461	87	170
LNSG::191ST ST::100	191ST STREET	GRANT STREET	SHERMAN STREET	Asphalt	S	317	28	8,883	100	255
LNSG::191ST ST::110	191ST STREET	SHERMAN STREET	BERNADINE STREET	Asphalt	S	786	28	21,998	25	274

Pavement ID	Road Name	From	To	Surface	Rank	Length (FT)	Width (FT)	Area (SF)	PCI	IRI
LNSG::191ST ST::20	191ST STREET	PARK AVENUE	OAKWOOD AVENUE	Asphalt	S	770	28	21,547	87	196
LNSG::191ST ST::30	191ST STREET	GREENBAY AVENUE	PARK AVENUE	Asphalt	S	826	28	23,131	68	190
LNSG::191ST ST::40	191ST STREET	191ST STREET	191ST STREET	Asphalt	S	156	28	4,358	76	305
LNSG::191ST ST::50	191ST STREET	BURNHAM AVENUE	GREENBAY AVENUE	Asphalt	S	218	28	6,118	94	328
LNSG::191ST ST::60	191ST STREET	BURNHAM AVENUE	WALTER STREET	Asphalt	S	219	28	6,138	100	287
LNSG::191ST ST::70	191ST STREET	WALTER STREET	ROY STREET	Asphalt	S	314	28	8,783	100	152
LNSG::191ST ST::80	191ST STREET	ROY STREET	ADA STREET	Asphalt	S	319	28	8,930	100	270
LNSG::191ST ST::90	191ST STREET	ADA STREET	GRANT STREET	Asphalt	S	301	28	8,437	100	125
LNSG::192ND PL::10	192ND PLACE	OAKWOOD AVENUE	WILDWOOD AVENUE	Asphalt	S	695	28	19,455	37	285
LNSG::192ND PL::20	192ND PLACE	OAKWOOD AVENUE	END	Asphalt	S	478	28	13,391	25	572
LNSG::192ND PL::30	192ND PLACE	ADA STREET	END	Asphalt	S	369	28	10,325	23	521
LNSG::192ND PL::40	192ND PLACE	BERNADINE STREET	WENTWORTH AVENUE	Asphalt	S	281	28	7,856	22	512
LNSG::192ND PL::50	192ND PLACE	WENTWORTH AVENUE	LANGE STREET	Asphalt	S	217	28	6,075	100	449
LNSG::192ND ST::10	192ND STREET	OAKWOOD AVENUE	WILDWOOD AVENUE	Asphalt	S	694	28	19,437	51	247
LNSG::192ND ST::20	192ND STREET	GREENBAY AVENUE	OAKWOOD AVENUE	Asphalt	S	1,398	28	39,147	55	259
LNSG::192ND ST::30	192ND STREET	WALTER STREET	ROY STREET	Asphalt	S	317	28	8,872	25	341
LNSG::192ND ST::40	192ND STREET	SHERMAN STREET	WENTWORTH AVENUE	Asphalt	S	1,166	28	32,658	53	265
LNSG::192ND ST::50	192ND STREET	LANGE STREET	MAPLE STREET	Asphalt	S	748	28	20,931	24	256
LNSG::192ND ST::60	192ND STREET	MAPLE STREET	END	Asphalt	S	255	28	7,153	26	223
LNSG::193RD CT::10	193RD COURT	193RD STREET	END	Asphalt	S	188	28	5,273	28	386
LNSG::193RD PL::10	193RD PLACE	WILDWOOD AVENUE	WILDWOOD AVENUE	Asphalt	S	328	28	9,191	53	283
LNSG::193RD PL::20	193RD PLACE	OAKWOOD AVENUE	WILDWOOD AVENUE	Asphalt	S	369	28	10,340	55	247
LNSG::193RD PL::30	193RD PLACE	OAKWOOD AVENUE	OAKWOOD AVENUE	Asphalt	S	336	28	9,412	56	198
LNSG::193RD PL::40	193RD PLACE	OAKWOOD AVENUE	END	Asphalt	S	144	28	4,032	39	313
LNSG::193RD PL::50	193RD PLACE	WENTWORTH AVENUE	END	Asphalt	S	968	28	27,106	29	347
LNSG::193RD ST::10	193RD STREET	OAKWOOD AVENUE	WILDWOOD AVENUE	Asphalt	S	696	28	19,483	37	302
LNSG::193RD ST::20	193RD STREET	OAKWOOD AVENUE	END	Asphalt	S	484	28	13,540	30	622
LNSG::193RD ST::30	193RD STREET	BURNHAM AVENUE	ADA STREET	Asphalt	S	872	28	24,405	25	295
LNSG::193RD ST::40	193RD STREET	ADA STREET	GRANT STREET	Asphalt	S	313	28	8,769	25	380
LNSG::193RD ST::50	193RD STREET	GRANT STREET	SHERMAN STREET	Asphalt	S	302	28	8,445	22	457
LNSG::193RD ST::60	193RD STREET	SHERMAN STREET	BERNADINE STREET	Asphalt	S	868	28	24,296	24	284
LNSG::193RD ST::70	193RD STREET	BERNADINE STREET	END	Asphalt	S	95	28	2,665	40	833
LNSG::193RD ST::80	193RD STREET	LANGE STREET	193RD COURT	Asphalt	S	433	28	12,129	23	310
LNSG::193RD ST::90	193RD STREET	193RD COURT	MAPLE STREET	Asphalt	S	312	28	8,731	27	347
LNSG::194TH PL::10	194TH PLACE	PARK AVENUE	OAK STREET	Asphalt	S	356	28	9,980	25	382
LNSG::194TH ST::10	194TH STREET	WILDWOOD AVENUE	OAKWOOD AVENUE	Asphalt	S	701	28	19,633	26	196
LNSG::195TH CT::10	195TH COURT	PARK AVENUE	END	Asphalt	S	288	28	8,067	24	310
LNSG::AD ST::10	ADA STREET	193RD STREET	192ND PLACE	Asphalt	S	334	28	9,338	29	222
LNSG::AD ST::20	ADA STREET	192ND PLACE	191ST STREET	Asphalt	S	881	28	24,659	33	301
LNSG::AD ST::30	ADA STREET	186TH STREET	SCHULTZ DRIVE S	Asphalt	S	372	28	10,405	36	280
LNSG::AD ST::40	ADA STREET	SCHULTZ DRIVE S	SCHULTZ DRIVE N	Asphalt	S	468	28	13,095	94	176
LNSG::AD ST::50	ADA STREET	SCHULTZ DRIVE N	ANN STREET	Asphalt	S	313	28	8,760	100	297
LNSG::AD ST::60	ADA STREET	ANN STREET	ILLINOIS STREET	Asphalt	S	542	28	15,180	56	285

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LNSG::AD ST::70	ADA STREET	ILLINOIS STREET	RIDGE ROAD	Asphalt	S	929	28	26,009	85	209
LNSG::ANN ST::10	ANN STREET	GREENBAY AVENUE	BURNHAM AVENUE	Asphalt	S	340	28	9,506	75	707
LNSG::ANN ST::20	ANN STREET	BURNHAM AVENUE	WALTER STREET	Asphalt	S	376	28	10,518	49	422
LNSG::ANN ST::30	ANN STREET	WALTER STREET	ROY STREET	Asphalt	S	368	28	10,305	84	186
LNSG::ANN ST::40	ANN STREET	ROY STREET	ADA STREET	Asphalt	S	375	28	10,508	80	646
LNSG::ANN ST::50	ANN STREET	ADA STREET	GRANT STREET	Asphalt	S	370	28	10,357	90	422
LNSG::ANN ST::60	ANN STREET	GRANT STREET	SHERMAN STREET	Asphalt	S	330	28	9,229	24	379
LNSG::ANN ST::70	ANN STREET	SHERMAN STREET	MILLER DRIVE	Asphalt	S	298	28	8,335	25	536
LNSG::BB MLKS DR::10	BOB MALKAS DRIVE	BURNHAM AVENUE	END	Asphalt	S	906	28	25,359	68	226
LNSG::BCK AVE::10	BOCK AVENUE	186TH STREET	185TH STREET	Asphalt	S	755	28	21,131	100	251
LNSG::BCK AVE::20	BOCK AVENUE	185TH STREET	FOREST COURT	Asphalt	S	384	28	10,764	26	236
LNSG::BCK AVE::30	BOCK AVENUE	FOREST COURT	183RD PLACE	Asphalt	S	469	28	13,140	25	222
LNSG::BCK AVE::40	BOCK AVENUE	183RD PLACE	183RD STREET	Asphalt	S	488	28	13,674	25	235
LNSG::BCK RD::10	BOCK ROAD	THORNTON LANSING ROAD	END	Asphalt	S	319	28	8,928	93	420
LNSG::BCK RD::20	BOCK ROAD	START	END	Asphalt	S	167	28	4,668	90	250
LNSG::BNSLY AVE::10	BENSLEY AVENUE	173RD STREET	BERNICE ROAD	Asphalt	S	657	28	18,383	30	390
LNSG::BNSLY AVE::20	BENSLEY AVENUE	173RD STREET	172ND STREET	Asphalt	S	647	28	18,116	27	567
LNSG::BRBR CT::10	BARBARA COURT	S MANOR DRIVE	END	Asphalt	S	368	28	10,292	23	391
LNSG::BRNC AVE::10	BERNICE AVENUE	OAK AVENUE	RAILROAD AVENUE	Asphalt	S	1,959	28	54,841	75	196
LNSG::BRNC AVE::100	BERNICE AVENUE	COMMUNITY STREET	HENRY STREET	Asphalt	S	330	28	9,239	77	202
LNSG::BRNC AVE::110	BERNICE AVENUE	HENRY STREET	BERNADINE STREET	Asphalt	S	953	28	26,690	68	244
LNSG::BRNC AVE::120	BERNICE AVENUE	WENTWORTH AVENUE	BERNICE AVENUE	Asphalt	S	106	28	2,957	95	477
LNSG::BRNC AVE::130	BERNICE AVENUE	BERNICE AVENUE	174TH COURT	Asphalt	S	370	28	10,351	75	263
LNSG::BRNC AVE::140	BERNICE AVENUE	174TH COURT	END	Asphalt	S	1,170	28	32,758	51	249
LNSG::BRNC AVE::20	BERNICE AVENUE	RAILROAD AVENUE	TON AVENUE	Asphalt	S	388	28	10,866	61	242
LNSG::BRNC AVE::30	BERNICE AVENUE	TON AVENUE	CHICAGO AVENUE	Asphalt	S	452	28	12,642	75	270
LNSG::BRNC AVE::40	BERNICE AVENUE	CHICAGO AVENUE	LORENZ AVENUE	Asphalt	S	612	28	17,131	70	226
LNSG::BRNC AVE::50	BERNICE AVENUE	LORENZ AVENUE	GREENBAY AVENUE	Asphalt	S	362	28	10,145	75	259
LNSG::BRNC AVE::60	BERNICE AVENUE	GREENBAY AVENUE	BURNHAM AVENUE	Asphalt	S	358	28	10,024	40	418
LNSG::BRNC AVE::70	BERNICE AVENUE	BURNHAM AVENUE	WALTER STREET	Asphalt	S	334	28	9,350	74	358
LNSG::BRNC AVE::80	BERNICE AVENUE	WALTER STREET	ROY STREET	Asphalt	S	332	28	9,305	66	291
LNSG::BRNC AVE::90	BERNICE AVENUE	ROY STREET	COMMUNITY STREET	Asphalt	S	330	28	9,250	70	273
LNSG::BRNC RD::10	BERNICE ROAD	PAXTON AVENUE	END	Asphalt	S	966	28	27,046	51	314
LNSG::BRNC RD::100	BERNICE ROAD	TON AVENUE	CHICAGO AVENUE	Asphalt	P	452	28	12,646	46	199
LNSG::BRNC RD::110	BERNICE ROAD	CHICAGO AVENUE	PARK AVENUE	Asphalt	P	340	28	9,526	47	241
LNSG::BRNC RD::120	BERNICE ROAD	PARK AVENUE	LORENZ AVENUE	Asphalt	P	264	28	7,399	29	195
LNSG::BRNC RD::130	BERNICE ROAD	LORENZ AVENUE	LORENZ AVENUE	Asphalt	P	68	28	1,896	70	233
LNSG::BRNC RD::140	BERNICE ROAD	LORENZ AVENUE	GREENBAY AVENUE	Asphalt	P	330	28	9,241	40	290
LNSG::BRNC RD::150	BERNICE ROAD	GREENBAY AVENUE	BURNHAM AVENUE	Asphalt	P	322	28	9,013	41	256
LNSG::BRNC RD::160	BERNICE ROAD	BURNHAM AVENUE	WALTER STREET	Asphalt	P	337	28	9,431	53	195
LNSG::BRNC RD::170	BERNICE ROAD	WALTER STREET	ROY STREET	Asphalt	P	331	28	9,271	49	179
LNSG::BRNC RD::180	BERNICE ROAD	ROY STREET	COMMUNITY STREET	Asphalt	P	331	28	9,261	79	160
LNSG::BRNC RD::190	BERNICE ROAD	COMMUNITY STREET	HENRY STREET	Asphalt	P	328	28	9,195	52	238

Pavement ID	Road Name	From	To	Surface	Rank	Length (FT)	Width (FT)	Area (SF)	PCI	IRI
LNSG::BRNC RD::20	BERNICE ROAD	BENSLEY AVENUE	PAXTON AVENUE	Asphalt	S	1,668	28	46,696	64	249
LNSG::BRNC RD::200	BERNICE ROAD	HENRY STREET	BERNADINE STREET	Asphalt	P	996	28	27,887	61	139
LNSG::BRNC RD::210	BERNICE ROAD	BERNADINE STREET	WENTWORTH AVENUE	Asphalt	P	331	28	9,267	63	286
LNSG::BRNC RD::30	BERNICE ROAD	173RD STREET	BENSLEY AVENUE	Asphalt	S	1,270	28	35,560	75	292
LNSG::BRNC RD::40	BERNICE ROAD	OAK AVENUE	END	Asphalt	S	425	28	11,890	67	261
LNSG::BRNC RD::50	BERNICE ROAD	OAK AVENUE	OAK AVENUE	Asphalt	P	34	28	945	95	486
LNSG::BRNC RD::60	BERNICE ROAD	OAK AVENUE	BERNICE ROAD	Asphalt	P	59	28	1,639	74	252
LNSG::BRNC RD::70	BERNICE ROAD	BERNICE ROAD	RAILROAD AVENUE	Asphalt	P	1,932	28	54,082	69	171
LNSG::BRNC RD::80	BERNICE ROAD	RAILROAD AVENUE	OAKWOOD AVENUE	Asphalt	P	375	28	10,490	44	322
LNSG::BRNC RD::90	BERNICE ROAD	OAKWOOD AVENUE	TON AVENUE	Asphalt	P	164	28	4,583	39	518
LNSG::BRNDN ST::10	BERNADINE STREET	193RD STREET	192ND PLACE	Asphalt	S	509	28	14,265	25	307
LNSG::BRNDN ST::100	BERNADINE STREET	BERNICE ROAD	BERNICE AVENUE	Asphalt	S	419	28	11,719	69	243
LNSG::BRNDN ST::110	BERNADINE STREET	173RD PLACE	173RD STREET	Asphalt	S	326	28	9,121	46	293
LNSG::BRNDN ST::120	BERNADINE STREET	173RD STREET	172ND PLACE	Asphalt	S	336	28	9,400	25	303
LNSG::BRNDN ST::130	BERNADINE STREET	172ND PLACE	BERNADINE STREET	Asphalt	S	144	28	4,040	67	302
LNSG::BRNDN ST::140	BERNADINE STREET	172ND PLACE	171ST STREET	Asphalt	S	583	28	16,335	51	265
LNSG::BRNDN ST::20	BERNADINE STREET	191ST PLACE	191ST STREET	Asphalt	S	277	28	7,752	24	431
LNSG::BRNDN ST::30	BERNADINE STREET	191ST STREET	190TH PLACE	Asphalt	S	255	28	7,148	100	481
LNSG::BRNDN ST::40	BERNADINE STREET	188TH STREET	SHERMAN STREET	Asphalt	S	739	28	20,702	86	348
LNSG::BRNDN ST::50	BERNADINE STREET	188TH STREET	187TH STREET	Asphalt	S	676	28	18,938	94	308
LNSG::BRNDN ST::60	BERNADINE STREET	187TH STREET	186TH STREET	Asphalt	S	679	28	19,026	100	377
LNSG::BRNDN ST::70	BERNADINE STREET	178TH STREET	177TH STREET	Asphalt	S	666	28	18,648	84	225
LNSG::BRNDN ST::80	BERNADINE STREET	177TH STREET	176TH STREET	Asphalt	S	643	28	17,992	86	194
LNSG::BRNDN ST::90	BERNADINE STREET	176TH STREET	175TH STREET	Asphalt	S	676	28	18,931	72	264
LNSG::BRTTNY LN::10	BRITTANY LANE	182ND STREET	181ST STREET	Asphalt	S	661	28	18,506	40	483
LNSG::BRTTNY LN::20	BRITTANY LANE	181ST STREET	180TH STREET	Asphalt	S	642	28	17,974	87	420
LNSG::CHCG AVE::100	CHICAGO AVENUE	180TH STREET	179TH STREET	Asphalt	S	663	28	18,553	64	299
LNSG::CHCG AVE::110	CHICAGO AVENUE	179TH STREET	178TH STREET	Asphalt	S	666	28	18,636	82	226
LNSG::CHCG AVE::120	CHICAGO AVENUE	178TH STREET	PARK BOULEVARD	Asphalt	S	486	28	13,601	74	206
LNSG::CHCG AVE::130	CHICAGO AVENUE	PARK BOULEVARD	SPRING LAKE DRIVE	Asphalt	S	507	28	14,208	70	197
LNSG::CHCG AVE::140	CHICAGO AVENUE	SPRING LAKE DRIVE	175TH STREET	Asphalt	S	977	28	27,355	77	165
LNSG::CHCG AVE::150	CHICAGO AVENUE	BERNICE ROAD	BERNICE AVENUE	Asphalt	S	378	28	10,588	67	307
LNSG::CHCG AVE::160	CHICAGO AVENUE	BERNICE ROAD	173RD PLACE	Asphalt	S	512	28	14,336	63	264
LNSG::CHCG AVE::170	CHICAGO AVENUE	BERNICE ROAD	173RD STREET	Asphalt	S	159	28	4,442	73	251
LNSG::CHCG AVE::180	CHICAGO AVENUE	173RD PLACE	FLANIGAN DRIVE	Asphalt	S	75	28	2,107	68	407
LNSG::CHCG AVE::190	CHICAGO AVENUE	FLANIGAN DRIVE	172ND STREET	Asphalt	S	576	28	16,136	48	238
LNSG::CHCG AVE::20	CHICAGO AVENUE	186TH STREET	185TH STREET	Asphalt	S	661	28	18,509	94	204
LNSG::CHCG AVE::200	CHICAGO AVENUE	172ND STREET	171ST STREET	Asphalt	S	666	28	18,639	52	236
LNSG::CHCG AVE::210	CHICAGO AVENUE	171ST STREET	170TH STREET	Asphalt	S	656	28	18,359	58	246
LNSG::CHCG AVE::220	CHICAGO AVENUE	170TH STREET	END	Asphalt	S	2,135	28	59,777	44	252
LNSG::CHCG AVE::30	CHICAGO AVENUE	185TH STREET	184TH STREET	Asphalt	S	663	28	18,565	21	302
LNSG::CHCG AVE::40	CHICAGO AVENUE	184TH STREET	183RD STREET	Asphalt	S	581	28	16,259	95	220
LNSG::CHCG AVE::50	CHICAGO AVENUE	183RD STREET	183RD STREET	Asphalt	S	81	28	2,281	42	641

Pavement ID	Road Name	From	To	Surface	Rank	Length (FT)	Width (FT)	Area (SF)	PCI	IRI
LNSG::CHCG AVE::60	CHICAGO AVENUE	183RD STREET	182ND PLACE	Asphalt	S	275	28	7,699	22	397
LNSG::CHCG AVE::70	CHICAGO AVENUE	182ND PLACE	RIDGE ROAD	Asphalt	S	384	28	10,763	23	363
LNSG::CHCG AVE::80	CHICAGO AVENUE	RIDGE ROAD	181ST STREET	Asphalt	S	662	28	18,550	78	313
LNSG::CHCG AVE::90	CHICAGO AVENUE	181ST STREET	180TH STREET	Asphalt	S	664	28	18,596	83	152
LNSG::CHPPL AVE::10	CHAPPEL AVENUE	THORNTON LANSING ROAD	177TH STREET	Asphalt	S	1,663	28	46,556	88	199
LNSG::CHPPL AVE::20	CHAPPEL AVENUE	177TH STREET	FRONTAGE ROAD	Asphalt	S	774	28	21,680	94	170
LNSG::CHRLTT DR::10	CHARLOTTE DRIVE	181ST PLACE	180TH STREET	Asphalt	S	1,002	28	28,055	69	174
LNSG::CHRRY LN::10	CHERRY LANE	188TH STREET	END	Asphalt	S	414	28	11,603	43	197
LNSG::CHRSTN DR::10	CHRISTINA DRIVE	OAKLEY AVENUE	186TH STREET	Asphalt	S	1,384	28	38,758	30	304
LNSG::CHRSTN DR::15	CHRISTINA DRIVE	CHRISTINA DRIVE	CHRISTINA DRIVE	Asphalt	S	322	28	9,018	59	362
LNSG::CLYD AVE::10	CLYDE AVENUE	186TH STREET	185TH STREET	Asphalt	S	638	28	17,868	46	214
LNSG::CLYD AVE::20	CLYDE AVENUE	185TH STREET	183RD PLACE	Asphalt	S	1,036	28	28,994	25	391
LNSG::CLYD AVE::30	CLYDE AVENUE	183RD PLACE	182ND PLACE	Asphalt	S	665	28	18,613	23	437
LNSG::CLYD AVE::40	CLYDE AVENUE	182ND PLACE	182ND STREET	Asphalt	S	323	28	9,042	23	260
LNSG::CLYD AVE::50	CLYDE AVENUE	182ND STREET	END	Asphalt	S	205	28	5,732	40	316
LNSG::CLYD AVE::60	CLYDE AVENUE	START	END	Asphalt	S	86	28	2,411	61	298
LNSG::CLYD AVE::70	CLYDE AVENUE	THORNTON LANSING ROAD	END	Asphalt	S	1,018	28	28,502	100	210
LNSG::CMMNTY ST::10	COMMUNITY STREET	MADISON STREET	ADAMS STREET	Asphalt	S	709	28	19,857	64	257
LNSG::CMMNTY ST::20	COMMUNITY STREET	ADAMS STREET	178TH STREET	Asphalt	S	693	28	19,412	87	241
LNSG::CMMNTY ST::30	COMMUNITY STREET	178TH STREET	177TH STREET	Asphalt	S	661	28	18,508	68	352
LNSG::CMMNTY ST::40	COMMUNITY STREET	177TH STREET	176TH STREET	Asphalt	S	662	28	18,543	67	338
LNSG::CMMNTY ST::50	COMMUNITY STREET	176TH STREET	175TH STREET	Asphalt	S	666	28	18,637	69	297
LNSG::CMMNTY ST::60	COMMUNITY STREET	BERNICE ROAD	BERNICE AVENUE	Asphalt	S	412	28	11,527	31	461
LNSG::CMMNTY ST::70	COMMUNITY STREET	BERNICE ROAD	173RD STREET	Asphalt	S	663	28	18,577	100	410
LNSG::CMMNTY ST::80	COMMUNITY STREET	173RD STREET	172ND STREET	Asphalt	S	660	28	18,471	100	495
LNSG::CMMNTY ST::90	COMMUNITY STREET	171ST STREET	170TH STREET	Asphalt	S	329	28	9,221	60	267
LNSG::CMMRCL ST::10	COMMERCIAL STREET	181ST STREET	180TH STREET	Asphalt	S	576	28	16,141	84	225
LNSG::CMMRCL ST::20	COMMERCIAL STREET	180TH STREET	179TH STREET	Asphalt	S	659	28	18,443	88	204
LNSG::CMMRCL ST::30	COMMERCIAL STREET	179TH STREET	178TH STREET	Asphalt	S	652	28	18,253	66	284
LNSG::CMMRCL ST::40	COMMERCIAL STREET	178TH STREET	177TH STREET	Asphalt	S	647	28	18,111	55	248
LNSG::CNTRL ST::10	CENTRAL STREET	WEST STREET	MERTLE COURT	Asphalt	S	320	28	8,973	55	249
LNSG::CNTRL ST::20	CENTRAL STREET	MERTLE COURT	WENTWORTH AVENUE	Asphalt	S	316	28	8,834	71	225
LNSG::CNTRY LN::10	COUNTRY LANE	186TH STREET	185TH STREET	Asphalt	S	643	28	18,013	25	244
LNSG::CNTRY LN::20	COUNTRY LANE	185TH STREET	183RD PLACE	Asphalt	S	1,026	28	28,740	56	242
LNSG::CNTRY LN::30	COUNTRY LANE	183RD PLACE	183RD STREET	Asphalt	S	334	28	9,364	49	301
LNSG::CRRG LN::10	CARRIAGE LANE	186TH STREET	185TH STREET	Asphalt	S	747	28	20,927	23	361
LNSG::CRRG LN::20	CARRIAGE LANE	185TH STREET	183RD PLACE	Asphalt	S	856	28	23,979	24	379
LNSG::CRYSTL LN::10	CRYSTAL LANE	182ND STREET	181ST STREET	Asphalt	S	663	28	18,550	18	274
LNSG::CRYSTL LN::20	CRYSTAL LANE	181ST STREET	180TH STREET	Asphalt	S	647	28	18,109	93	171
LNSG::DCKR AVE::10	DECKER AVENUE	179TH STREET	END	Asphalt	S	532	28	14,900	32	386
LNSG::DCKR AVE::20	DECKER AVENUE	179TH STREET	END	Asphalt	S	636	28	17,794	47	278
LNSG::DJNG LN::10	DEJONG LANE	185TH STREET	183RD PLACE	Asphalt	S	1,040	28	29,115	23	357
LNSG::DJNG LN::20	DEJONG LANE	183RD PLACE	182ND PLACE	Asphalt	S	643	28	17,999	23	371

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LNSG::DMS ST::10	ADAMS STREET	BURNHAM AVENUE	WALTER STREET	Asphalt	S	330	28	9,250	94	399
LNSG::DMS ST::20	ADAMS STREET	WALTER STREET	ROY STREET	Asphalt	S	331	28	9,263	95	309
LNSG::DMS ST::30	ADAMS STREET	ROY STREET	COMMUNITY STREET	Asphalt	S	335	28	9,368	95	277
LNSG::DMS ST::40	ADAMS STREET	COMMUNITY STREET	HENRY STREET	Asphalt	S	328	28	9,189	77	456
LNSG::DMS ST::50	ADAMS STREET	HENRY STREET	WILLIAM STREET	Asphalt	S	634	28	17,757	17	1,031
LNSG::DMS ST::60	ADAMS STREET	WILLIAM STREET	WENTWORTH AVENUE	Asphalt	S	696	28	19,476	24	898
LNSG::DMS ST::70	ADAMS STREET	WENTWORTH AVENUE	MAPLE STREET	Asphalt	S	845	28	23,661	52	516
LNSG::DNT AVE::10	DANTE AVENUE	186TH STREET	END	Asphalt	S	590	15	8,855	8	205
LNSG::DNT AVE::20	DANTE AVENUE	START	END	Asphalt	S	313	20	6,267	8	205
LNSG::DNTS DR::10	DONATUS DRIVE	181ST PLACE	181ST STREET	Asphalt	S	580	28	16,240	76	154
LNSG::DNTS DR::20	DONATUS DRIVE	181ST STREET	180TH STREET	Asphalt	S	435	28	12,174	81	267
LNSG::DRCHST AVE::10	DORCHESTER AVENUE	182ND STREET	181ST STREET	Asphalt	S	753	28	21,084	90	131
LNSG::DRCHST AVE::20	DORCHESTER AVENUE	181ST STREET	WEST FRONTAGE ROAD	Asphalt	S	1,148	28	32,150	91	147
LNSG::FLNGN DR::10	FLANIGAN DRIVE	CHICAGO AVENUE	END	Asphalt	S	297	28	8,319	57	239
LNSG::FR STN3 DR::10	FIRE STATION 3 DRIVE	BURNHAM AVENUE	END	Concrete	S	121	28	3,385	96	1,012
LNSG::FRD ST::10	FRED STREET	183RD STREET	END	Asphalt	S	176	28	4,924	81	466
LNSG::FRD ST::20	FRED STREET	183RD STREET	RIDGE ROAD	Asphalt	S	412	28	11,534	65	766
LNSG::FRNTG RD::10	FRONTAGE ROAD	VOLBRECHT ROAD	CHAPPEL AVENUE	Asphalt	S	1,761	28	49,297	78	141
LNSG::FRNTG RD::20	FRONTAGE ROAD	CHAPPEL AVENUE	PAXTON AVENUE	Asphalt	S	974	28	27,270	84	152
LNSG::FRNTG RD::30	FRONTAGE ROAD	PAXTON AVENUE	END	Asphalt	S	802	28	22,461	82	251
LNSG::FRST CT::10	FOREST COURT	BOCK AVENUE	LOCUST STREET	Asphalt	S	297	28	8,324	30	479
LNSG::FRST CT::20	FOREST COURT	VIOLET ROAD	TORRENCE AVENUE	Asphalt	S	193	28	5,413	60	522
LNSG::FRST LN::10	FOREST LANE	190TH STREET	END	Asphalt	S	445	28	12,461	50	281
LNSG::FRST VW LN::10	FOREST VIEW LANE	186TH STREET	END	Asphalt	S	1,247	28	34,909	36	263
LNSG::GLN OK AVE::10	GLEN OAK AVENUE	186TH STREET	185TH STREET	Asphalt	S	658	28	18,423	100	230
LNSG::GLN OK AVE::20	GLEN OAK AVENUE	185TH STREET	184TH STREET	Asphalt	S	666	28	18,644	58	233
LNSG::GLN OK AVE::30	GLEN OAK AVENUE	184TH STREET	END	Asphalt	S	294	28	8,233	43	463
LNSG::GLN OK AVE::40	GLEN OAK AVENUE	THORNTON LANSING ROAD	179TH STREET	Asphalt	S	882	28	24,704	65	220
LNSG::GLN OK AVE::50	GLEN OAK AVENUE	179TH STREET	178TH STREET	Asphalt	S	653	28	18,277	86	212
LNSG::GLN TERR::10	GLEN TERRACE	RIDGE ROAD	INDIANA AVENUE	Asphalt	S	571	28	15,994	43	412
LNSG::GRNBY AVE::10	GREENBAY AVENUE	GLENWOOD LANSING ROAD	END	Asphalt	S	249	28	6,985	31	667
LNSG::GRNBY AVE::100	GREENBAY AVENUE	ILLINOIS STREET	182ND PLACE	Asphalt	S	562	28	15,729	55	296
LNSG::GRNBY AVE::110	GREENBAY AVENUE	176TH STREET	175TH STREET	Asphalt	S	656	28	18,355	86	217
LNSG::GRNBY AVE::120	GREENBAY AVENUE	BERNICE AVENUE	END	Asphalt	S	237	28	6,629	21	559
LNSG::GRNBY AVE::130	GREENBAY AVENUE	BERNICE ROAD	173RD STREET	Asphalt	S	667	28	18,686	39	384
LNSG::GRNBY AVE::140	GREENBAY AVENUE	173RD STREET	172ND STREET	Asphalt	S	661	28	18,497	24	351
LNSG::GRNBY AVE::150	GREENBAY AVENUE	172ND STREET	171ST STREET	Asphalt	S	659	28	18,458	36	350
LNSG::GRNBY AVE::160	GREENBAY AVENUE	171ST STREET	170TH STREET	Asphalt	S	647	28	18,121	42	376
LNSG::GRNBY AVE::20	GREENBAY AVENUE	192ND STREET	END	Asphalt	S	154	28	4,308	100	348
LNSG::GRNBY AVE::30	GREENBAY AVENUE	192ND STREET	191ST PLACE	Asphalt	S	355	28	9,950	100	279
LNSG::GRNBY AVE::40	GREENBAY AVENUE	191ST STREET	191ST STREET	Asphalt	S	139	28	3,903	95	351
LNSG::GRNBY AVE::50	GREENBAY AVENUE	189TH STREET	190TH STREET	Asphalt	S	442	28	12,380	64	171
LNSG::GRNBY AVE::60	GREENBAY AVENUE	189TH STREET	188TH PLACE	Asphalt	S	282	28	7,895	55	257

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LNSG::GRNBY AVE::70	GREENBAY AVENUE	188TH PLACE	188TH STREET	Asphalt	S	320	28	8,954	83	183
LNSG::GRNBY AVE::80	GREENBAY AVENUE	SCHULTZ DRIVE	ANN STREET	Asphalt	S	521	28	14,576	54	331
LNSG::GRNBY AVE::90	GREENBAY AVENUE	ANN STREET	ILLINOIS STREET	Asphalt	S	560	28	15,686	100	174
LNSG::GRNT ST::10	GRANT STREET	193RD STREET	191ST STREET	Asphalt	S	1,218	28	34,103	24	383
LNSG::GRNT ST::20	GRANT STREET	SCHULTZ DRIVE S	SCHULTZ DRIVE N	Asphalt	S	464	28	12,985	50	234
LNSG::GRNT ST::30	GRANT STREET	ANN STREET	ILLINOIS STREET	Asphalt	S	543	28	15,213	91	224
LNSG::GRNT ST::40	GRANT STREET	ILLINOIS STREET	STEWART AVENUE	Asphalt	S	691	28	19,352	86	298
LNSG::GRNT ST::50	GRANT STREET	STEWART AVENUE	RIDGE ROAD	Asphalt	S	185	28	5,180	85	205
LNSG::GRNT ST::60	GRANT STREET	172ND PLACE	171ST STREET	Asphalt	S	726	28	20,334	31	292
LNSG::GRNT ST::70	GRANT STREET	171ST STREET	171ST STREET	Asphalt	S	296	28	8,291	28	558
LNSG::GRNT ST::80	GRANT STREET	171ST STREET	170TH STREET	Asphalt	S	262	28	7,333	43	562
LNSG::HCKRY ST::10	HICKORY STREET	186TH STREET	185TH COURT	Asphalt	S	205	28	5,729	30	571
LNSG::HCKRY ST::20	HICKORY STREET	185TH COURT	185TH STREET	Asphalt	S	521	28	14,583	25	416
LNSG::HCKRY ST::30	HICKORY STREET	183RD PLACE	182ND PLACE	Asphalt	S	609	28	17,042	38	427
LNSG::HCKRY ST::40	HICKORY STREET	THORNTON LANSING ROAD	179TH STREET	Asphalt	S	794	28	22,232	60	343
LNSG::HCKRY ST::50	HICKORY STREET	179TH STREET	END	Asphalt	S	615	28	17,216	27	376
LNSG::HLLDY TERR::10	HOLLIDAY TERRACE	176TH PLACE	END	Asphalt	S	738	28	20,671	79	232
LNSG::HLLND RD::10	HOLLAND ROAD	185TH STREET	183RD STREET	Asphalt	S	1,355	28	37,947	57	215
LNSG::HNRV CT::10	HENRY COURT	188TH STREET	END	Asphalt	S	218	28	6,104	48	389
LNSG::HNRV ST::10	HENRY STREET	188TH STREET	187TH STREET	Asphalt	S	686	28	19,198	51	234
LNSG::HNRV ST::100	HENRY STREET	178TH STREET	177TH STREET	Asphalt	S	649	28	18,184	67	377
LNSG::HNRV ST::110	HENRY STREET	177TH STREET	176TH STREET	Asphalt	S	662	28	18,523	48	304
LNSG::HNRV ST::120	HENRY STREET	176TH STREET	175TH STREET	Asphalt	S	669	28	18,732	69	370
LNSG::HNRV ST::130	HENRY STREET	BERNICE ROAD	BERNICE AVENUE	Asphalt	S	412	28	11,544	75	227
LNSG::HNRV ST::140	HENRY STREET	BERNICE ROAD	173RD PLACE	Asphalt	S	470	28	13,169	48	395
LNSG::HNRV ST::150	HENRY STREET	173RD PLACE	173RD STREET	Asphalt	S	193	28	5,417	54	358
LNSG::HNRV ST::160	HENRY STREET	173RD STREET	173RD STREET	Asphalt	S	139	28	3,889	56	300
LNSG::HNRV ST::170	HENRY STREET	173RD STREET	172ND PLACE	Asphalt	S	331	28	9,273	36	261
LNSG::HNRV ST::180	HENRY STREET	172ND PLACE	172ND STREET	Asphalt	S	190	28	5,325	60	529
LNSG::HNRV ST::190	HENRY STREET	171ST STREET	170TH STREET	Asphalt	S	350	28	9,789	39	363
LNSG::HNRV ST::20	HENRY STREET	187TH STREET	OTTO STREET	Asphalt	S	172	28	4,814	93	218
LNSG::HNRV ST::30	HENRY STREET	OTTO STREET	186TH STREET	Asphalt	S	511	28	14,296	78	277
LNSG::HNRV ST::35	HENRY STREET	RIDGE ROAD	LAKE STREET	Asphalt	S	312	28	8,731	88	275
LNSG::HNRV ST::40	HENRY STREET	LAKE STREET	RANDOLPH STREET	Asphalt	S	353	28	9,876	89	386
LNSG::HNRV ST::50	HENRY STREET	RANDOLPH STREET	WASHINGTON STREET	Asphalt	S	346	28	9,685	85	332
LNSG::HNRV ST::60	HENRY STREET	WASHINGTON STREET	MADISON STREET	Asphalt	S	353	28	9,885	88	327
LNSG::HNRV ST::70	HENRY STREET	MADISON STREET	MONROE STREET	Asphalt	S	347	28	9,728	16	698
LNSG::HNRV ST::80	HENRY STREET	MONROE STREET	ADAMS STREET	Asphalt	S	363	28	10,159	23	766
LNSG::HNRV ST::90	HENRY STREET	ADAMS STREET	178TH STREET	Asphalt	S	702	28	19,642	56	327
LNSG::HRPR AVE::10	HARPER AVENUE	184TH STREET	END	Gravel	T	252	10	2,519	Gravel	Gravel
LNSG::HRPR AVE::20	HARPER AVENUE	THORNTON LANSING ROAD	END	Asphalt	S	165	28	4,631	81	698
LNSG::HRPR ST::10	HARPER STREET	182ND STREET	181ST PLACE	Asphalt	S	177	28	4,952	82	355
LNSG::HRTH CR::10	HEARTH CIRCLE	OLDE FARM ROAD	END	Asphalt	S	167	28	4,688	100	336

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LNSG::INDN AVE::10	INDIANA AVENUE	START	END	Asphalt	S	211	28	5,912	100	299
LNSG::INDN AVE::20	INDIANA AVENUE	LOCUST STREET	VIOLET ROAD	Asphalt	S	949	28	26,577	60	358
LNSG::INDN AVE::30	INDIANA AVENUE	VIOLET ROAD	TORRENCE AVENUE	Asphalt	S	370	28	10,356	51	372
LNSG::INDN AVE::40	INDIANA AVENUE	TORRENCE AVENUE	GLEN TERRACE	Asphalt	S	811	28	22,695	100	252
LNSG::INDN AVE::50	INDIANA AVENUE	GLEN TERRACE	OAK AVENUE	Asphalt	S	201	28	5,640	100	237
LNSG::INDN AVE::60	INDIANA AVENUE	OAK AVENUE	OAKLEY AVENUE	Asphalt	S	337	28	9,430	100	91
LNSG::INDN AVE::70	INDIANA AVENUE	OAKLEY AVENUE	ESCANABA AVENUE	Asphalt	S	351	28	9,820	100	108
LNSG::INDN AVE::80	INDIANA AVENUE	ESCANABA AVENUE	EXCHANGE AVENUE	Asphalt	S	320	28	8,960	100	151
LNSG::INDN AVE::90	INDIANA AVENUE	EXCHANGE AVENUE	COMMERCIAL STREET	Asphalt	S	679	28	19,008	100	170
LNSG::JCKSN ST::10	JACKSON STREET	WILLIAM STREET	WENTWORTH AVENUE	Asphalt	S	690	28	19,315	41	370
LNSG::JCKSN ST::20	JACKSON STREET	WENTWORTH AVENUE	MAPLE STREET	Asphalt	S	849	28	23,774	51	334
LNSG::JSN LN::10	JASON LANE	182ND STREET	181ST STREET	Asphalt	S	591	28	16,544	47	209
LNSG::JSN LN::20	JASON LANE	181ST STREET	180TH PLACE	Asphalt	S	553	28	15,492	53	159
LNSG::JSN LN::30	JASON LANE	180TH PLACE	180TH STREET	Asphalt	S	229	28	6,406	54	164
LNSG::KLY AVE::10	OAKLEY AVENUE	186TH STREET	185TH STREET	Asphalt	S	660	28	18,492	100	347
LNSG::KLY AVE::100	OAKLEY AVENUE	178TH STREET	177TH STREET	Asphalt	S	647	28	18,113	48	164
LNSG::KLY AVE::20	OAKLEY AVENUE	185TH STREET	END	Asphalt	S	519	28	14,542	100	313
LNSG::KLY AVE::30	OAKLEY AVENUE	183RD PLACE	END	Asphalt	S	200	28	5,601	87	284
LNSG::KLY AVE::40	OAKLEY AVENUE	183RD STREET	183RD PLACE	Asphalt	S	331	28	9,280	71	372
LNSG::KLY AVE::50	OAKLEY AVENUE	183RD STREET	183RD STREET	Asphalt	S	251	28	7,034	73	324
LNSG::KLY AVE::60	OAKLEY AVENUE	183RD STREET	182ND PLACE	Asphalt	S	62	28	1,739	90	410
LNSG::KLY AVE::70	OAKLEY AVENUE	182ND PLACE	RIDGE ROAD	Asphalt	S	350	28	9,798	67	256
LNSG::KLY AVE::80	OAKLEY AVENUE	INDIANA AVENUE	180TH STREET	Asphalt	S	815	28	22,815	41	285
LNSG::KLY AVE::90	OAKLEY AVENUE	180TH STREET	178TH STREET	Asphalt	S	1,314	28	36,786	51	202
LNSG::KTTL CT::10	KETTLE COURT	OLDE FARM ROAD	END	Asphalt	S	170	28	4,758	100	270
LNSG::KWD AVE::10	OAKWOOD AVENUE	GLENWOOD LANSING ROAD	194TH STREET	Asphalt	S	201	28	5,621	30	313
LNSG::KWD AVE::100	OAKWOOD AVENUE	188TH PLACE	188TH STREET	Asphalt	S	255	28	7,142	81	321
LNSG::KWD AVE::110	OAKWOOD AVENUE	188TH STREET	OAKWOOD AVENUE	Asphalt	S	542	28	15,168	93	272
LNSG::KWD AVE::120	OAKWOOD AVENUE	OAKWOOD AVENUE	186TH STREET	Asphalt	S	639	28	17,892	100	178
LNSG::KWD AVE::130	OAKWOOD AVENUE	185TH STREET	END	Asphalt	S	472	28	13,206	23	360
LNSG::KWD AVE::140	OAKWOOD AVENUE	185TH STREET	184TH STREET	Asphalt	S	663	28	18,554	77	470
LNSG::KWD AVE::150	OAKWOOD AVENUE	184TH STREET	183RD STREET	Asphalt	S	659	28	18,454	46	470
LNSG::KWD AVE::160	OAKWOOD AVENUE	183RD STREET	RIDGE ROAD	Asphalt	S	658	28	18,419	49	401
LNSG::KWD AVE::170	OAKWOOD AVENUE	RIDGE ROAD	181ST STREET	Asphalt	S	664	28	18,590	50	368
LNSG::KWD AVE::180	OAKWOOD AVENUE	181ST STREET	180TH STREET	Asphalt	S	666	28	18,656	56	330
LNSG::KWD AVE::190	OAKWOOD AVENUE	180TH STREET	179TH STREET	Asphalt	S	661	28	18,509	55	236
LNSG::KWD AVE::20	OAKWOOD AVENUE	194TH STREET	193RD PLACE	Asphalt	S	308	28	8,635	37	283
LNSG::KWD AVE::200	OAKWOOD AVENUE	179TH STREET	178TH STREET	Asphalt	S	654	28	18,302	76	304
LNSG::KWD AVE::210	OAKWOOD AVENUE	BERNICE ROAD	WEST LAKE ROAD	Asphalt	S	177	28	4,950	47	286
LNSG::KWD AVE::220	OAKWOOD AVENUE	WEST LAKE ROAD	173RD PLACE	Asphalt	S	164	28	4,590	30	301
LNSG::KWD AVE::230	OAKWOOD AVENUE	172ND STREET	171ST STREET	Asphalt	S	664	28	18,587	55	245
LNSG::KWD AVE::240	OAKWOOD AVENUE	171ST STREET	170TH STREET	Asphalt	S	655	28	18,341	66	304
LNSG::KWD AVE::30	OAKWOOD AVENUE	193RD PLACE	193RD STREET	Asphalt	S	314	28	8,805	53	291

Pavement ID	Road Name	From	To	Surface	Rank	Length (FT)	Width (FT)	Area (SF)	PCI	IRI
LNSG::KWD AVE::40	OAKWOOD AVENUE	193RD STREET	192ND PLACE	Asphalt	S	349	28	9,770	53	326
LNSG::KWD AVE::50	OAKWOOD AVENUE	192ND PLACE	192ND STREET	Asphalt	S	313	28	8,757	58	364
LNSG::KWD AVE::60	OAKWOOD AVENUE	192ND STREET	191ST PLACE	Asphalt	S	346	28	9,686	100	202
LNSG::KWD AVE::70	OAKWOOD AVENUE	191ST PLACE	191ST STREET	Asphalt	S	290	28	8,108	100	165
LNSG::KWD AVE::80	OAKWOOD AVENUE	191ST STREET	190TH PLACE	Asphalt	S	261	28	7,307	100	306
LNSG::KWD AVE::90	OAKWOOD AVENUE	190TH PLACE	190TH STREET	Asphalt	S	437	28	12,237	35	255
LNSG::LCST ST::10	LOCUST STREET	185TH PLACE	184TH PLACE	Asphalt	S	504	28	14,108	59	275
LNSG::LCST ST::100	LOCUST STREET	177TH STREET	178TH STREET	Asphalt	S	708	28	19,814	81	307
LNSG::LCST ST::110	LOCUST STREET	176TH PLACE	177TH STREET	Asphalt	S	338	28	9,463	76	348
LNSG::LCST ST::20	LOCUST STREET	184TH PLACE	FOREST COURT	Asphalt	S	130	28	3,634	62	386
LNSG::LCST ST::30	LOCUST STREET	FOREST COURT	184TH STREET	Asphalt	S	139	28	3,904	60	193
LNSG::LCST ST::40	LOCUST STREET	184TH STREET	183RD PLACE	Asphalt	S	298	28	8,336	60	545
LNSG::LCST ST::50	LOCUST STREET	183RD PLACE	183RD STREET	Asphalt	S	516	28	14,442	44	369
LNSG::LCST ST::60	LOCUST STREET	183RD STREET	182ND PLACE	Asphalt	S	328	28	9,176	43	333
LNSG::LCST ST::70	LOCUST STREET	182ND PLACE	182ND STREET	Asphalt	S	234	28	6,554	50	345
LNSG::LCST ST::80	LOCUST STREET	INDIANA AVENUE	END	Gravel	T	394	28	11,035	Gravel	Gravel
LNSG::LCST ST::90	LOCUST STREET	THORNTON LANSING ROAD	END	Asphalt	S	1,323	28	37,042	29	394
LNSG::LD FRM RD::10	OLDE FARM ROAD	186TH STREET	185TH STREET	Asphalt	S	744	28	20,827	23	292
LNSG::LD FRM RD::20	OLDE FARM ROAD	185TH STREET	KETTLE COURT	Asphalt	S	290	28	8,125	100	580
LNSG::LD FRM RD::30	OLDE FARM ROAD	KETTLE COURT	HEARTH CIRCLE	Asphalt	S	289	28	8,080	100	193
LNSG::LD FRM RD::40	OLDE FARM ROAD	HEARTH CIRCLE	183RD PLACE	Asphalt	S	283	28	7,936	43	290
LNSG::LD FRM RD::50	OLDE FARM ROAD	183RD PLACE	183RD PLACE	Asphalt	S	241	28	6,737	40	483
LNSG::LD FRM RD::60	OLDE FARM ROAD	183RD PLACE	182ND STREET	Asphalt	S	943	28	26,412	24	329
LNSG::LK ST::10	LAKE STREET	HENRY STREET	WILLIAM STREET	Asphalt	S	625	28	17,512	53	326
LNSG::LK ST::20	LAKE STREET	WILLIAM STREET	WENTWORTH AVENUE	Asphalt	S	712	28	19,939	79	351
LNSG::LK ST::30	LAKE STREET	WENTWORTH AVENUE	LANGE STREET	Asphalt	S	377	28	10,565	58	276
LNSG::LK ST::40	LAKE STREET	LANGE STREET	MAPLE STREET	Asphalt	S	445	28	12,456	88	258
LNSG::LK ST::50	LAKE STREET	MAPLE STREET	STATE LINE AVENUE	Asphalt	S	329	28	9,214	53	462
LNSG::LL ND DR::10	ILLI INDI DRIVE	LANGE STREET	ILLI INDI DRIVE	Asphalt	S	126	28	3,530	32	454
LNSG::LL ND DR::20	ILLI INDI DRIVE	LANGE STREET	MAPLE STREET	Asphalt	S	277	28	7,754	95	292
LNSG::LL ND DR::30	ILLI INDI DRIVE	MAPLE STREET	WILLOW LANE	Asphalt	S	446	28	12,479	91	320
LNSG::LLNS ST::10	ILLINOIS STREET	GREENBAY AVENUE	BURNHAM AVENUE	Asphalt	S	420	28	11,772	100	259
LNSG::LLNS ST::20	ILLINOIS STREET	BURNHAM AVENUE	WALTER STREET	Asphalt	S	378	28	10,577	53	372
LNSG::LLNS ST::30	ILLINOIS STREET	WALTER STREET	ROY STREET	Asphalt	S	372	28	10,404	92	445
LNSG::LLNS ST::40	ILLINOIS STREET	ROY STREET	ADA STREET	Asphalt	S	374	28	10,462	92	344
LNSG::LLNS ST::50	ILLINOIS STREET	ADA STREET	GRANT STREET	Asphalt	S	370	28	10,367	100	242
LNSG::LLNS ST::60	ILLINOIS STREET	GRANT STREET	SHERMAN STREET	Asphalt	S	327	28	9,160	84	319
LNSG::LLNS ST::70	ILLINOIS STREET	SHERMAN STREET	STEWART AVENUE	Asphalt	S	224	28	6,269	92	252
LNSG::LNG ST::10	LANGE STREET	192ND PLACE	193RD STREET	Asphalt	S	248	28	6,941	27	442
LNSG::LNG ST::20	LANGE STREET	192ND PLACE	192ND STREET	Asphalt	S	224	28	6,275	26	418
LNSG::LNG ST::30	LANGE STREET	LANGE STREET	ILLI INDI DRIVE	Asphalt	S	94	28	2,622	95	345
LNSG::LNG ST::40	LANGE STREET	TERRACE DRIVE	LANGE STREET	Asphalt	S	542	28	15,170	81	186
LNSG::LNG ST::50	LANGE STREET	TERRACE DRIVE	TERRACE DRIVE	Asphalt	S	50	28	1,399	95	841

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LNSG::LNG ST::60	LANGE STREET	RIDGE ROAD	TERRACE DRIVE	Asphalt	S	900	28	25,205	91	316
LNSG::LNG ST::70	LANGE STREET	RIDGE ROAD	LAKE STREET	Asphalt	S	549	28	15,374	73	280
LNSG::LNG ST::80	LANGE STREET	LAKE STREET	RANDOLPH STREET	Asphalt	S	348	28	9,741	90	250
LNSG::LNG ST::90	LANGE STREET	RANDOLPH STREET	WASHINGTON STREET	Asphalt	S	353	28	9,888	46	331
LNSG::LRNZ AVE::10	LORENZ AVENUE	188TH PLACE	189TH STREET	Asphalt	S	387	28	10,828	100	168
LNSG::LRNZ AVE::100	LORENZ AVENUE	172ND STREET	171ST STREET	Asphalt	S	658	28	18,427	58	229
LNSG::LRNZ AVE::110	LORENZ AVENUE	171ST STREET	170TH STREET	Asphalt	S	656	28	18,377	73	253
LNSG::LRNZ AVE::20	LORENZ AVENUE	RIDGE ROAD	181ST STREET	Asphalt	S	645	28	18,053	79	320
LNSG::LRNZ AVE::30	LORENZ AVENUE	181ST STREET	180TH STREET	Asphalt	S	668	28	18,699	86	280
LNSG::LRNZ AVE::40	LORENZ AVENUE	180TH STREET	179TH STREET	Asphalt	S	655	28	18,338	79	242
LNSG::LRNZ AVE::50	LORENZ AVENUE	179TH STREET	178TH STREET	Asphalt	S	693	28	19,394	66	269
LNSG::LRNZ AVE::60	LORENZ AVENUE	176TH STREET	175TH STREET	Asphalt	S	645	28	18,070	53	202
LNSG::LRNZ AVE::70	LORENZ AVENUE	BERNICE ROAD	BERNICE AVENUE	Asphalt	S	405	28	11,350	55	243
LNSG::LRNZ AVE::80	LORENZ AVENUE	BERNICE ROAD	173RD STREET	Asphalt	S	668	28	18,716	60	279
LNSG::LRNZ AVE::90	LORENZ AVENUE	173RD STREET	172ND STREET	Asphalt	S	661	28	18,504	66	270
LNSG::LS CT::10	LOUISE COURT	LOUISE DRIVE	END	Asphalt	S	86	28	2,415	45	575
LNSG::LS DR::10	LOUISE DRIVE	W LOUISE DRIVE	LOUISE COURT	Asphalt	S	375	28	10,491	31	189
LNSG::LS DR::20	LOUISE DRIVE	LOUISE COURT	N MANOR DRIVE	Asphalt	S	760	28	21,288	43	212
LNSG::LS DR::30	LOUISE DRIVE	S MANOR DRIVE	N MANOR DRIVE	Asphalt	S	314	28	8,794	24	284
LNSG::MDSN ST::10	MADISON STREET	ROY STREET	COMMUNITY STREET	Asphalt	S	334	28	9,340	39	343
LNSG::MDSN ST::20	MADISON STREET	COMMUNITY STREET	HENRY STREET	Asphalt	S	330	28	9,239	58	513
LNSG::MDSN ST::30	MADISON STREET	HENRY STREET	WILLIAM STREET	Asphalt	S	630	28	17,638	24	539
LNSG::MDSN ST::40	MADISON STREET	WILLIAM STREET	WENTWORTH AVENUE	Asphalt	S	702	28	19,669	36	826
LNSG::MDSN ST::50	MADISON STREET	WENTWORTH AVENUE	MAPLE STREET	Asphalt	S	837	28	23,428	20	1,011
LNSG::MLLR DR::10	MILLER DRIVE	186TH STREET	SCHULTZ DRIVE S	Asphalt	S	476	28	13,329	100	389
LNSG::MLLR DR::20	MILLER DRIVE	ANN STREET	SCHULTZ DRIVE N	Asphalt	S	377	28	10,553	73	323
LNSG::MNR ST::10	MONROE STREET	HENRY STREET	WILLIAM STREET	Asphalt	S	631	28	17,679	20	1,003
LNSG::MNR ST::20	MONROE STREET	WILLIAM STREET	WENTWORTH AVENUE	Asphalt	S	699	28	19,580	72	479
LNSG::MNR ST::30	MONROE STREET	WENTWORTH AVENUE	MAPLE STREET	Asphalt	S	841	28	23,542	54	273
LNSG::MPL AVE::10	MAPLE AVENUE	WENTWORTH AVENUE	SHIRLEY DRIVE	Asphalt	S	294	28	8,221	13	399
LNSG::MPL AVE::20	MAPLE AVENUE	SHIRLEY DRIVE	MAPLE COURT	Asphalt	S	417	28	11,685	24	357
LNSG::MPL AVE::30	MAPLE AVENUE	176TH PLACE	MAPLE COURT	Asphalt	S	497	28	13,914	25	452
LNSG::MPL AVE::40	MAPLE AVENUE	177TH STREET	176TH PLACE	Asphalt	S	373	28	10,457	82	328
LNSG::MPL AVE::50	MAPLE AVENUE	177TH PLACE	177TH STREET	Asphalt	S	328	28	9,190	74	386
LNSG::MPL AVE::60	MAPLE AVENUE	178TH STREET	177TH PLACE	Asphalt	S	332	28	9,291	81	211
LNSG::MPL CT::10	MAPLE COURT	MAPLE AVENUE	END	Asphalt	S	159	28	4,451	45	688
LNSG::MPL ST::10	MAPLE STREET	193RD STREET	192ND STREET	Asphalt	S	458	28	12,829	24	337
LNSG::MPL ST::100	MAPLE STREET	WASHINGTON STREET	MADISON STREET	Asphalt	S	359	28	10,038	53	299
LNSG::MPL ST::110	MAPLE STREET	MADISON STREET	MONROE STREET	Asphalt	S	343	28	9,599	40	202
LNSG::MPL ST::120	MAPLE STREET	MONROE STREET	ADAMS STREET	Asphalt	S	354	28	9,920	48	270
LNSG::MPL ST::130	MAPLE STREET	ADAMS STREET	JACKSON STREET	Asphalt	S	344	28	9,637	50	203
LNSG::MPL ST::140	MAPLE STREET	JACKSON STREET	178TH STREET	Asphalt	S	358	28	10,011	44	257
LNSG::MPL ST::20	MAPLE STREET	TERRACE DRIVE	ILLI INDI DRIVE	Asphalt	S	873	28	24,447	67	175

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LNSG::MPL ST::30	MAPLE STREET	TERRACE DRIVE	TERRACE DRIVE	Asphalt	S	63	28	1,762	92	781
LNSG::MPL ST::40	MAPLE STREET	TERRACE DRIVE	TERRACE DRIVE	Asphalt	S	35	28	979	88	814
LNSG::MPL ST::50	MAPLE STREET	TERRACE DRIVE	TERRACE DRIVE	Asphalt	S	72	28	2,019	84	551
LNSG::MPL ST::60	MAPLE STREET	TERRACE DRIVE	TERRACE DRIVE	Asphalt	S	41	28	1,134	77	618
LNSG::MPL ST::70	MAPLE STREET	TERRACE DRIVE	END	Asphalt	S	494	28	13,823	55	387
LNSG::MPL ST::80	MAPLE STREET	RIDGE ROAD	LAKE STREET	Asphalt	S	623	28	17,453	77	261
LNSG::MPL ST::90	MAPLE STREET	LAKE STREET	RANDOLPH STREET	Asphalt	S	364	28	10,197	74	228
LNSG::MRTL CT::10	MERTLE COURT	CENTRAL STREET	WENTWORTH AVENUE	Asphalt	S	490	28	13,713	77	291
LNSG::N MNR DR::10	N MANOR DRIVE	W LOUISE DRIVE	LOUISE DRIVE	Asphalt	S	839	28	23,503	28	212
LNSG::OK AVE::10	OAK AVENUE	188TH STREET	187TH PLACE	Asphalt	S	310	28	8,672	30	279
LNSG::OK AVE::100	OAK AVENUE	OAK AVENUE	173RD STREET	Asphalt	P	61	28	1,706	82	268
LNSG::OK AVE::110	OAK AVENUE	173RD STREET	172ND STREET	Asphalt	S	654	28	18,316	77	177
LNSG::OK AVE::20	OAK AVENUE	187TH PLACE	187TH STREET	Asphalt	S	334	28	9,347	24	229
LNSG::OK AVE::30	OAK AVENUE	187TH STREET	186TH PLACE	Asphalt	S	346	28	9,676	23	359
LNSG::OK AVE::40	OAK AVENUE	186TH PLACE	186TH STREET	Asphalt	S	349	28	9,783	25	346
LNSG::OK AVE::50	OAK AVENUE	186TH STREET	185TH STREET	Asphalt	S	659	28	18,447	46	522
LNSG::OK AVE::60	OAK AVENUE	185TH STREET	184TH STREET	Asphalt	S	662	28	18,546	59	274
LNSG::OK AVE::70	OAK AVENUE	INDIANA AVENUE	180TH STREET	Asphalt	S	772	28	21,608	29	323
LNSG::OK AVE::80	OAK AVENUE	BERNICE ROAD	BERNICE AVENUE	Asphalt	S	466	28	13,060	75	235
LNSG::OK AVE::90	OAK AVENUE	BERNICE ROAD	OAK AVENUE	Asphalt	P	609	28	17,057	48	187
LNSG::OK ST::10	OAK STREET	GLENWOOD LANSING ROAD	194TH PLACE	Asphalt	S	188	28	5,256	36	359
LNSG::OK ST::20	OAK STREET	182ND PLACE	END	Asphalt	S	221	28	6,183	91	211
LNSG::OK ST::30	OAK STREET	182ND PLACE	OAK STREET	Asphalt	S	109	28	3,056	67	704
LNSG::OK ST::40	OAK STREET	OAK STREET	RIDGE ROAD	Asphalt	S	273	28	7,641	75	226
LNSG::OTT ST::10	OTTO STREET	BURNHAM AVENUE	WALTER STREET	Asphalt	S	899	28	25,184	45	303
LNSG::OTT ST::20	OTTO STREET	WALTER STREET	HENRY STREET	Asphalt	S	423	28	11,844	100	199
LNSG::PRK AVE::10	PARK AVENUE	195TH COURT	END	Asphalt	S	137	28	3,834	36	338
LNSG::PRK AVE::100	PARK AVENUE	180TH STREET	179TH STREET	Asphalt	S	661	28	18,497	47	316
LNSG::PRK AVE::110	PARK AVENUE	179TH STREET	178TH STREET	Asphalt	S	662	28	18,544	84	180
LNSG::PRK AVE::120	PARK AVENUE	BERNICE ROAD	173RD STREET	Asphalt	S	670	28	18,747	50	388
LNSG::PRK AVE::130	PARK AVENUE	173RD STREET	172ND STREET	Asphalt	S	659	28	18,441	63	253
LNSG::PRK AVE::140	PARK AVENUE	172ND STREET	171ST STREET	Asphalt	S	659	28	18,465	49	264
LNSG::PRK AVE::150	PARK AVENUE	171ST STREET	170TH STREET	Asphalt	S	656	28	18,368	61	253
LNSG::PRK AVE::20	PARK AVENUE	194TH PLACE	195TH COURT	Asphalt	S	318	28	8,898	78	256
LNSG::PRK AVE::30	PARK AVENUE	GLENWOOD LANSING ROAD	194TH PLACE	Asphalt	S	210	28	5,890	31	312
LNSG::PRK AVE::40	PARK AVENUE	191ST PLACE	191ST STREET	Asphalt	S	294	28	8,233	37	254
LNSG::PRK AVE::50	PARK AVENUE	190TH STREET	189TH STREET	Asphalt	S	302	28	8,443	51	286
LNSG::PRK AVE::60	PARK AVENUE	189TH STREET	188TH PLACE	Asphalt	S	530	28	14,853	58	284
LNSG::PRK AVE::70	PARK AVENUE	188TH PLACE	188TH STREET	Asphalt	S	184	28	5,152	58	388
LNSG::PRK AVE::80	PARK AVENUE	RIDGE ROAD	181ST STREET	Asphalt	S	656	28	18,367	61	234
LNSG::PRK AVE::90	PARK AVENUE	181ST STREET	180TH STREET	Asphalt	S	665	28	18,617	58	320
LNSG::PRK BLVD::10	PARK BOULEVARD	178TH STREET	CHICAGO AVENUE	Asphalt	S	623	28	17,446	58	178
LNSG::PXTN AVE::10	PAXTON AVENUE	START	END	Asphalt	S	275	28	7,710	84	232

Pavement ID	Road Name	From	To	Surface	Rank	Length (FT)	Width (FT)	Area (SF)	PCI	IRI
LNSG::PXTN AVE::20	PAXTON AVENUE	START	177TH PLACE	Asphalt	S	287	28	8,036	85	268
LNSG::PXTN AVE::30	PAXTON AVENUE	177TH PLACE	177TH STREET	Asphalt	S	419	28	11,725	73	247
LNSG::PXTN AVE::40	PAXTON AVENUE	177TH STREET	176TH PLACE	Asphalt	S	289	28	8,092	50	273
LNSG::PXTN AVE::50	PAXTON AVENUE	176TH PLACE	FRONTAGE ROAD	Asphalt	S	758	28	21,226	47	258
LNSG::RCD AVE::10	ARCADIA AVENUE	185TH STREET	END	Asphalt	S	201	28	5,636	56	518
LNSG::RCD AVE::20	ARCADIA AVENUE	185TH STREET	184TH STREET	Asphalt	S	658	28	18,431	76	186
LNSG::RCD AVE::30	ARCADIA AVENUE	THORNTON LANSING ROAD	END	Asphalt	S	312	28	8,730	89	280
LNSG::RCD AVE::40	ARCADIA AVENUE	THORNTON LANSING ROAD	180TH STREET	Asphalt	S	218	28	6,095	78	263
LNSG::RCD AVE::50	ARCADIA AVENUE	180TH STREET	179TH STREET	Asphalt	S	658	28	18,435	55	415
LNSG::RCD AVE::60	ARCADIA AVENUE	179TH STREET	178TH STREET	Asphalt	S	653	28	18,274	54	380
LNSG::RCD AVE::70	ARCADIA AVENUE	178TH STREET	END	Asphalt	S	645	28	18,068	71	247
LNSG::RDG RD::10	RIDGE ROAD	TORRENCE AVENUE	RIDGE ROAD	Asphalt	P	263	40	10,528	52	176
LNSG::RDG RD::100	RIDGE ROAD	OAKWOOD AVENUE	RIDGEWOOD AVENUE	Asphalt	P	337	40	13,465	66	272
LNSG::RDG RD::110	RIDGE ROAD	RIDGEWOOD AVENUE	CHICAGO AVENUE	Asphalt	P	329	40	13,145	67	172
LNSG::RDG RD::120	RIDGE ROAD	CHICAGO AVENUE	PARK AVENUE	Asphalt	P	337	40	13,461	68	103
LNSG::RDG RD::130	RIDGE ROAD	PARK AVENUE	OAK STREET	Asphalt	P	78	40	3,114	78	140
LNSG::RDG RD::140	RIDGE ROAD	OAK STREET	LORENZ AVENUE	Asphalt	P	252	40	10,087	56	126
LNSG::RDG RD::150	RIDGE ROAD	LORENZ AVENUE	BURNHAM AVENUE	Asphalt	P	672	55	36,966	59	155
LNSG::RDG RD::160	RIDGE ROAD	BURNHAM AVENUE	WALTER STREET	Asphalt	P	372	55	20,436	80	149
LNSG::RDG RD::170	RIDGE ROAD	WALTER STREET	ROY STREET	Asphalt	P	281	45	12,628	64	231
LNSG::RDG RD::180	RIDGE ROAD	ROY STREET	ROY STREET	Asphalt	P	99	40	3,947	61	167
LNSG::RDG RD::190	RIDGE ROAD	ROY STREET	ADA STREET	Asphalt	P	375	40	15,020	57	131
LNSG::RDG RD::20	RIDGE ROAD	RIDGE ROAD	FRED STREET	Asphalt	P	421	40	16,844	46	216
LNSG::RDG RD::200	RIDGE ROAD	ADA STREET	GRANT STREET	Asphalt	P	350	40	14,005	63	164
LNSG::RDG RD::210	RIDGE ROAD	HENRY STREET	SHERMAN STREET	Asphalt	P	358	40	14,328	58	154
LNSG::RDG RD::220	RIDGE ROAD	SHERMAN STREET	WILLIAM STREET	Asphalt	P	119	40	4,768	62	205
LNSG::RDG RD::230	RIDGE ROAD	WILLIAM STREET	WEST STREET	Asphalt	P	75	40	2,983	75	109
LNSG::RDG RD::240	RIDGE ROAD	WEST STREET	WENTWORTH AVENUE	Asphalt	P	653	50	32,649	58	159
LNSG::RDG RD::250	RIDGE ROAD	WENTWORTH AVENUE	LANGE STREET	Asphalt	P	381	50	19,029	67	191
LNSG::RDG RD::260	RIDGE ROAD	LANGE STREET	MAPLE STREET	Asphalt	P	458	45	20,621	75	168
LNSG::RDG RD::270	RIDGE ROAD	MAPLE STREET	STATE LINE AVENUE	Asphalt	P	333	40	13,331	51	240
LNSG::RDG RD::30	RIDGE ROAD	FRED STREET	GLEN TERRACE	Asphalt	P	109	40	4,376	65	171
LNSG::RDG RD::40	RIDGE ROAD	GLEN TERRACE	OAKLEY AVENUE	Asphalt	P	542	40	21,675	55	108
LNSG::RDG RD::50	RIDGE ROAD	OAKLEY AVENUE	EXCHANGE AVENUE	Asphalt	P	536	40	21,452	60	158
LNSG::RDG RD::60	RIDGE ROAD	EXCHANGE AVENUE	WARD STREET	Asphalt	P	556	40	22,260	53	124
LNSG::RDG RD::70	RIDGE ROAD	WARD STREET	SCHOOL STREET	Asphalt	P	248	40	9,925	53	146
LNSG::RDG RD::80	RIDGE ROAD	SCHOOL STREET	WILDWOOD AVENUE	Asphalt	P	320	40	12,809	68	163
LNSG::RDG RD::90	RIDGE ROAD	WILDWOOD AVENUE	OAKWOOD AVENUE	Asphalt	P	331	40	13,245	63	236
LNSG::RDGLD AVE::10	RIDGELAND AVENUE	START	180TH STREET	Asphalt	S	397	28	11,116	95	297
LNSG::RDGWD AVE::10	RIDGEWOOD AVENUE	190TH STREET	189TH PLACE	Asphalt	S	305	28	8,541	53	235
LNSG::RDGWD AVE::100	RIDGEWOOD AVENUE	180TH STREET	179TH STREET	Asphalt	S	662	28	18,531	86	234
LNSG::RDGWD AVE::110	RIDGEWOOD AVENUE	179TH STREET	178TH STREET	Asphalt	S	663	28	18,574	90	275
LNSG::RDGWD AVE::120	RIDGEWOOD AVENUE	172ND STREET	171ST STREET	Asphalt	S	672	28	18,805	62	223

Pavement ID	Road Name	From	To	Surface	Rank	Length (FT)	Width (FT)	Area (SF)	PCI	IRI
LNSG::RDGWD AVE::130	RIDGEWOOD AVENUE	171ST STREET	170TH STREET	Asphalt	S	655	28	18,350	63	310
LNSG::RDGWD AVE::20	RIDGEWOOD AVENUE	189TH PLACE	189TH STREET	Asphalt	S	292	28	8,180	56	220
LNSG::RDGWD AVE::30	RIDGEWOOD AVENUE	189TH STREET	188TH PLACE	Asphalt	S	233	28	6,518	72	216
LNSG::RDGWD AVE::40	RIDGEWOOD AVENUE	186TH STREET	185TH STREET	Asphalt	S	656	28	18,373	85	298
LNSG::RDGWD AVE::50	RIDGEWOOD AVENUE	185TH STREET	184TH STREET	Asphalt	S	663	28	18,559	22	299
LNSG::RDGWD AVE::60	RIDGEWOOD AVENUE	184TH STREET	183RD STREET	Asphalt	S	661	28	18,497	23	509
LNSG::RDGWD AVE::70	RIDGEWOOD AVENUE	183RD STREET	RIDGE ROAD	Asphalt	S	659	28	18,440	26	359
LNSG::RDGWD AVE::80	RIDGEWOOD AVENUE	RIDGE ROAD	181ST STREET	Asphalt	S	663	28	18,570	29	513
LNSG::RDGWD AVE::90	RIDGEWOOD AVENUE	181ST STREET	180TH STREET	Asphalt	S	665	28	18,626	24	533
LNSG::RLRD AVE::10	RAILROAD AVENUE	BERNICE ROAD	BERNICE AVENUE	Asphalt	S	370	28	10,371	60	319
LNSG::RNDLPH ST::07	RANDOLPH STREET	181ST PLACE	HENRY STREET	Asphalt	S	405	28	11,330	94	295
LNSG::RNDLPH ST::10	RANDOLPH STREET	HENRY STREET	WILLIAM STREET	Asphalt	S	627	28	17,554	78	245
LNSG::RNDLPH ST::20	RANDOLPH STREET	WILLIAM STREET	WENTWORTH AVENUE	Asphalt	S	709	28	19,849	78	245
LNSG::RNDLPH ST::30	RANDOLPH STREET	WENTWORTH AVENUE	LANGE STREET	Asphalt	S	379	28	10,616	45	435
LNSG::RNDLPH ST::40	RANDOLPH STREET	LANGE STREET	MAPLE STREET	Asphalt	S	441	28	12,341	70	213
LNSG::RPRT DR::10	AIRPORT DRIVE	GLENWOOD LANSING ROAD	END	Asphalt	S	1,314	28	36,800	75	252
LNSG::RS AVE::10	ROSE AVENUE	185TH COURT	185TH PLACE	Asphalt	S	288	28	8,076	37	424
LNSG::RS AVE::20	ROSE AVENUE	185TH STREET	184TH PLACE	Asphalt	S	298	28	8,332	31	569
LNSG::RS AVE::30	ROSE AVENUE	183RD PLACE	182ND PLACE	Asphalt	S	745	28	20,850	55	472
LNSG::RS AVE::40	ROSE AVENUE	THORNTON LANSING ROAD	END	Asphalt	S	1,383	28	38,736	86	185
LNSG::RSWD DR::10	ROSEWOOD DRIVE	177TH PLACE	END	Asphalt	S	272	28	7,606	35	443
LNSG::RSWD DR::20	ROSEWOOD DRIVE	177TH PLACE	177TH STREET	Asphalt	S	299	28	8,379	41	302
LNSG::RSWD DR::30	ROSEWOOD DRIVE	177TH STREET	176TH PLACE	Asphalt	S	392	28	10,987	54	455
LNSG::RY ST::10	ROY STREET	192ND STREET	191ST STREET	Asphalt	S	543	28	15,193	23	426
LNSG::RY ST::100	ROY STREET	ADAMS STREET	178TH STREET	Asphalt	S	696	28	19,489	55	400
LNSG::RY ST::110	ROY STREET	178TH STREET	177TH STREET	Asphalt	S	662	28	18,526	70	480
LNSG::RY ST::120	ROY STREET	177TH STREET	176TH STREET	Asphalt	S	663	28	18,564	72	325
LNSG::RY ST::130	ROY STREET	176TH STREET	175TH STREET	Asphalt	S	658	28	18,423	67	377
LNSG::RY ST::140	ROY STREET	BERNICE ROAD	BERNICE AVENUE	Asphalt	S	412	28	11,547	69	257
LNSG::RY ST::150	ROY STREET	BERNICE ROAD	173RD STREET	Asphalt	S	663	28	18,572	62	229
LNSG::RY ST::160	ROY STREET	173RD STREET	172ND STREET	Asphalt	S	660	28	18,478	55	269
LNSG::RY ST::170	ROY STREET	172ND STREET	171ST STREET	Asphalt	S	658	28	18,416	54	277
LNSG::RY ST::20	ROY STREET	SCHULTZ DRIVE S	SCHULTZ DRIVE N	Asphalt	S	469	28	13,130	91	337
LNSG::RY ST::30	ROY STREET	ANN STREET	ILLINOIS STREET	Asphalt	S	541	28	15,147	94	265
LNSG::RY ST::40	ROY STREET	ILLINOIS STREET	RIDGE ROAD	Asphalt	S	915	28	25,615	94	288
LNSG::RY ST::50	ROY STREET	RIDGE ROAD	181ST PLACE	Asphalt	S	479	28	13,412	83	404
LNSG::RY ST::60	ROY STREET	181ST PLACE	181ST STREET	Asphalt	S	224	28	6,260	92	327
LNSG::RY ST::70	ROY STREET	181ST STREET	ROY STREET	Asphalt	S	212	28	5,945	59	296
LNSG::RY ST::80	ROY STREET	ROY STREET	MADISON STREET	Asphalt	S	364	28	10,186	50	698
LNSG::RY ST::90	ROY STREET	MADISON STREET	ADAMS STREET	Asphalt	S	709	28	19,853	42	287
LNSG::S MNR DR::10	S MANOR DRIVE	BURNHAM AVENUE	W LOUISE DRIVE	Asphalt	S	169	28	4,731	57	272
LNSG::S MNR DR::20	S MANOR DRIVE	W LOUISE DRIVE	LOUISE DRIVE	Asphalt	S	1,036	28	29,018	33	207
LNSG::S MNR DR::30	S MANOR DRIVE	LOUISE DRIVE	SHARON COURT	Asphalt	S	319	28	8,922	27	177

Pavement ID	Road Name	From	To	Surface	Rank	Length (FT)	Width (FT)	Area (SF)	PCI	IRI
LNSG::S MNR DR::40	S MANOR DRIVE	SHARON COURT	BARBARA COURT	Asphalt	S	314	28	8,806	26	266
LNSG::S MNR DR::50	S MANOR DRIVE	BARBARA COURT	WENTWORTH AVENUE	Asphalt	S	870	28	24,347	24	247
LNSG::SCHL ST::10	SCHOOL STREET	185TH STREET	END	Asphalt	S	112	28	3,145	42	637
LNSG::SCHL ST::100	SCHOOL STREET	179TH STREET	178TH STREET	Asphalt	S	650	28	18,202	72	240
LNSG::SCHL ST::110	SCHOOL STREET	178TH STREET	177TH STREET	Asphalt	S	651	28	18,216	35	307
LNSG::SCHL ST::20	SCHOOL STREET	185TH STREET	184TH STREET	Asphalt	S	662	28	18,543	24	444
LNSG::SCHL ST::30	SCHOOL STREET	184TH STREET	183RD STREET	Asphalt	S	656	28	18,371	50	263
LNSG::SCHL ST::40	SCHOOL STREET	183RD STREET	RIDGE ROAD	Asphalt	S	660	28	18,477	91	271
LNSG::SCHL ST::50	SCHOOL STREET	RIDGE ROAD	INDIANA AVENUE	Asphalt	S	322	28	9,012	64	347
LNSG::SCHL ST::60	SCHOOL STREET	INDIANA AVENUE	181ST STREET	Asphalt	S	340	28	9,517	70	213
LNSG::SCHL ST::70	SCHOOL STREET	181ST STREET	181ST STREET	Asphalt	S	86	28	2,419	75	318
LNSG::SCHL ST::80	SCHOOL STREET	181ST STREET	180TH STREET	Asphalt	S	582	28	16,297	83	196
LNSG::SCHL ST::90	SCHOOL STREET	180TH STREET	179TH STREET	Asphalt	S	659	28	18,465	59	242
LNSG::SCHLZ DR N::10	SCHULTZ DRIVE	GREENBAY AVENUE	BURNHAM AVENUE	Asphalt	S	346	28	9,685	28	563
LNSG::SCHLZ DR N::100	SCHULTZ DRIVE	SHERMAN STREET	MILLER DRIVE	Asphalt	S	342	28	9,573	86	403
LNSG::SCHLZ DR N::110	SCHULTZ DRIVE	MILLER DRIVE	SCHULTZ DRIVE S	Asphalt	S	265	28	7,426	93	366
LNSG::SCHLZ DR N::120	SCHULTZ DRIVE	SCHULTZ DRIVE N	WENTWORTH AVENUE	Asphalt	S	212	28	5,924	32	397
LNSG::SCHLZ DR N::20	SCHULTZ DRIVE	BURNHAM AVENUE	SCHULTZ DRIVE N	Asphalt	S	242	28	6,771	51	600
LNSG::SCHLZ DR N::30	SCHULTZ DRIVE	SCHULTZ DRIVE	WALTER STREET	Asphalt	S	200	28	5,588	58	362
LNSG::SCHLZ DR N::40	SCHULTZ DRIVE	WALTER STREET	ROY STREET	Asphalt	S	381	28	10,663	84	292
LNSG::SCHLZ DR N::50	SCHULTZ DRIVE	ROY STREET	ADA STREET	Asphalt	S	315	28	8,815	92	250
LNSG::SCHLZ DR N::60	SCHULTZ DRIVE	ADA STREET	ADA STREET	Asphalt	S	57	28	1,608	95	450
LNSG::SCHLZ DR N::70	SCHULTZ DRIVE	ADA STREET	GRANT STREET	Asphalt	S	454	28	12,708	37	471
LNSG::SCHLZ DR N::80	SCHULTZ DRIVE	GRANT STREET	SHERMAN STREET	Asphalt	S	246	28	6,890	70	571
LNSG::SCHLZ DR N::90	SCHULTZ DRIVE	SHERMAN STREET	SHERMAN STREET	Asphalt	S	66	28	1,851	81	670
LNSG::SCHLZ DR S::10	SCHULTZ DRIVE S	SCHULTZ DRIVE	WALTER STREET	Asphalt	S	202	28	5,661	51	265
LNSG::SCHLZ DR S::20	SCHULTZ DRIVE S	WALTER STREET	ROY STREET	Asphalt	S	387	28	10,828	93	224
LNSG::SCHLZ DR S::30	SCHULTZ DRIVE S	ROY STREET	ADA STREET	Asphalt	S	321	28	9,000	83	313
LNSG::SCHLZ DR S::40	SCHULTZ DRIVE S	ADA STREET	ADA STREET	Asphalt	S	64	28	1,796	95	368
LNSG::SCHLZ DR S::50	SCHULTZ DRIVE S	ADA STREET	GRANT STREET	Asphalt	S	443	28	12,400	95	230
LNSG::SCHLZ DR S::60	SCHULTZ DRIVE S	GRANT STREET	SHERMAN STREET	Asphalt	S	256	28	7,169	48	276
LNSG::SCHLZ DR S::70	SCHULTZ DRIVE S	SHERMAN STREET	SHERMAN STREET	Asphalt	S	62	28	1,739	62	413
LNSG::SCHLZ DR S::80	SCHULTZ DRIVE S	SHERMAN STREET	MILLER DRIVE	Asphalt	S	423	28	11,840	49	249
LNSG::SCHLZ DR S::90	SCHULTZ DRIVE S	MILLER DRIVE	SCHULTZ DRIVE N	Asphalt	S	210	28	5,885	92	304
LNSG::SCNB AVE::10	ESCANABA AVENUE	186TH STREET	END	Asphalt	S	432	28	12,108	54	229
LNSG::SCNB AVE::20	ESCANABA AVENUE	INDIANA AVENUE	180TH STREET	Asphalt	S	858	28	24,012	52	497
LNSG::SCNB AVE::30	ESCANABA AVENUE	180TH STREET	179TH STREET	Asphalt	S	657	28	18,399	73	234
LNSG::SCNB AVE::40	ESCANABA AVENUE	179TH STREET	178TH STREET	Asphalt	S	656	28	18,356	60	179
LNSG::SCNB AVE::50	ESCANABA AVENUE	178TH STREET	177TH STREET	Asphalt	S	652	28	18,269	59	227
LNSG::SHRMN ST::10	SHERMAN STREET	193RD STREET	193RD STREET	Asphalt	S	155	28	4,340	30	418
LNSG::SHRMN ST::100	SHERMAN STREET	186TH STREET	SCHULTZ DRIVE S	Asphalt	S	371	28	10,388	100	223
LNSG::SHRMN ST::110	SHERMAN STREET	SCHULTZ DRIVE S	SCHULTZ DRIVE N	Asphalt	S	465	28	13,021	23	256
LNSG::SHRMN ST::120	SHERMAN STREET	SCHULTZ DRIVE N	ANN STREET	Asphalt	S	308	28	8,631	26	304

Pavement ID	Road Name	From	To	Surface	Rank	Length (FT)	Width (FT)	Area (SF)	PCI	IRI
LNSG::SHRMN ST::130	SHERMAN STREET	ANN STREET	ILLINOIS STREET	Asphalt	S	550	28	15,396	42	433
LNSG::SHRMN ST::140	SHERMAN STREET	ILLINOIS STREET	STEWART AVENUE	Asphalt	S	284	28	7,957	54	327
LNSG::SHRMN ST::150	SHERMAN STREET	RIDGE ROAD	END	Asphalt	S	281	28	7,873	33	205
LNSG::SHRMN ST::20	SHERMAN STREET	193RD STREET	192ND STREET	Asphalt	S	655	28	18,343	26	277
LNSG::SHRMN ST::30	SHERMAN STREET	192ND STREET	191ST PLACE	Asphalt	S	370	28	10,351	25	273
LNSG::SHRMN ST::40	SHERMAN STREET	191ST PLACE	191ST STREET	Asphalt	S	249	28	6,972	55	252
LNSG::SHRMN ST::50	SHERMAN STREET	191ST STREET	190TH PLACE	Asphalt	S	280	28	7,832	53	501
LNSG::SHRMN ST::60	SHERMAN STREET	WILLIAM STREET	BERNADINE STREET	Asphalt	S	498	28	13,932	54	216
LNSG::SHRMN ST::70	SHERMAN STREET	188TH STREET	WILLIAM STREET	Asphalt	S	678	28	18,982	46	198
LNSG::SHRMN ST::80	SHERMAN STREET	188TH STREET	187TH STREET	Asphalt	S	683	28	19,111	45	260
LNSG::SHRMN ST::90	SHERMAN STREET	187TH STREET	186TH STREET	Asphalt	S	682	28	19,082	47	268
LNSG::SHRN CT::10	SHARON COURT	S MANOR DRIVE	END	Asphalt	S	584	28	16,349	25	312
LNSG::SHRY DR::10	SHIRLEY DRIVE	MAPLE AVENUE	END	Asphalt	S	367	28	10,277	24	464
LNSG::SLR LN::10	SAILOR LANE	183RD PLACE	183RD STREET	Asphalt	S	304	28	8,515	85	218
LNSG::ST LN AVE::10	STATE LINE AVENUE	RIDGE ROAD	LAKE STREET	Asphalt	S	678	28	18,977	24	368
LNSG::ST LN AVE::20	STATE LINE AVENUE	LAKE STREET	WASHINGTON STREET	Asphalt	S	696	28	19,491	24	309
LNSG::ST LN AVE::30	STATE LINE AVENUE	WASHINGTON STREET	177TH STREET	Asphalt	S	2,411	28	67,505	38	250
LNSG::ST LN AVE::40	STATE LINE AVENUE	177TH STREET	176TH PLACE	Asphalt	S	375	28	10,505	45	514
LNSG::ST LN AVE::50	STATE LINE AVENUE	176TH PLACE	175TH STREET	Asphalt	S	666	28	18,654	66	146
LNSG::ST NDRW ST::10	ST ANDREWS STREET	STONY ISLAND AVENUE	END	Asphalt	S	271	28	7,592	26	205
LNSG::STN L AVE::10	STONY ISLAND AVENUE	186TH STREET	185TH STREET	Asphalt	P	477	28	13,365	45	376
LNSG::STN L AVE::20	STONY ISLAND AVENUE	185TH STREET	ST ANDREWS STREET	Asphalt	P	186	28	5,206	62	263
LNSG::STN L AVE::30	STONY ISLAND AVENUE	ST ANDREWS STREET	184TH STREET	Asphalt	P	654	28	18,304	62	333
LNSG::STN L AVE::40	STONY ISLAND AVENUE	182ND STREET	181ST STREET	Asphalt	P	666	28	18,646	52	275
LNSG::STN L AVE::50	STONY ISLAND AVENUE	181ST STREET	180TH STREET	Asphalt	P	544	28	15,229	73	248
LNSG::STN L AVE::60	STONY ISLAND AVENUE	180TH STREET	180TH STREET	Asphalt	P	95	28	2,653	83	248
LNSG::STN L AVE::70	STONY ISLAND AVENUE	180TH STREET	THORNTON LANSING ROAD	Asphalt	P	641	28	17,958	81	206
LNSG::STWRT AVE::10	STEWART AVENUE	GRANT STREET	SHERMAN STREET	Asphalt	S	517	28	14,469	92	219
LNSG::STWRT AVE::20	STEWART AVENUE	SHERMAN STREET	ILLINOIS STREET	Asphalt	S	360	28	10,078	93	252
LNSG::STWRT AVE::30	STEWART AVENUE	ILLINOIS STREET	WENTWORTH AVENUE	Asphalt	S	970	28	27,162	94	151
LNSG::THRTLNS RD::10	THORNTON-LANSING ROAD	TORRENCE AVENUE	GLEN OAK AVENUE	Asphalt	S	333	35	11,662	100	175
LNSG::THRTLNS RD::20	THORNTON-LANSING ROAD	GLEN OAK AVENUE	ARCADIA AVENUE	Asphalt	S	338	30	10,148	90	200
LNSG::TN AVE::10	TON AVENUE	BERNICE ROAD	BERNICE AVENUE	Asphalt	S	338	28	9,461	54	295
LNSG::TRRC DR::10	TERRACE DRIVE	WENTWORTH AVENUE	LANGE STREET	Asphalt	S	472	28	13,218	79	170
LNSG::TRRC DR::20	TERRACE DRIVE	LANGE STREET	MAPLE STREET	Asphalt	S	256	28	7,159	86	297
LNSG::TRRC DR::30	TERRACE DRIVE	MAPLE STREET	WILLOW LANE	Asphalt	S	221	28	6,188	55	436
LNSG::TRRC DR::40	TERRACE DRIVE	WILLOW LANE	END	Asphalt	S	155	28	4,331	93	397
LNSG::VLBRCHT RD::10	VOLBRECHT ROAD	THORNTON LANSING ROAD	FRONTAGE ROAD	Asphalt	S	2,172	28	60,818	50	262
LNSG::VLT RD::10	VIOLET ROAD	185TH STREET	184TH PLACE	Asphalt	S	280	28	7,831	28	510
LNSG::VLT RD::20	VIOLET ROAD	184TH PLACE	FOREST COURT	Asphalt	S	151	28	4,222	46	374
LNSG::VLT RD::30	VIOLET ROAD	FOREST COURT	184TH STREET	Asphalt	S	130	28	3,652	89	511
LNSG::VLT RD::40	VIOLET ROAD	184TH STREET	183RD PLACE	Asphalt	S	299	28	8,359	63	402
LNSG::VLT RD::50	VIOLET ROAD	RIDGE ROAD	INDIANA AVENUE	Asphalt	S	642	28	17,980	33	392

Pavement ID	Road Name	From	To	Surface	Rank	Length (FT)	Width (FT)	Area (SF)	PCI	IRI
LNSG::W LS DR::10	W LOUISE DRIVE	S MANOR DRIVE	N MANOR DRIVE	Asphalt	S	207	28	5,808	43	271
LNSG::W LS DR::20	W LOUISE DRIVE	N MANOR DRIVE	LOUISE DRIVE	Asphalt	S	693	28	19,403	33	308
LNSG::WHTMN LN::10	WHITMAN LANE	181ST STREET	END	Asphalt	S	133	28	3,726	95	343
LNSG::WHTMN LN::20	WHITMAN LANE	181ST STREET	180TH PLACE	Asphalt	S	586	28	16,415	51	345
LNSG::WLDWD AVE::10	WILDWOOD AVENUE	GLENWOOD LANSING ROAD	194TH STREET	Asphalt	S	202	28	5,666	100	178
LNSG::WLDWD AVE::100	WILDWOOD AVENUE	189TH PLACE	189TH STREET	Asphalt	S	489	28	13,689	74	287
LNSG::WLDWD AVE::110	WILDWOOD AVENUE	189TH STREET	188TH STREET	Asphalt	S	426	28	11,941	64	219
LNSG::WLDWD AVE::120	WILDWOOD AVENUE	186TH STREET	END	Asphalt	S	217	28	6,067	37	506
LNSG::WLDWD AVE::130	WILDWOOD AVENUE	185TH STREET	END	Asphalt	S	282	28	7,908	23	419
LNSG::WLDWD AVE::140	WILDWOOD AVENUE	185TH STREET	184TH STREET	Asphalt	S	662	28	18,548	33	324
LNSG::WLDWD AVE::150	WILDWOOD AVENUE	184TH STREET	183RD STREET	Asphalt	S	658	28	18,412	50	415
LNSG::WLDWD AVE::160	WILDWOOD AVENUE	183RD STREET	RIDGE ROAD	Asphalt	S	657	28	18,396	50	372
LNSG::WLDWD AVE::170	WILDWOOD AVENUE	181ST STREET	END	Asphalt	S	379	28	10,611	89	315
LNSG::WLDWD AVE::180	WILDWOOD AVENUE	181ST STREET	180TH STREET	Asphalt	S	667	28	18,686	90	320
LNSG::WLDWD AVE::190	WILDWOOD AVENUE	180TH STREET	179TH STREET	Asphalt	S	660	28	18,487	84	287
LNSG::WLDWD AVE::20	WILDWOOD AVENUE	194TH STREET	193RD PLACE	Asphalt	S	311	28	8,716	100	123
LNSG::WLDWD AVE::200	WILDWOOD AVENUE	179TH STREET	178TH STREET	Asphalt	S	648	28	18,149	73	257
LNSG::WLDWD AVE::30	WILDWOOD AVENUE	193RD PLACE	193RD STREET	Asphalt	S	308	28	8,621	25	238
LNSG::WLDWD AVE::40	WILDWOOD AVENUE	193RD STREET	192ND PLACE	Asphalt	S	343	28	9,618	61	256
LNSG::WLDWD AVE::50	WILDWOOD AVENUE	192ND PLACE	192ND STREET	Asphalt	S	318	28	8,895	45	258
LNSG::WLDWD AVE::60	WILDWOOD AVENUE	192ND STREET	191ST PLACE	Asphalt	S	350	28	9,787	42	329
LNSG::WLDWD AVE::70	WILDWOOD AVENUE	191ST PLACE	191ST STREET	Asphalt	S	286	28	7,996	50	239
LNSG::WLDWD AVE::80	WILDWOOD AVENUE	191ST STREET	190TH PLACE	Asphalt	S	269	28	7,520	45	315
LNSG::WLDWD AVE::90	WILDWOOD AVENUE	190TH STREET	189TH PLACE	Asphalt	S	187	28	5,236	62	236
LNSG::WLLM ST::10	WILLIAM STREET	188TH STREET	SHERMAN STREET	Asphalt	S	579	28	16,199	55	264
LNSG::WLLM ST::100	WILLIAM STREET	ADAMS STREET	JACKSON STREET	Asphalt	S	339	28	9,505	42	584
LNSG::WLLM ST::110	WILLIAM STREET	JACKSON STREET	178TH STREET	Asphalt	S	355	28	9,932	80	521
LNSG::WLLM ST::120	WILLIAM STREET	178TH STREET	177TH STREET	Asphalt	S	665	28	18,634	88	573
LNSG::WLLM ST::130	WILLIAM STREET	177TH STREET	176TH STREET	Asphalt	S	645	28	18,065	52	534
LNSG::WLLM ST::140	WILLIAM STREET	176TH STREET	175TH STREET	Asphalt	S	659	28	18,448	62	245
LNSG::WLLM ST::150	WILLIAM STREET	173RD PLACE	173RD STREET	Asphalt	S	332	28	9,295	55	211
LNSG::WLLM ST::160	WILLIAM STREET	173RD STREET	172ND PLACE	Asphalt	S	340	28	9,528	70	233
LNSG::WLLM ST::170	WILLIAM STREET	172ND PLACE	WILLIAM STREET	Asphalt	S	136	28	3,822	52	463
LNSG::WLLM ST::180	WILLIAM STREET	172ND PLACE	171ST STREET	Asphalt	S	591	28	16,546	26	337
LNSG::WLLM ST::190	WILLIAM STREET	171ST STREET	170TH PLACE	Asphalt	S	294	28	8,246	49	367
LNSG::WLLM ST::20	WILLIAM STREET	188TH STREET	187TH STREET	Asphalt	S	679	28	19,026	52	295
LNSG::WLLM ST::30	WILLIAM STREET	187TH STREET	186TH STREET	Asphalt	S	681	28	19,056	91	217
LNSG::WLLM ST::40	WILLIAM STREET	RIDGE ROAD	LAKE STREET	Asphalt	S	353	28	9,877	83	386
LNSG::WLLM ST::50	WILLIAM STREET	LAKE STREET	RANDOLPH STREET	Asphalt	S	351	28	9,831	78	268
LNSG::WLLM ST::60	WILLIAM STREET	RANDOLPH STREET	WASHINGTON STREET	Asphalt	S	348	28	9,755	53	317
LNSG::WLLM ST::70	WILLIAM STREET	WASHINGTON STREET	MADISON STREET	Asphalt	S	352	28	9,863	42	404
LNSG::WLLM ST::80	WILLIAM STREET	MADISON STREET	MONROE STREET	Asphalt	S	346	28	9,678	23	511
LNSG::WLLM ST::90	WILLIAM STREET	MONROE STREET	ADAMS STREET	Asphalt	S	364	28	10,202	21	535

Pavement ID	Road Name	From	To	Surface	Rank	Length (FT)	Width (FT)	Area (SF)	PCI	IRI
LNSG::WLLW LN::10	WILLOW LANE	ILLI INDI DRIVE	END	Asphalt	S	492	28	13,771	71	238
LNSG::WLLW LN::20	WILLOW LANE	TERRACE DRIVE	ILLI INDI DRIVE	Asphalt	S	1,003	28	28,073	23	282
LNSG::WLTR ST::10	WALTER STREET	191ST STREET	192ND STREET	Asphalt	S	549	28	15,385	25	359
LNSG::WLTR ST::100	WALTER STREET	177TH STREET	176TH STREET	Asphalt	S	664	28	18,585	51	268
LNSG::WLTR ST::110	WALTER STREET	176TH STREET	175TH STREET	Asphalt	S	664	28	18,586	52	276
LNSG::WLTR ST::120	WALTER STREET	BERNICE ROAD	BERNICE AVENUE	Asphalt	S	413	28	11,568	53	291
LNSG::WLTR ST::130	WALTER STREET	BERNICE ROAD	173RD STREET	Asphalt	S	663	28	18,565	54	283
LNSG::WLTR ST::140	WALTER STREET	173RD STREET	172ND STREET	Asphalt	S	660	28	18,484	39	331
LNSG::WLTR ST::150	WALTER STREET	172ND STREET	171ST STREET	Asphalt	S	661	28	18,519	38	248
LNSG::WLTR ST::20	WALTER STREET	188TH STREET	OTTO STREET	Asphalt	S	851	28	23,838	23	311
LNSG::WLTR ST::30	WALTER STREET	186TH STREET	SCHULTZ DRIVE S	Asphalt	S	479	28	13,410	25	273
LNSG::WLTR ST::40	WALTER STREET	SCHULTZ DRIVE N	ANN STREET	Asphalt	S	383	28	10,737	94	169
LNSG::WLTR ST::50	WALTER STREET	ANN STREET	ILLINOIS STREET	Asphalt	S	540	28	15,121	95	251
LNSG::WLTR ST::60	WALTER STREET	ILLINOIS STREET	RIDGE ROAD	Asphalt	S	945	28	26,448	82	210
LNSG::WLTR ST::70	WALTER STREET	ADAMS STREET	END	Asphalt	S	390	28	10,932	47	244
LNSG::WLTR ST::80	WALTER STREET	ADAMS STREET	178TH STREET	Asphalt	S	704	28	19,724	61	288
LNSG::WLTR ST::90	WALTER STREET	178TH STREET	177TH STREET	Asphalt	S	657	28	18,386	64	225
LNSG::WRD ST::10	WARD STREET	RIDGE ROAD	END	Asphalt	S	582	28	16,294	52	290
LNSG::WRGHT ST::10	WRIGHT STREET	177TH STREET	END	Asphalt	S	264	28	7,389	82	255
LNSG::WRGHT ST::20	WRIGHT STREET	177TH STREET	176TH STREET	Asphalt	S	648	28	18,137	85	154
LNSG::WRGHT ST::30	WRIGHT STREET	176TH STREET	175TH STREET	Asphalt	S	666	28	18,639	88	149
LNSG::WSHG TN ST::10	WASHINGTON STREET	HENRY STREET	END	Asphalt	S	453	28	12,690	80	313
LNSG::WSHG TN ST::20	WASHINGTON STREET	HENRY STREET	WILLIAM STREET	Asphalt	S	628	28	17,596	74	315
LNSG::WSHG TN ST::30	WASHINGTON STREET	WILLIAM STREET	WENTWORTH AVENUE	Asphalt	S	706	28	19,759	100	523
LNSG::WSHG TN ST::40	WASHINGTON STREET	WENTWORTH AVENUE	LANGE STREET	Asphalt	S	382	28	10,686	54	266
LNSG::WSHG TN ST::50	WASHINGTON STREET	LANGE STREET	MAPLE STREET	Asphalt	S	451	28	12,625	63	198
LNSG::WSHG TN ST::60	WASHINGTON STREET	MAPLE STREET	STATE LINE AVENUE	Asphalt	S	341	28	9,540	73	237
LNSG::WST LK RD::10	WEST LAKE ROAD	OAKWOOD AVENUE	WEST LAKE ROAD	Asphalt	S	359	28	10,046	71	435
LNSG::WST ST::10	WEST STREET	CENTRAL STREET	WENTWORTH AVENUE	Asphalt	S	1,070	28	29,963	89	160
LNSG::WST ST::20	WEST STREET	RIDGE ROAD	CENTRAL STREET	Asphalt	S	321	28	8,982	78	427
LNSG::WSTFRNT RD::10	WEST FRONTAGE ROAD	186TH STREET	184TH STREET	Asphalt	S	1,284	28	35,943	91	204
LNSG::WSTFRNT RD::20	WEST FRONTAGE ROAD	184TH STREET	182ND STREET	Asphalt	S	1,339	28	37,505	86	210
LNSG::WSTFRNT RD::30	WEST FRONTAGE ROAD	182ND STREET	DORCHESTER AVENUE	Asphalt	S	2,613	28	73,157	92	152
LNSG::WSTFRNT RD::40	WEST FRONTAGE ROAD	DORCHESTER AVENUE	THORNTON LANSING ROAD	Concrete	S	165	28	4,619	92	1,013
LNSG::WSTFRNT RD::50	WEST FRONTAGE ROAD	THORNTON LANSING ROAD	END	Concrete	S	772	28	21,609	92	282
LNSG::WTFRD DR::10	WATERFORD DRIVE	170TH STREET	WATERFORD DRIVE	Asphalt	S	1,657	28	46,394	76	294
LNSG::WWRTH AVE::10	WENTWORTH AVENUE	GLENWOOD LANSING ROAD	193RD PLACE	Asphalt	P	576	35	20,151	84	146
LNSG::WWRTH AVE::100	WENTWORTH AVENUE	186TH STREET	SCHULTZ DRIVE N	Asphalt	P	632	35	22,114	84	173
LNSG::WWRTH AVE::110	WENTWORTH AVENUE	SCHULTZ DRIVE N	STEWART AVENUE	Asphalt	P	310	35	10,841	85	134
LNSG::WWRTH AVE::120	WENTWORTH AVENUE	STEWART AVENUE	WEST STREET	Asphalt	P	354	35	12,375	86	160
LNSG::WWRTH AVE::130	WENTWORTH AVENUE	WEST STREET	TERRACE DRIVE	Asphalt	P	44	35	1,531	92	221
LNSG::WWRTH AVE::140	WENTWORTH AVENUE	TERRACE DRIVE	MERTLE COURT	Asphalt	P	419	35	14,668	81	184
LNSG::WWRTH AVE::150	WENTWORTH AVENUE	MERTLE COURT	CENTRAL STREET	Asphalt	P	383	35	13,394	79	177

Pavement ID	Road Name	From	To	Surface	Rank	Length (FT)	Width (FT)	Area (SF)	PCI	IRI
LNSG::WWRTH AVE::160	WENTWORTH AVENUE	CENTRAL STREET	RIDGE ROAD	Asphalt	P	211	35	7,398	73	405
LNSG::WWRTH AVE::170	WENTWORTH AVENUE	RIDGE ROAD	WENTWORTH AVENUE	Asphalt	P	49	40	1,941	53	935
LNSG::WWRTH AVE::180	WENTWORTH AVENUE	WENTWORTH AVENUE	LAKE STREET	Asphalt	P	437	40	17,466	58	310
LNSG::WWRTH AVE::190	WENTWORTH AVENUE	LAKE STREET	RANDOLPH STREET	Asphalt	P	349	40	13,972	56	319
LNSG::WWRTH AVE::20	WENTWORTH AVENUE	193RD PLACE	192ND PLACE	Asphalt	P	602	35	21,060	87	126
LNSG::WWRTH AVE::200	WENTWORTH AVENUE	RANDOLPH STREET	WASHINGTON STREET	Asphalt	P	351	40	14,048	35	454
LNSG::WWRTH AVE::210	WENTWORTH AVENUE	WASHINGTON STREET	MADISON STREET	Asphalt	P	351	40	14,025	27	687
LNSG::WWRTH AVE::220	WENTWORTH AVENUE	MADISON STREET	MONROE STREET	Asphalt	P	344	40	13,774	43	636
LNSG::WWRTH AVE::230	WENTWORTH AVENUE	MONROE STREET	ADAMS STREET	Asphalt	P	360	40	14,392	40	511
LNSG::WWRTH AVE::240	WENTWORTH AVENUE	ADAMS STREET	JACKSON STREET	Asphalt	P	342	40	13,663	43	456
LNSG::WWRTH AVE::250	WENTWORTH AVENUE	JACKSON STREET	178TH STREET	Asphalt	P	355	40	14,187	51	380
LNSG::WWRTH AVE::260	WENTWORTH AVENUE	178TH STREET	177TH PLACE	Asphalt	P	333	40	13,316	50	503
LNSG::WWRTH AVE::270	WENTWORTH AVENUE	177TH PLACE	END	Asphalt	P	221	40	8,838	47	431
LNSG::WWRTH AVE::280	WENTWORTH AVENUE	176TH STREET	MAPLE AVENUE	Asphalt	S	424	30	12,719	73	300
LNSG::WWRTH AVE::290	WENTWORTH AVENUE	MAPLE AVENUE	175TH STREET	Asphalt	S	211	30	6,329	64	276
LNSG::WWRTH AVE::30	WENTWORTH AVENUE	192ND PLACE	192ND STREET	Asphalt	P	309	35	10,828	77	169
LNSG::WWRTH AVE::40	WENTWORTH AVENUE	192ND STREET	191ST PLACE	Asphalt	P	299	35	10,459	77	163
LNSG::WWRTH AVE::50	WENTWORTH AVENUE	191ST PLACE	190TH PLACE	Asphalt	P	598	35	20,919	83	167
LNSG::WWRTH AVE::60	WENTWORTH AVENUE	190TH PLACE	S MANOR DRIVE	Asphalt	P	304	35	10,626	87	170
LNSG::WWRTH AVE::70	WENTWORTH AVENUE	S MANOR DRIVE	188TH STREET	Asphalt	P	1,307	35	45,750	80	172
LNSG::WWRTH AVE::80	WENTWORTH AVENUE	188TH STREET	187TH STREET	Asphalt	P	673	35	23,569	88	126
LNSG::WWRTH AVE::90	WENTWORTH AVENUE	187TH STREET	186TH STREET	Asphalt	P	679	35	23,758	85	178
LNSG::XCHNG AVE::10	EXCHANGE AVENUE	RIDGE ROAD	END	Asphalt	S	1,546	28	43,285	40	363
LNSG::XCHNG AVE::20	EXCHANGE AVENUE	INDIANA AVENUE	181ST STREET	Asphalt	S	332	28	9,300	42	325
LNSG::XCHNG AVE::30	EXCHANGE AVENUE	181ST STREET	180TH STREET	Asphalt	S	580	28	16,237	88	284
LNSG::XCHNG AVE::40	EXCHANGE AVENUE	180TH STREET	179TH STREET	Asphalt	S	658	28	18,421	76	245
LNSG::XCHNG AVE::50	EXCHANGE AVENUE	179TH STREET	178TH STREET	Asphalt	S	654	28	18,305	34	339
LNSG::XCHNG AVE::60	EXCHANGE AVENUE	178TH STREET	177TH STREET	Asphalt	S	652	28	18,246	52	312
LNSG::XCHNG AVE::70	EXCHANGE AVENUE	170TH STREET	TORRENCE AVENUE	Asphalt	S	3,935	28	110,190	89	204