

2020 “State of the Streets”

Final Report

Prepared for:

**Town of Cicero, Illinois &
*Chicago Metropolitan Agency for Planning***

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ACRONYMS

| Acronym | Definition |
|---------|---|
| AC | Asphalt concrete pavement |
| APC | Asphalt concrete overlay on Portland cement concrete pavement |
| ASTM | American Society for Testing and Materials |
| BR | Brick pavement |
| CIP | Capital Improvement Plan |
| CMAP | Chicago Metropolitan Agency for Planning |
| CSU | Colorado State University |
| FT | Foot |
| G&AI | Gorronzona and Associates, Inc. |
| GIS | Geographic information system |
| GR | Gravel pavement |
| IRI | International Roughness Index |
| K | Thousand |
| L&T | Longitudinal and transverse cracking |
| LCD | Last construction date |
| M | Million |
| M&R | Maintenance and rehabilitation |
| P | Primary rank pavement |
| PAVER | PAVER Pavement Management System |
| PCC | Portland cement concrete pavement |
| PCI | Pavement Condition Index |
| PMP | Pavement management program |
| PMS | Pavement management system |
| S | Secondary rank pavement section |
| SF | Square feet |
| SU | Sample unit |
| SY | Square yard |
| T | Tertiary rank pavement section |

1 EXECUTIVE SUMMARY

1.1 History

In October of 2020, the Chicago Metropolitan Agency for Planning (CMAP) retained the services of Gorrondona and Associates, Inc. (G&AI) to implement a pavement management system for the Town of Cicero that will enable the Town to manage its roadway network in a more proactive, cost-effective, and sustainable way. To accomplish this objective, G&AI: 1) assessed the condition of the Town’s roadways, 2) implemented and customized a pavement management system for the Town, and 3) developed near- and long-term pavement maintenance and rehabilitation (M&R) recommendations for the Town’s roadways.

During the fall of 2019 and the spring of 2020, G&AI’s state-of-the-art PathRunner pavement condition data collection system (shown in Figure 1) was deployed to capture continuous, high-resolution pavement cracking, rutting, and roughness data of the Town’s roads. Collected data were entered into the PAVER Pavement Management System (PAVER), and baseline pavement condition scores were determined for each roadway.

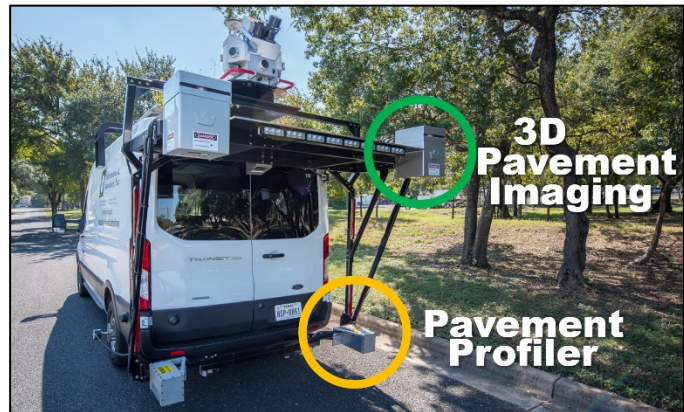


Figure 1. PathRunner pavement condition data collection system.

In July of 2020, preliminary results of the condition survey were presented to the Town. G&AI has since worked with the Town to collect additional pavement M&R records and M&R unit cost data with which to calibrate PAVER so that it is specific to the Town.

The collected pavement condition data along with both the historical M&R data and unit prices provided by the Town were used to develop network-level M&R recommendations presented herein for the Town’s consideration.

1.2 PAVER Pavement Management System

PAVER stores two primary “measures” of pavement condition. The most obvious measure of pavement condition is the **International Roughness Index (IRI)**, which describes the rideability (i.e., smoothness) of the roadway as experienced by the driver.

The second measure of pavement condition is the **Pavement Condition Index (PCI)**, which provides an indication of both the structural integrity and surface operational condition of the roadway. PAVER uses PCI values to determine the most cost-effective level of M&R likely needed. PAVER prioritizes funding for life-extending, lower-cost preventive maintenance activities (e.g., crack sealing, slurry seals, and localized patching) above more costly funding of last resort major M&R activities, such as resurfacing and reconstruction. This prioritization in the PAVER algorithm seeks a proactive and cost-effective approach to pavement management with the avoidance of – unless necessary – more costly reactive practices.

In addition to routinely collected IRI and PCI data, PAVER stores pavement inventory information, historical M&R records, and M&R unit cost data. The system uses this information to predict future

pavement conditions and identify network-level deterioration trends and M&R needs over time. It will also allow the Town to evaluate if present M&R methods are performing as expected.

1.3 Purpose and scope

The purpose of this project is to implement a comprehensive pavement management system for the Town’s roadways. The scope of this project includes all roadways managed by the Town, which total approximately 93.4 centerline miles. This pavement management system will serve as a primary tool to assist the Town in more efficiently allocating its pavement M&R funding.

To this end, G&AI:

1. Developed an inventory of the Town’s roadways in PAVER. The PAVER inventory contains pavement surface type, functional classification, M&R unit costs, and historical M&R data. *Note: Inventory development is a one-time effort that can be used by the Town if PAVER is retained, only requiring updates to address changes to the Town’s roadway network and changes in M&R unit costs.*
2. Performed a pavement condition survey of the Town’s roadways. This survey was used to determine PCI and IRI values for analysis purposes and will serve as an initial baseline of roadway conditions.
3. Used the condition survey with the developed PAVER inventory to determine the impact of different funding levels on the Town’s roadways and identify potential network-level pavement M&R needs.

1.4 Results

Pavement Condition Index (PCI) and **International Roughness Index (IRI)** values were determined for each roadway. PCI values provide an indication of both the structural integrity and surface operational condition of a pavement. PCI values range from 0 (a failed pavement) to 100 (a pavement in excellent condition). Table 1 shows the categories chosen to represent the Town’s PCI assessment criteria, which includes typical pavement distresses and levels of M&R needed within each category.

Table 1. Town’s pavement condition categories.

| Category | Typical Distresses and Typical Level of M&R Needed | PCI Range |
|---------------------|---|-----------|
| Good | Longitudinal and transverse cracking and weathering of surface Preventive maintenance: <i>Crack sealing and surface treatments</i> | 86-100 |
| Satisfactory | More extensive longitudinal and transverse cracking and weathering of surface Preventive maintenance: <i>Crack sealing and surface treatments</i> | 71-85 |
| Fair | Extensive longitudinal and transverse cracking, early stage alligator (fatigue) cracking, early stage rutting, and weathering of surface Global preventive maintenance and localized repairs: <i>Localized surface and/or full-depth patching, surface treatments, and thin overlays</i> | 56-70 |
| Poor | More extensive and severe longitudinal and transverse cracking, alligator (fatigue) cracking, rutting, and weathering of surface Major rehabilitation: <i>Localized full-depth patching, mill and overlays, and traditional overlays</i> | 41-55 |
| Very Poor | More extensive and more severe longitudinal and transverse cracking, alligator (fatigue) cracking, rutting, weathering of surface, potholes Major rehabilitation: <i>Full-depth patching, mill and overlays, traditional overlays, and reconstruction</i> | 26-40 |
| Serious | Extensive and severe failure of pavement surface Major rehabilitation: <i>Reconstruction</i> | 11-25 |
| Failed | Complete failure of pavement surface Major rehabilitation: <i>Reconstruction</i> | 0-10 |

At the time of G&AI’s inspection, the Town’s pavements were found to have an average PCI of 61, indicating that the Town’s roadways are in overall “fair” condition.

IRI values measure the roughness (vertical displacement over a fixed interval reported in inches per mile) of a roadway pavement:

- IRI values less than 200 inches/mile indicate “smooth” pavement.
- IRI values between 200 and 400 inches/mile indicate a “marginally rough” pavement.
- IRI values greater than 400 inches/mile indicate “rough” pavement.

The Town’s roadways were found to have an average IRI value of 326 inches/mile, which indicates overall “marginally rough” pavement.

Following this executive summary, Map 1 shows PCI categories for each roadway. Roadways that were planned for resurfacing or reconstruction in 2020 (i.e., after the field inspection was performed) were assigned an assumed PCI value of 100. All other PCI values shown on Map 1 reflect the conditions of the

roadways at the time of the field inspection. Map 2 shows IRI categories for each roadway at the time of inspection. IRI values reflect a physical measurement of roughness. Consequently, IRI values were not adjusted for roadways that were planned for resurfacing or reconstruction in 2020.

The causes of pavement deterioration as quantified by the PCI may be divided into three general categories:

- Vehicle load related.
- Climate/durability related.
- Other (construction defects and material issues).

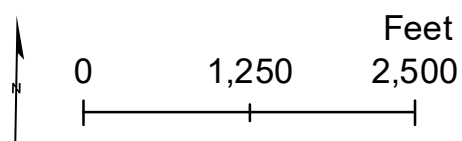
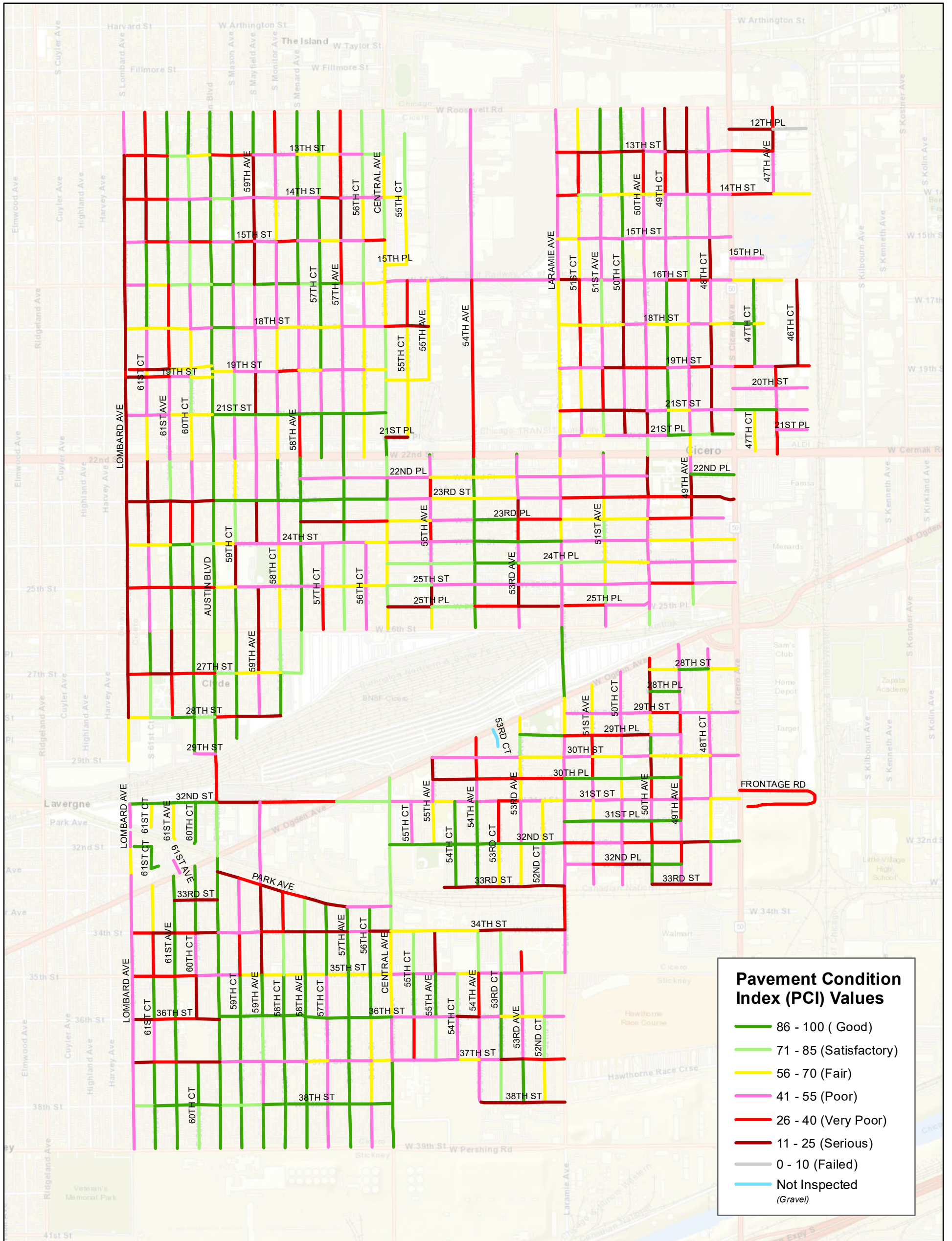
The deterioration observed on the Town’s pavements at the time of inspection was caused by a mixture of vehicle load- and climate-related distresses. Vehicle load-related distresses, including alligator cracking and rutting, were pronounced on many of the Town’s roadways and contributed most to lower PCI values. Significant climate-related distresses, including block cracking and weathering, were also observed on the Town’s roadways.

1.5 Recommendations

For the Town to get the most return on their investment from PAVER, the system must be considered a living entity. The Town should:

1. Implement pavement preservation techniques to cost-effectively extend the life of its roadways.
2. Determine when resurfacing is no longer a cost-effective option and reconstruction is needed.
3. Annually update M&R activities performed on Town roadways in the PAVER database.
4. Annually update M&R unit costs (or whenever economic conditions cause changes in unit prices).
5. Commit future funding to the routine collection of pavement condition data (all roadways should be inspected on a two- to three-year cycle).
6. Use collected pavement condition data to assess the performance of the roadways and applied M&R activities.

With such attention, PAVER will become a repository of accurate, up-to-date data and the primary tool that the Town uses for more cost-effectively programming M&R funding.



Map 1:
Pavement Condition Index
(PCI) Values

Cicero, Illinois

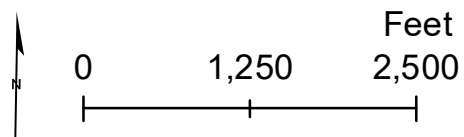
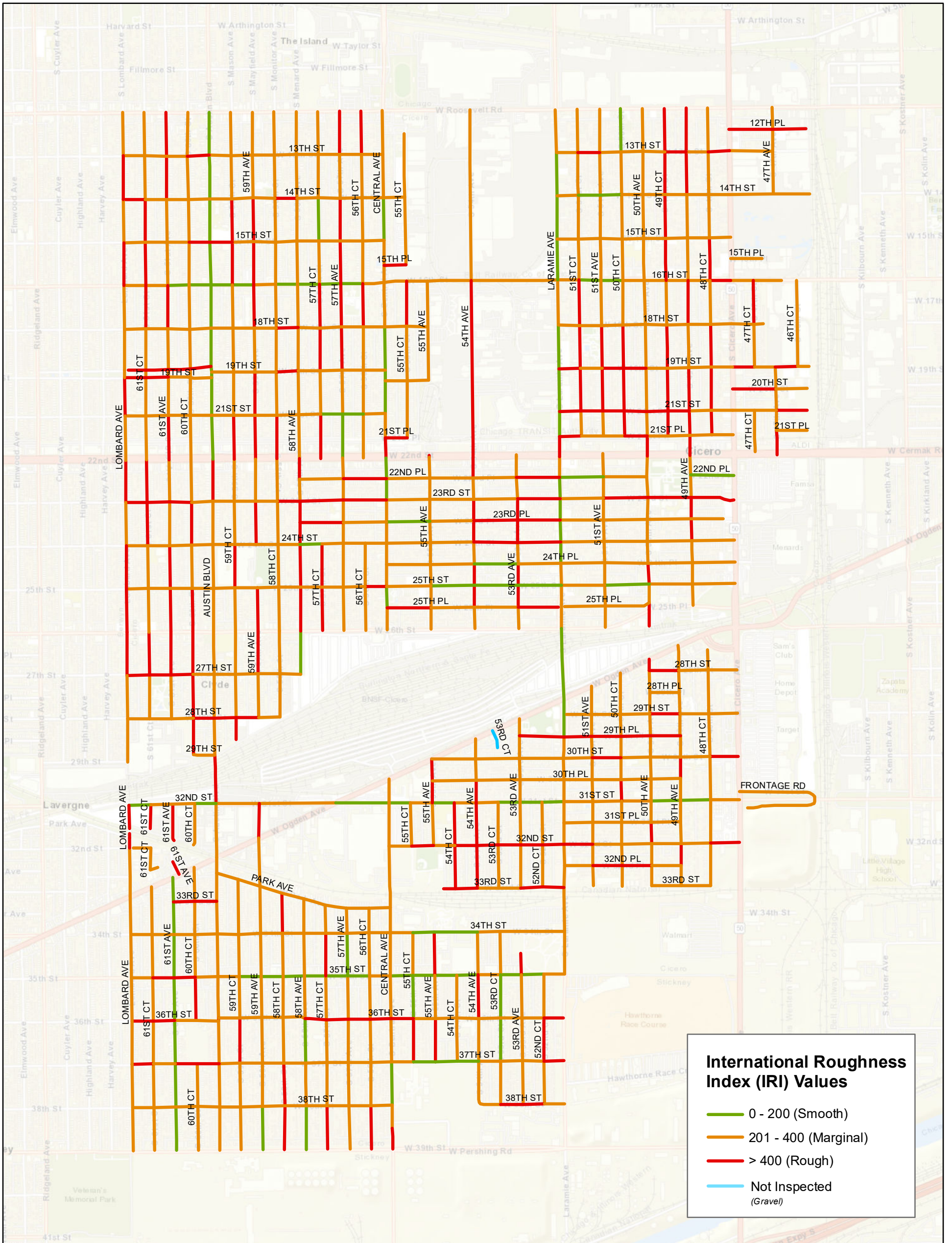
Pavement Management Program



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Map 2:
International Roughness
Index (IRI) Values

Cicero, Illinois

Pavement Management Program



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2 INTRODUCTION

2.1 Foreword

This section of the report expands on the Executive Summary and provides the reader with information pertaining to the creation and implementation of this pavement management system for the Town.

At the core of a modern pavement management system is a geocentric database that contains pavement inventory and condition information. Combined with up-to-date M&R unit cost data, calibrated deterioration models, and owner-specific M&R practices, this information is used by analysis tools in the pavement management system to predict future pavement conditions, develop multi-year M&R plans, and forecast anticipated funding needs.

This section provides a conceptual overview of pavement management and follows with the benefits and costs of implementing a pavement management system. Implementation of the Town’s pavement management system is detailed in Sections 3, 4 and 5. This section closes with an overview of effective preventive maintenance strategies that should be considered by the Town.

2.2 Background, scope, and objectives

The Chicago Metropolitan Agency for Planning (CMAP) retained the services of Gorrondona and Associates, Inc. (G&AI) to assess the existing condition of the roadways maintained by the Town. The primary objectives of this project are to implement a comprehensive and Town-wide pavement management system, perform a network-level pavement condition survey, and identify future pavement M&R needs.

The project will provide the Town with a better understanding of the current condition of its roadways and network-level recommendations for future M&R based on the results of the pavement condition survey. Moving forward, the pavement management system will continue to serve as a repository for pavement condition data, historical M&R records, and pavement condition deterioration trends.

PAVER was implemented for the Town, and a state-of-the-art PathRunner pavement condition data collection system was deployed to capture continuous, high-resolution pavement cracking, rutting, and roughness data of the Town’s roadways.

G&AI has since developed the PAVER inventory database and worked with the Town to collect additional pavement M&R records and M&R unit cost data with which to calibrate the PAVER database so that it is Town specific. These M&R records and M&R unit costs, along with the collected pavement condition data, have been used to identify present network-level M&R needs.

2.3 Project tasks

To successfully accomplish the objectives of this project, G&AI performed the following tasks, which are covered in greater detail in Sections 3, 4, and 5 of this report, respectively:

1. Pavement management system implementation
G&AI developed an inventory of the Town’s roadway pavements and implemented PAVER.
2. Pavement condition survey
G&AI performed a network-level pavement condition survey on the roadway pavements using a state-of-the-art pavement imaging and profiling data collection system. The pavement condition survey was performed in the fall of 2019 and spring of 2020.
3. M&R analyses
G&AI reviewed the collected condition data and determined the impact of several funding scenarios on the Town’s roadways and identified potential pavement M&R needs using PAVER.

The 3D pavement imaging and profiling technology used to assess the condition of the Town’s roadway pavements is the most comprehensive available. This technology has evolved rapidly over the past several years, and it is now used across the United States by more than half of the state DOTs. Unlike the inherently subjective windshield pavement condition surveys of years past, high resolution cracking, rutting, and roughness condition data were captured continuously for each of the Town’s roadways surveyed.

The collected data were then analyzed using a hybrid methodology that incorporates both automated crack detection and classification along with manual quality control. This approach yields a complete set of pavement condition data that may be used for both network-level (high-level budgeting) multi-year M&R planning as well as project-level (estimating M&R quantities) analyses. The collected data were then entered into and analyzed using PAVER. Continuously developed by the US Army Corps of Engineers, PAVER is a sophisticated, non-proprietary system widely used by municipal agencies across the United States and around the world.

2.4 Conceptual overview of pavement management

The use of a pavement management system is intended to provide municipal agencies with a systematic process for cost-effectively managing their pavement network, which may include roadways, parking lots, and alleys. The American Public Works Association (APWA) defines pavement management in the following way:

Pavement management is a systematic method for routinely collecting, storing, and retrieving the kind of decision-making information needed to make maximum use of limited maintenance (and construction) dollars.

Combined with local knowledge and practical judgment, the recommendations from a pavement management system may be used to help make better pavement M&R decisions.

At the core of a pavement management system is the method for assessing pavement condition. The most widely used method for assessing pavement condition is the Pavement Condition Index (PCI), which is industry standard practice and defined in ASTM D6433. The PCI method outlines a process for more objectively assessing the condition of a pavement based on visual observations and measurements that take place during a field inspection. These observations and measurements are then distilled into a PCI

value that ranges between 0 and 100. A PCI value of 0 indicates a failed pavement, and a PCI value of 100 indicates a pavement in good condition.

PCI values help determine the level of M&R needed to cost-effectively maintain or rehabilitate the pavement. These values may also be used to prioritize roadway improvements for the purpose of developing strategic capital improvements programs. When a pavement is in good condition, preventive maintenance can be applied to extend the life of the pavement. However, once a pavement falls below critical condition, preventive maintenance may no longer be cost effective, and more significant and perhaps more costly rehabilitation strategies should be considered.

The “Critical PCI” value for a pavement is the PCI value below which cost-effective preventive maintenance is no longer a viable option, and more significant rehabilitation and sometimes reconstruction may be necessary. As shown in Figure 2, the primary objective of pavement management is to preserve pavements in good condition above the Critical PCI with less costly preventive M&R rather than allow them to deteriorate below the Critical PCI, resulting in the need for more costly major M&R (rehabilitation or reconstruction).

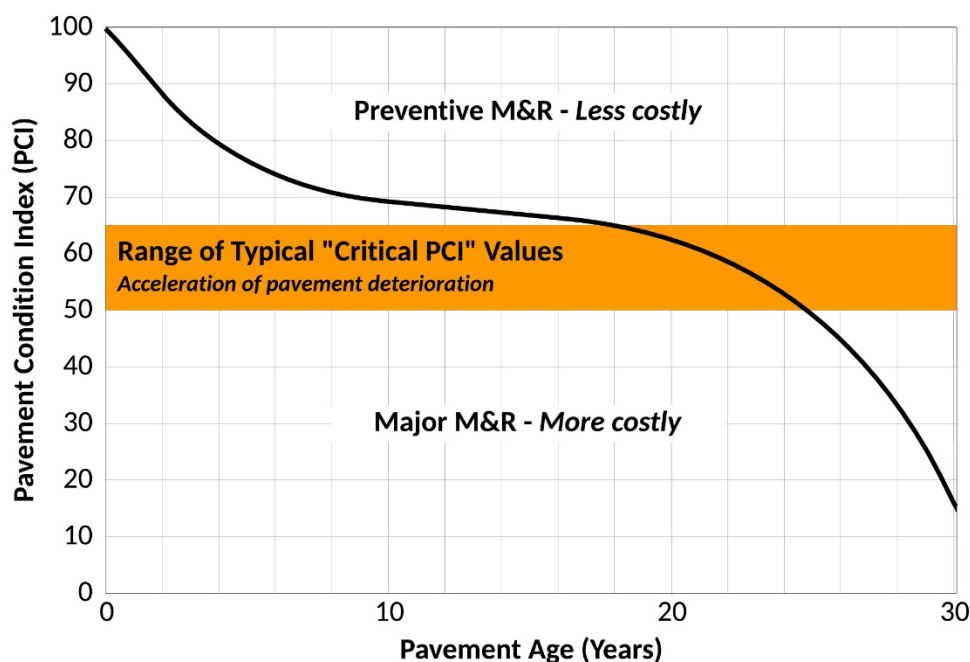


Figure 2. Example of the correct timing of preventive and major M&R relative to the Critical PCI.

The Critical PCI value is determined based on the repeated measurement of pavement condition over time as well as agency-specific M&R policies. Critical PCI values typically range between 50 and 65 (as shown in Figure 2) because the acceleration of pavement deterioration, and subsequent need for more costly M&R, typically occurs then. Setting a higher Critical PCI value simply results in pavements being recommended for major M&R earlier. Some agencies set higher Critical PCI values for their arterial roadways than for their local roadways to ensure that the roadways most heavily traveled (and often at higher speeds) are maintained to a higher standard.

PAVER’s default Critical PCI value of 55 has been used for the Town’s roadways. The Town may change this value as more condition data and historical M&R data are captured and the deterioration rates

of the Town’s roadways are better understood. Typically, two to three PCI inspections are needed to converge on acceptable Critical PCI values. The Town may choose to set Critical PCI values for each functional classification of roadway based on desired policy goals.

When the appropriate preventive maintenance treatments (e.g., crack sealing, seal coats, and patching) are undertaken at the correct times during a pavement’s service life, these relatively inexpensive preventive M&R treatments can extend the service life of the pavement, as shown in Figure 3.

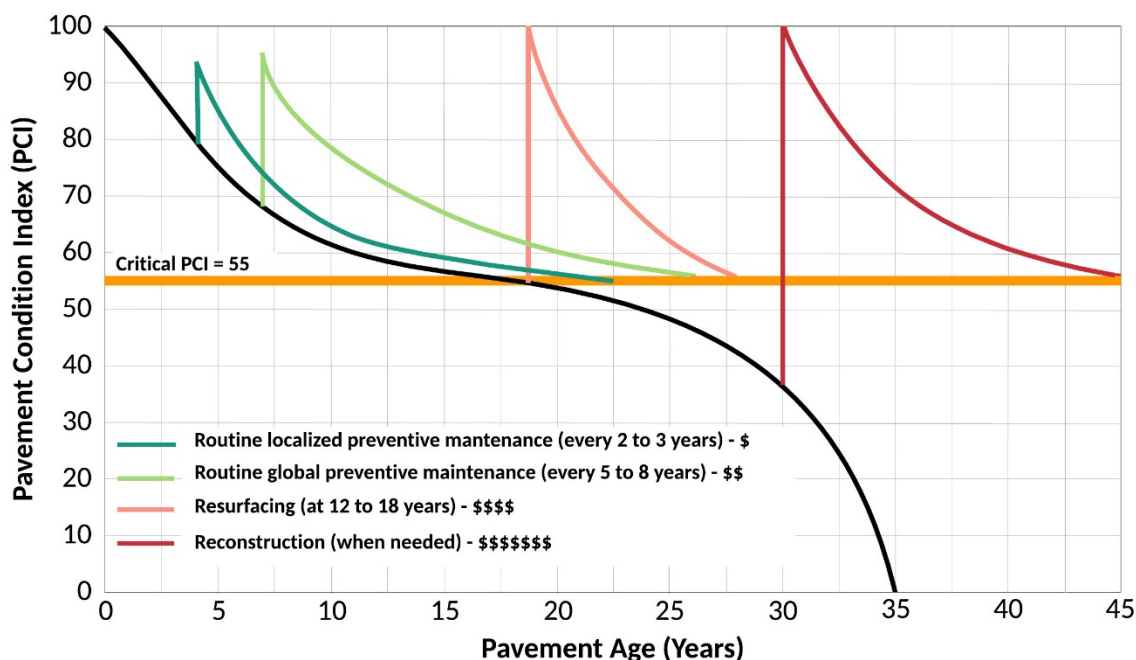


Figure 3. Example of the increasing prices and decreasing benefits of M&R.

It is important to note that the IRI, which provides a useful measure of pavement smoothness, does not correlate well to the level of M&R needed to correct smoothness issues. Consequently, IRI values are not considered when forecasting future M&R needs. Instead, IRI values are used in pavement management systems to identify pavements requiring a special inspection, or they may be used in conjunction with PCI values when prioritizing M&R projects.

As pavement management concepts have gained traction, computer-based pavement management systems have been developed to assist agencies in more optimally managing their pavements. Pavement management systems currently rely on a detailed pavement inventory, routine pavement condition assessments, pavement performance modeling, and sophisticated analysis tools that can forecast future pavement condition and estimate future M&R needs and costs.

2.5 Benefits and costs of implementing a pavement management system

Pavement management systems provide:

- A centralized location for storing pavement condition and inventory data, including construction, maintenance, and rehabilitation records.
- Decision-making support tools for:
 - ✓ Evaluating maintenance and rehabilitation alternatives.
 - ✓ Analyzing the consequences of alternative funding levels on pavement conditions.

- ✓ Improved scheduling and coordination of pavement M&R projects and other infrastructure projects.
- Analysis tools for evaluating the effectiveness of historical methods of rehabilitation.
- Reporting tools for distilling complex data and justifying funding needs to elected officials.

The benefits of implementing and maintaining a pavement management system improve over time as more data are entered into the system. The costs associated with maintaining a pavement management system include:

- Pavement inventory data collection and routine updates (typically performed annually following the end of the paving season).
- Routine pavement condition data collection (arterials and collectors are typically surveyed every other year and local roadways are surveyed on a three-year cycle).
- Evaluating pavement performance and developing M&R plans (typically performed annually following the end of the paving season – or following a condition survey – to determine candidate roadways for the next paving season).
- Software acquisition, installation, system maintenance, and updates.
- Staff training, as needed.

To ensure the success of a pavement management system, agencies should develop a plan for staffing, maintaining, and funding the system appropriately.

2.6 Incorporating pavement preservation strategies

The implementation of a pavement management system has the added benefit of assisting agencies in determining which pavements may be candidates for preventive maintenance. The use of preventive maintenance early in the life of a pavement, before any significant deterioration, has been demonstrated to be a cost-effective way to extend a pavement’s service life.

In the Federal Highway Administration (FHWA) publication, Pavement Preservation, A Road Map to the Future, preventive maintenance is defined as:

“...the planned strategy of cost-effective treatments to an existing roadway system and its appurtenances that preserves the system, retards future deterioration, and maintains or improves the functional condition of the system (without significantly increasing the structural capacity).”

The FHWA adds that preventive maintenance:

“...is typically applied to pavements in good condition having significant remaining service life. As a major component of pavement preservation, preventive maintenance is a strategy of extending the service life by applying cost-effective treatments to the surface or near-surface of structurally sound pavements.”

The following preventive maintenance treatments have been demonstrated to be effective when applied at the right time during a pavement’s service life:

- Crack sealing, crack filling, and joint sealing of flexible and rigid pavements
- Patching and edge repairs
- Chip seals, fog seals, and slurry seals
- Micro-surfacing
- Thin “functional” and “maintenance” overlay projects

Too frequently these activities are incorrectly applied as “stop-gap” or “cosmetic” treatments for pavements in poor condition rather than as true preservation activities. Preventive maintenance strategies should be applied to pavements that are in relatively good condition, and the activities should be planned and applied systematically following either the resurfacing or reconstruction of a pavement. The following FHWA website provides additional information for pavement preservation:
<https://www.fhwa.dot.gov/pavement/preservation/>.

2.7 Summary

This section provided the reader with background information pertaining to the creation and implementation of the non-proprietary PAVER system for the Town. The section provided a conceptual overview of pavement management and discussed:

1. The benefits the Town will see from the implementation of the pavement management system.
2. The costs expected to be incurred with the maintenance of the system.
3. The additional functionality beyond the obvious support the system can provide by objectively assisting the Town in optimizing the allocation of its M&R funding.

Implementation of the Town’s pavement management system is detailed in Sections 3, 4, and 5. This section closed with an overview of effective preventive maintenance strategies that should be considered by the Town moving forward.

3 PAVEMENT MANAGEMENT SYSTEM IMPLEMENTATION

3.1 Foreword

This section discusses the first task of this project: Implementing a pavement management system. One of the CMAP’s primary desires was to have a non-proprietary pavement management system for participating agencies. This section provides an overview of PAVER, a brief description of the modules available to the Town in PAVER, and insight into the PAVER database development. *(Note: The information presented in the section may be supplemented by the PAVER User Manual, which is available as a navigable PDF file in the PAVER software.)*



3.2 Objective

The objective of this task was to implement a pavement management system for the Town’s roadway pavements. G&AI implemented PAVER, which is developed and continually updated by the US Army Corps of Engineers. This task required developing an inventory of the Town’s roadway pavements and collecting current pavement condition data and entering it in PAVER.

3.3 PAVER Pavement Management System overview

PAVER assists agencies in determining when, where, and what level of pavement M&R is required and approximately how much it will cost. The system provides a suite of pavement management tools, or “modules”, that will help the Town with the following tasks:

- Developing and organizing their pavement inventory.
- Assessing the current condition of their pavements.
- Developing models to predict future pavement conditions.
- Reporting on past and future pavement performance.
- Developing scenarios for M&R based on either funding or pavement condition goals.
- Planning M&R projects.

PAVER modules include:

- Inventory
- M&R history
- Inspection
- Prediction modeling
- Condition analysis
- M&R planning
- Project planning
- Reporting

A brief description of these modules is presented in the following sub-sections.

Note: Upon request by the municipality, a one-year PAVER license shall be purchased by CMAP for the municipality from Colorado State University (CSU). The PAVER license does not expire. However, after the first year, the municipality will be responsible for purchasing software updates and technical support, if desired. Current pricing for PAVER may be found at: www.paver.colostate.edu.

3.3.1 Inventory and maintenance and rehabilitation (M&R) history modules

The PAVER **Inventory** and **M&R History** modules, shown in Figure 4 and Figure 5, are based on a hierarchical structure composed of networks (groups of roadways managed with one source of funding), branches (specific roadways), and sections. Sections are the smallest area for which conditions are reported and M&R activities recommended. Sections typically conform to existing GIS segmentation and are commonly defined from intersection to intersection by default.

One network is defined for the Town and each roadway is a branch. Pavement sections are defined within each branch following the Town’s existing GIS segmentation in the Illinois Roadway Information System (IRIS). This structure allows the Town to easily organize their inventory and historical M&R data and provides a simple and efficient way for rolling-up data to higher levels of the pavement hierarchy. The Town provided G&AI with historical M&R records, and this information was entered in PAVER.

3.3.2 Inspection module

PAVER uses the PCI as the primary measure of pavement condition. The **Inspection** module, shown in Figure 6, enables agencies to store raw pavement condition survey data and then calculate PCI values. IRI values are also stored in the **Inspection** module.

3.3.3 Prediction modeling module

The **Prediction Modeling** module in PAVER enables the user to group pavements of similar construction that are subjected to similar traffic, weather, and any other factors affecting pavement performance into “families.” Historical pavement condition

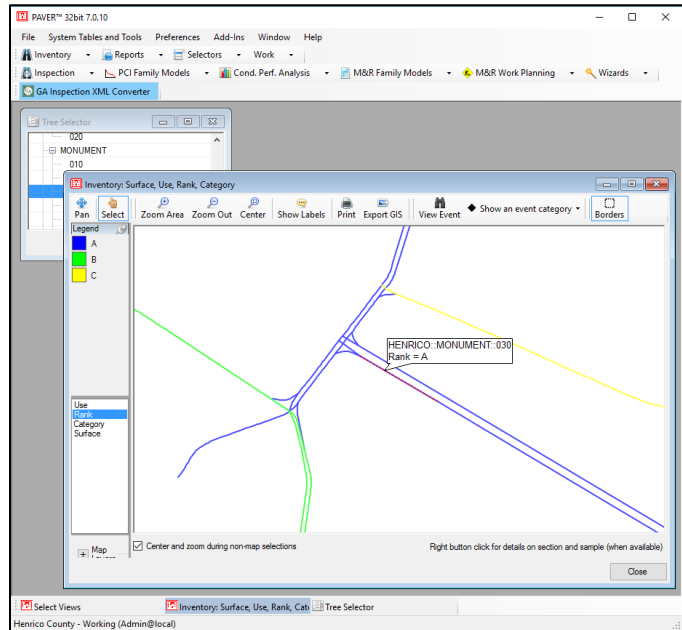


Figure 4. Example roadway functional classifications (ranks) stored in the Inventory module.

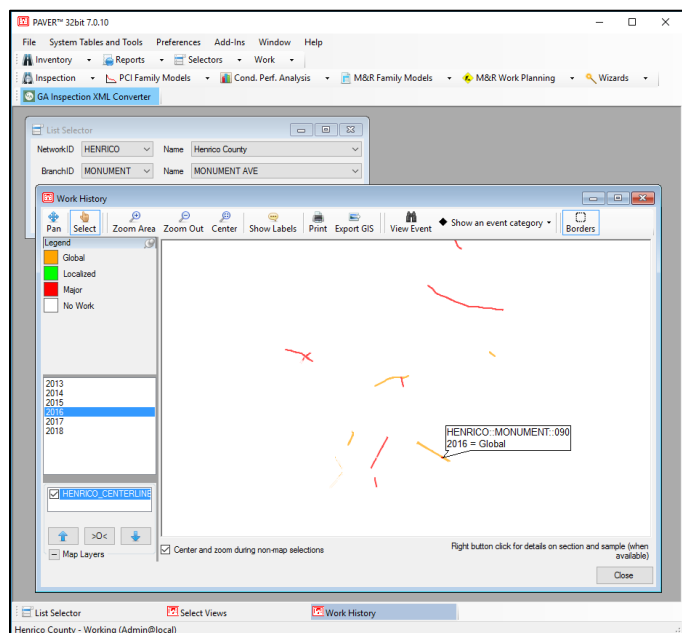


Figure 5. Example historical M&R records stored in the M&R History module.

data are used to build models that can be used to predict future pavement performance. The **Prediction Modeling** module is a hands-on module and prediction models should be updated by the Town following each condition survey. If historical pavement condition data are not available, PAVER provides default pavement prediction curves (shown in Figure 7) and allows the user to develop site specific prediction models.

3.3.4 Condition analysis module

The Condition Analysis module allows the Town to view the condition of the entire pavement network or any subset of the network over time. The module reports past conditions based on interpolated values between historical condition data, and it reports projected conditions based on the application of prediction models developed using the **Prediction Modeling** module.

3.3.5 M&R planning module

The **M&R Planning** module can determine the consequence of a predetermined funding level on pavement conditions and estimate the resulting backlog of major work. This information assists in determining funding requirements to meet specific Town pavement condition goals. These capabilities will enable the Town to develop more optimal M&R programs based on available resources and to justify M&R needs.

3.3.6 Reporting module

Each previously described module of PAVER can generate various reports that will assist the Town in analyzing, interpreting, and presenting pavement data. In addition to module-specific reports, PAVER also comes equipped with several “canned” reports, which include:

- GIS reports – *Internal/external reporting of inventory and condition data*
- Summary Charts – *Simple graphs and data tables of inventory and inspection data*
- Inspection Reports – *Summary of collected pavement condition data*
- Work History – *Summary of historical maintenance, repair, and rehabilitation data*
- Branch Listing – *Summary of overall pavement inventory data*

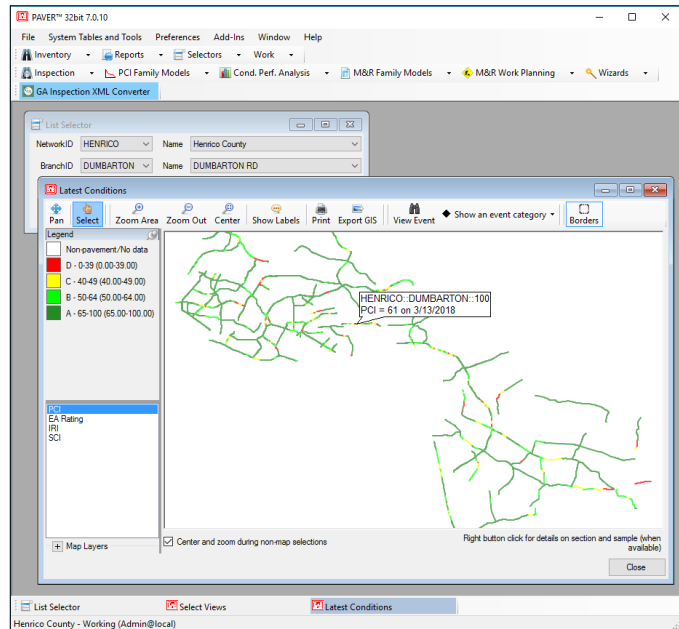


Figure 6. Example PCI values in the Inspection module.

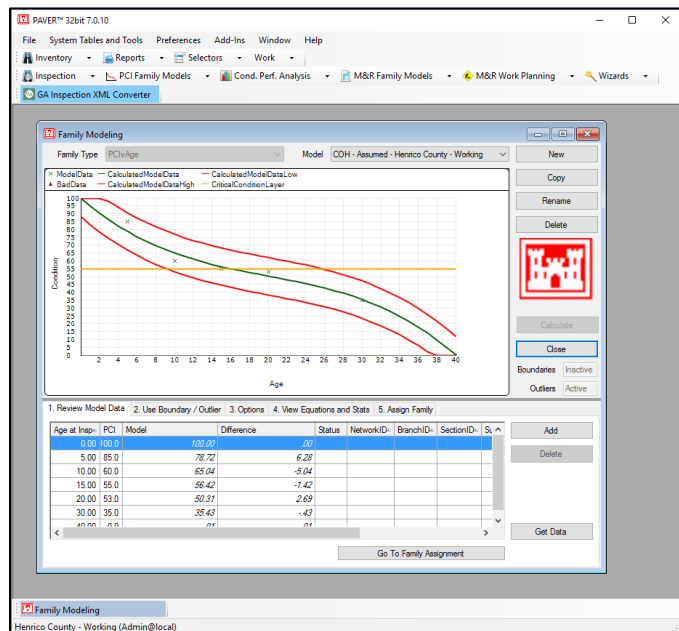


Figure 7. Example deterioration trend developed using the Prediction Modeling module.

- Branch Condition – *Summary of overall pavement condition data*
- Section Condition – *Summary of individual section data*

PAVER can generate on-the-fly “user-defined” reports, which can be tailored to meet the Town’s specific reporting needs. PAVER’s user-defined reporting capability enables the user to extract any data stored in the system and export it to a GIS shapefile, spreadsheet, or text file.

3.4 Summary

This section discussed the first task of this project: Implementing a pavement management system. This section provided an overview of the non-proprietary PAVER system, a brief description of the modules available to the Town in PAVER, and insight into the PAVER database development. The Town’s PAVER database has been developed to include specific and relevant data pertaining to the Town’s roadway pavement network. PAVER’s suite of analysis and planning tools will enable the Town to more effectively manage its roadway pavement network.

4 PAVEMENT INVENTORY

4.1 Foreword

This section describes the Town’s roadway pavement inventory as it exists in PAVER. The data sources used in developing the inventory are discussed in this section, and summary data are presented.

4.2 Objective

The objective of this task was to develop a comprehensive inventory of the Town’s roadway pavements for inclusion in PAVER. The roadway pavement inventory provides the underlying data on which analysis and reporting is performed with PAVER. In addition, the inventory provides the framework in which all routinely collected pavement condition data and historical work data are stored.

Moving forward, the Town should update the pavement inventory in PAVER to reflect the addition, realignment, widening, and/or removal of roadways managed by the Town. Typically, these types of changes are infrequent and may be done annually or prior to performing any analysis or reporting tasks with PAVER.

4.3 PAVER inventory development

The Town’s PAVER inventory was based on the IRIS GIS provided by CMAP. Relevant pavement data available in the IRIS GIS were supplemented with aerial imagery and field observations and entered in the Town’s PAVER database. These data included: number of lanes, pavement surface type, approximate roadway width, and from/to intersections for each pavement section.

Roadways were also assigned “ranks” (i.e., priorities) of primary (P), secondary (S), and tertiary (T). Federal aid eligible roads were assigned the rank of primary, since these tend to be the more heavily trafficked roadways. Residential roads were assigned the rank of secondary, and unpaved roadways and roadways in industrial zones were assigned the rank of tertiary.

A shapefile generated from the Town’s GIS was linked to the PAVER database. This enables the Town to conveniently navigate the roadways within PAVER and generate a variety of map-based inventory and condition reports in PAVER. Historical M&R records provided by the Town were entered in the PAVER database as well as unit cost data.

4.4 Inventory summary

The Town’s roadway network consists of approximately 93.4 centerline miles of predominantly asphalt surfaced, two-lane roadways. Table 2 shows the distribution of the Town’s roadway network in mileage and area by pavement rank, and Table 3 shows the distribution by pavement surface type.

Table 2. Roadway summary data by pavement rank.

| Rank | Centerline Miles | Lane Miles | Area (SY) |
|--------------|------------------|--------------|------------------|
| Primary, P | 12.6 | 28.7 | 302,941 |
| Secondary, S | 80.8 | 109.0 | 1,252,030 |
| Tertiary, T | 0.1 | 0.1 | 787 |
| Total | 93.4 | 137.8 | 1,555,758 |

Table 3. Roadway summary data by pavement surface type.

| Surface Type | Centerline Miles | Lane Miles | Area (SY) |
|---------------|------------------|--------------|------------------|
| Asphalt, AC | 92.7 | 135.9 | 1,538,282 |
| Concrete, PCC | 0.7 | 1.7 | 16,690 |
| Gravel, GR | 0.1 | 0.1 | 787 |
| Total | 93.4 | 137.8 | 1,555,758 |

Appendix A maps A-1 and A-2 present pavement rank and surface type data graphically.

5 PAVEMENT CONDITION INSPECTION

5.1 Foreword

This section discusses the second task of this project: Performing a comprehensive pavement condition survey of the Town’s roadways. The condition survey included the collection of high-resolution pavement imagery and profile measurements using a state-of-the-art PathRunner pavement condition survey system. The collected data were analyzed and PCI and IRI values were calculated for each of the Town’s roadways surveyed. This section describes the pavement condition survey system, the data collection methodology, how the collected data were analyzed, and a discussion of field observations. It concludes with several examples of pavement conditions from the Town’s roadways.

5.2 Objective

The objective of the pavement condition survey is to assess the existing structural integrity and surface operational condition of the Town’s roadways. The survey provides a comprehensive snapshot of pavement conditions at the time of data collection.

Moving forward, the Town should perform pavement condition surveys on a routine basis to objectively monitor pavement performance, determine near-term M&R needs, evaluate the effectiveness of M&R activities, develop pavement deterioration trends, and forecast near- and long-term pavement M&R needs.

5.3 Pavement condition data acquisition

G&AI deployed a state-of-the-art PathRunner pavement data collection system to capture high-resolution pavement imagery and surface data necessary to assess the condition of the Town’s roadways. The PathRunner system is shown in Figure 8.

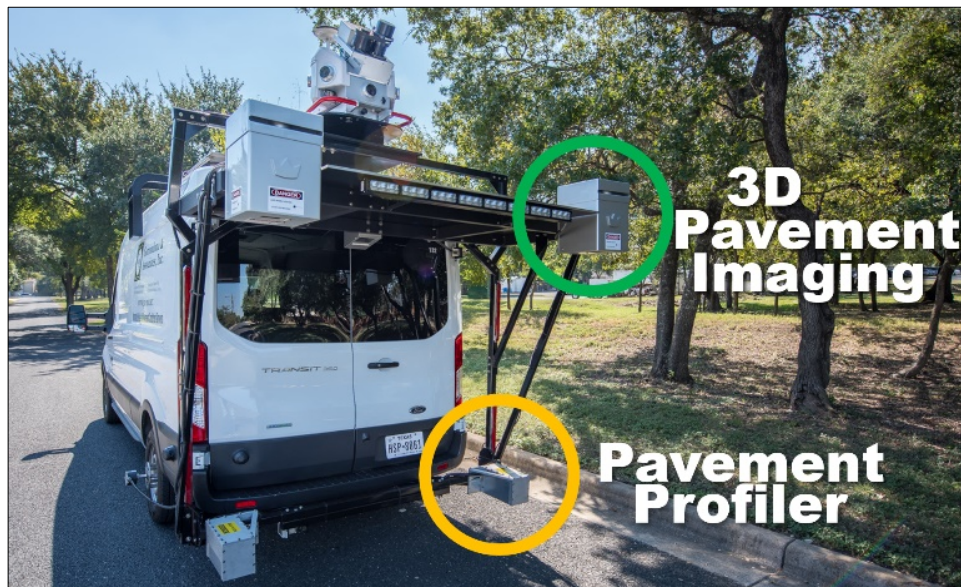


Figure 8. PathRunner pavement condition data collection system.

The PathRunner was driven on all roadways within the Town. By agreement with CMAP, only a single lane of two-lane roadways was collected and the outermost lanes in both directions of four-lane and greater roadways were collected. Based on G&AI’s experience, contiguous lanes are usually of similar

character, and this inspection approach was deemed to be cost effective for the Town while still providing sufficiently detailed information to assess existing pavement conditions. The PathRunner system continuously collected the following data for each roadway:

- High-resolution 2D and 3D pavement images for evaluating pavement distresses and determining Pavement Condition Index (PCI) values.
- Transverse profiles to measure rutting.
- Longitudinal profiles to calculate International Roughness Index (IRI) values.
- High-resolution, forward-facing, right-of-way images for manual review of all data.

These data were processed using automated tools verified by manual review to assess pavement conditions, and the results were entered in the Town’s PAVER database.

5.4 Pavement Condition Index (PCI) method

The pavement condition survey was performed following the PCI method. The PCI method is based on a set of definitions and procedures for measuring pavement distress types, severities, and quantities during a field inspection. This information is then distilled into a PCI value, which provides an indication of the structural integrity and surface operational condition (roughness) for a pavement section. The PCI method is widely used and provides a significantly more objective and repeatable method for assessing pavement condition than inherently subjective windshield surveys commonly used in the past.

The Town’s roadway network consists primarily of asphalt pavements with only a few concrete and gravel roadways. During a PCI inspection, several distress types are identified and evaluated for asphalt pavements, as shown in Table 4. The severity and quantity of each observed distress is recorded, and these data are then input into the PCI algorithm to calculate a PCI value, as shown in Figure 9.

Table 4. Asphalt and concrete pavement distress types.

| Asphalt Pavement Distresses | | Concrete Pavement Distresses | |
|--------------------------------------|--------------------|-------------------------------------|--------------------|
| Distress | Cause | Distress | Cause |
| Alligator Cracking | Load | Blowup/Buckling | Climate/Durability |
| Bleeding | Other | Corner Break | Load |
| Block Cracking | Climate/Durability | Divided Slab | Load |
| Bumps and Sags | Other | Durability ("D") Cracking | Climate/Durability |
| Corrugation | Other | Faulting | Other |
| Depression | Other | Joint Seal Damage | Climate/Durability |
| Edge Cracking | Load | Lane/Shoulder Drop-Off | Other |
| Joint Reflection Cracking | Climate/Durability | Linear Cracking | Load |
| Lane/Shoulder Drop-Off | Other | Patching, Large and Utility Cuts | Other |
| Longitudinal and Transverse Cracking | Climate/Durability | Patching, Small | Other |
| Patching and Utility Cut Patching | Other | Polished Aggregate | Other |
| Polished Aggregate | Other | Popouts | Other |
| Pothole | Load | Pumping | Other |
| Railroad Crossing | Other | Punchout | Load |
| Rutting | Load | Railroad Crossing | Other |
| Shoving | Other | Scaling, Map Cracking, and Cracking | Other |
| Slippage Cracking | Other | Shrinkage Cracks | Climate/Durability |
| Swell | Other | Spalling, Corner | Climate/Durability |
| Raveling | Climate/Durability | Spalling, Joint | Climate/Durability |
| Weathering | Climate/Durability | | |

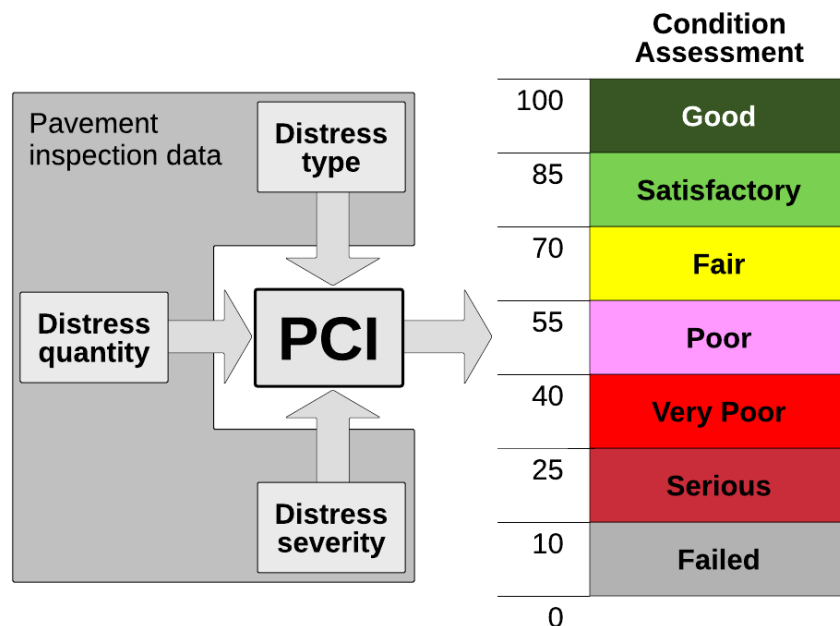


Figure 9. PCI inputs and the Town’s assessment scale.

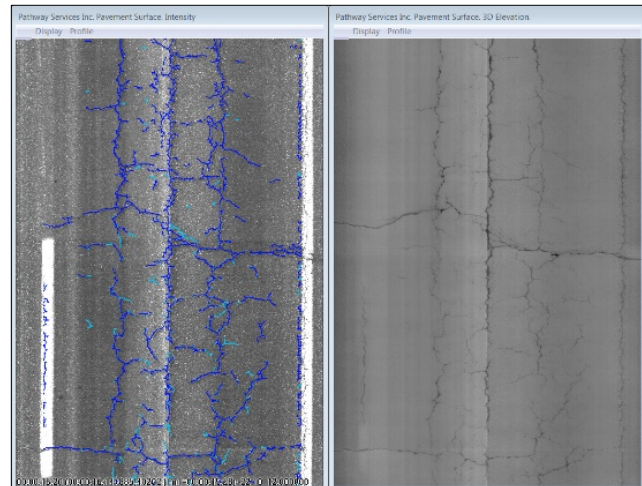
If properly designed and constructed, a new pavement begins its service life with a PCI of 100. Because of distress caused by vehicle loads, environmental factors, and aging, a pavement deteriorates over time. For each combination of distress type, severity level, and quantity observed during the inspection, points

are deducted from the initial value of 100, thereby decreasing the PCI. When multiple distresses are present, the “deduct values” are modified such that the impact of multiple distresses is not unnecessarily compounded. Due to the complexity of the PCI algorithm, PCI values are typically computed using a pavement management software package, such as PAVER. It is important to note that the PCI method does not directly measure the load carrying capacity or the rideability of a pavement. Structural testing combined with coring is needed to determine permissible pavement loadings.

5.5 Pavement Condition Index (PCI) data interpretation

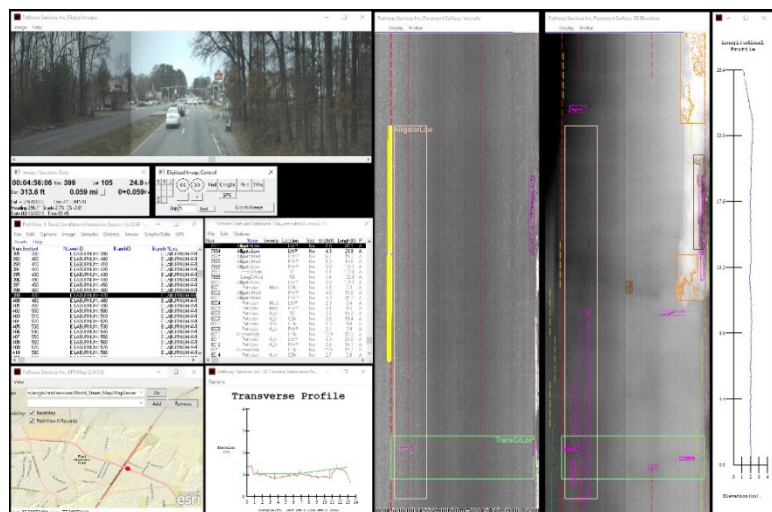
The PathRunner system captures 2D and 3D images of the roadway surface from which pavement surface distresses are evaluated. During the data collection effort, G&AI extracted pavement distress data from georeferenced digital images and rutting measurements from transverse profile measurement to determine PCI values. This process involves four distinct steps:

1. **AutoCrack Software** – This software detects cracking in the pavement imagery.
2. **AutoClass Software** – This software classifies the type of cracking detected.
3. **Manual image rating** – G&AI’s team of trained and experienced raters review the imagery and identify any distress types that the automated crack detection and classification software did not observe or incorrectly identified. Performing this manual image rating is considered the Quality Control (QC) review assuring detailed accuracy and completeness of the ratings.
4. **Quality Assurance (QA) rating** – An independent team of G&AI’s raters and project engineers perform a systematic QA review of the rated data to ensure proper evaluation of the collected imagery prior to import into PAVER.



Steps 1 and 2: Initial Automated Crack Detection and Rutting Analyses

The QC and QA ratings are the most important steps in the project. G&AI uses the PathView software for evaluating distresses using both automated algorithms and manual supplemental rating. All QC/QA is performed by highly trained and experienced engineers and technicians using PathView. The same software system has been used for more than 25 state DOTs and several municipal agency pavement condition survey projects and is a well proven review tool.



Steps 3 and 4: Manual Rating and QC/QA of Pavements using PathView

In addition to capturing 2D and 3D imagery from which pavement surface

distresses are evaluated, the PathRunner system also captures high-resolution longitudinal and transverse profiles of the roadway surface at 2mm intervals. The longitudinal profile data are analyzed to determine the IRI values, or the “roughness” of the roadway, and the transverse profiles are used to measure rutting.

5.6 Existing pavement conditions and field observations

The collected pavement survey data were used to calculate a PCI value for each pavement section in the Town. Table 5 shows the pavement condition assessment criteria used to analyze the pavement network.

Table 5. Town’s pavement condition categories.

| Category | Typical Distresses and Typical Level of M&R Needed | PCI Range |
|---------------------|---|-----------|
| Good | Longitudinal and transverse cracking and weathering of surface Preventive maintenance: <i>Crack sealing and surface treatments</i> | 86-100 |
| Satisfactory | More extensive longitudinal and transverse cracking and weathering of surface Preventive maintenance: <i>Crack sealing and surface treatments</i> | 71-85 |
| Fair | Extensive longitudinal and transverse cracking, early stage alligator (fatigue) cracking, early stage rutting, and weathering of surface Global preventive maintenance and localized repairs: <i>Localized surface and/or full-depth patching, surface treatments, and thin overlays</i> | 56-70 |
| Poor | More extensive and severe longitudinal and transverse cracking, alligator (fatigue) cracking, rutting, and weathering of surface Major rehabilitation: <i>Localized full-depth patching, mill and overlays, and traditional overlays</i> | 41-55 |
| Very Poor | More extensive and more severe longitudinal and transverse cracking, alligator (fatigue) cracking, rutting, weathering of surface, potholes Major rehabilitation: <i>Full-depth patching, mill and overlays, traditional overlays, and reconstruction</i> | 26-40 |
| Serious | Extensive and severe failure of pavement surface Major rehabilitation: <i>Reconstruction</i> | 11-25 |
| Failed | Complete failure of pavement surface Major rehabilitation: <i>Reconstruction</i> | 0-10 |

At the time of G&AI’s inspection, the Town’s pavements were found to be in overall “fair” condition and have an average PCI of 61. The condition distribution of the Town’s pavements at the time of inspection is shown in Figure 10, and detailed condition maps can be found in Appendix A.

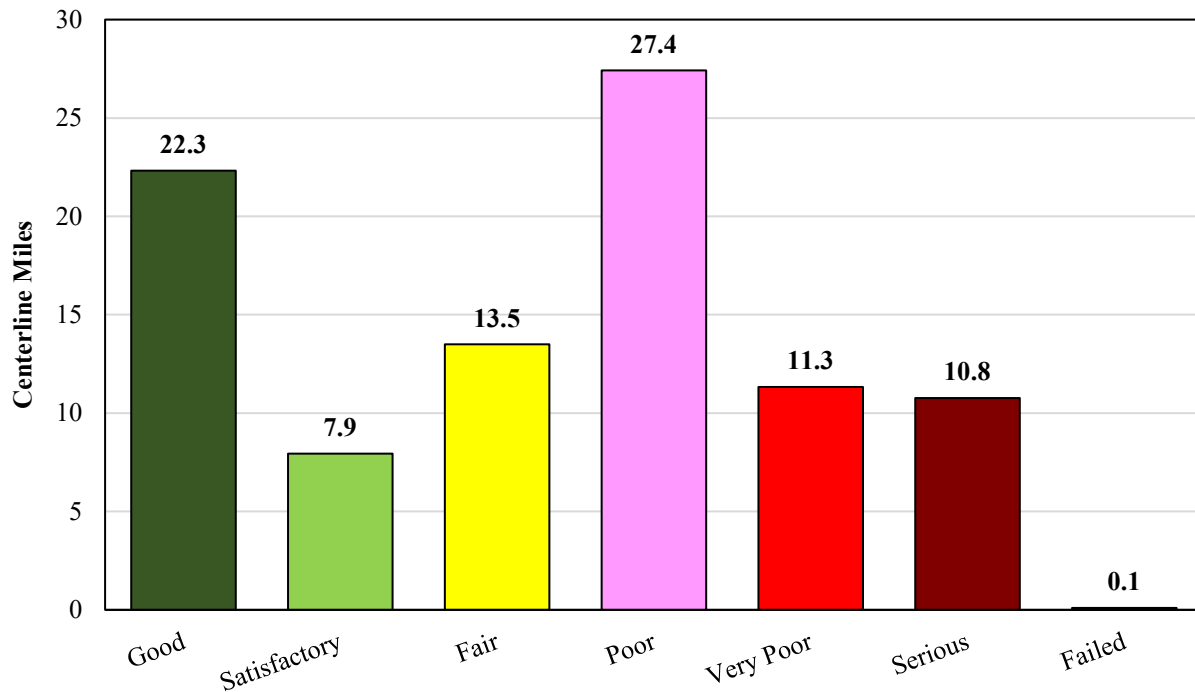


Figure 10. Town's roadway pavement condition distribution by PCI category.
 (Note: Excludes gravel roadways.)

Pavement condition data summarized by pavement ranks and surface types are presented in the following two tables, respectively.

Table 6. Roadway summary condition data by pavement rank.

| Rank | Centerline Miles | Lane Miles | Area (SY) | PCI | IRI |
|--------------|------------------|--------------|------------------|-----------|------------|
| Primary, P | 12.6 | 28.7 | 302,941 | 68 | 232 |
| Secondary, S | 80.8 | 109.0 | 1,252,030 | 59 | 349 |
| Tertiary, T | 0.1 | 0.1 | 787 | --* | --* |
| Total | 93.4 | 137.8 | 1,555,758 | 61 | 326 |

*Note: Tertiary roads were gravel and PCI values are not applicable.

Table 7. Roadway summary condition data by pavement surface type.

| Surface Type | Centerline Miles | Lane Miles | Area (SY) | PCI | IRI |
|---------------|------------------|--------------|------------------|-----------|------------|
| Asphalt, AC | 92.7 | 135.9 | 1,538,282 | 60 | 327 |
| Concrete, PCC | 0.7 | 1.7 | 16,690 | 91 | 249 |
| Gravel, GR | 0.1 | 0.1 | 787 | --* | --* |
| Total | 93.4 | 137.8 | 1,555,758 | 61 | 326 |

*Note: PCI values are not applicable to gravel roads.

The causes of pavement deterioration as quantified by the PCI may be divided into three general categories:

- Vehicle load related.
- Climate/durability related.
- Other (construction defects and material issues).

Pavement deterioration and ultimate failure is a complex process that often involves a combination of several deterioration mechanisms working together. The deterioration observed on the Town’s pavements was caused primarily by a mixture of load- and climate-related distresses. Vehicle load-related distresses, including alligator cracking and rutting, were pronounced on many of the Town’s roadways and accounted for most of the distress negatively impacting overall roadway conditions. In addition, climate-related distresses, including longitudinal and transverse cracking and block cracking, were found across the Town’s pavement inventory.

In practice, visually observed pavement distresses collected during a network-level condition survey are used to determine the likely mechanism(s) contributing to the deterioration of a roadway. However, prior to developing a specific M&R strategy, the root cause of pavement deterioration should be determined. Determining the root cause of pavement deterioration may be accomplished through an appropriate combination of traffic load analyses, drainage investigations, structural testing, coring, and material testing.

For example, vehicle load-related distresses such as alligator cracking may be addressed through load analyses and material testing. Contributing root causes may range from the roadway consistently exposed to loads in excess of its design loading to the pavement section having simply reached the end of its design life. Climate/durability-related distresses, such as transverse cracking, may result from a combination of freeze/thaw cycling and oxidation (embrittlement) of the asphalt layer. The cause(s) of “other” distresses may be determined through a combination of coring, boring, and material testing.

In addition to PCI values, IRI values were determined for each of the Town’s roadways. IRI values, reported in inches per mile, describe the amount of roughness in both wheel paths over a given length of pavement. The IRI is a standard measure of roughness used worldwide. The Town’s IRI assessment scale is shown in Table 8.

Table 8: Town’s IRI assessment criteria.

| Category | IRI Value |
|----------|-----------|
| Smooth | 0-200 |
| Marginal | 201-400 |
| Rough | >401 |

At the time of G&AI’s inspection, the Town’s pavements were found to be in overall “marginally rough” condition, with an average IRI of 326. Detailed condition maps can be found in Appendix A.

It is worth noting that IRI and PCI values do not necessarily correlate with one another. A roadway can ride well yet still be structurally deficient and in need of major M&R, and vice versa. For example, asphalt-surfaced roadways supported by structurally adequate base (e.g., crushed rock) and subgrade (e.g., existing soil) layers may exhibit extensive cracking in the asphalt surface layer due to fatigue failure of the asphalt. In situations such as these, removal of the existing asphalt layer and replacement with a thicker layer may be enough to rehabilitate the pavement. Conversely, a roadway that rides poorly may be structurally adequate and may only require minimal rehabilitation. Poor construction practices may unfortunately lead to roughness being “built into” an otherwise structurally adequate roadway at the time

of construction. Roadways exhibiting this type of roughness may require grinding and/or an additional surface course to remedy the issue.

5.7 Example pavement conditions through the Town

Figure 11 illustrates a variety of pavement conditions observed throughout the Town during the pavement condition survey. The figure includes PCI and IRI values for each pavement section along with observed distress types and recommended M&R.

| | Location + History | PCI (IRI) | Recommended M&R Activity (Typical) |
|---|--|---------------------|---|
|  | <p>24th St. (Section 30) Last resurfacing date 2015</p> | <p>91 (314)</p> | <p>Preventive maintenance <i>Seal joints between pavement and curb and gutter.</i></p> |
|  | <p>61st Ave. (Section 150) Last resurfacing date 2015</p> | <p>85 (352)</p> | <p>Preventive maintenance <i>Seal cracks as well as paving lane joint and joints between pavement and curb and gutter + surface treatment.</i></p> |
|  | <p>56th Ct. (Section 100) Last resurfacing date unknown</p> | <p>62 (282)</p> | <p>Preventive maintenance <i>Seal cracks as well as paving lane joint and joints between pavement and curb and gutter + edge patching + surface treatment.</i></p> |
|  | <p>57th Ave. (Section 160) Last resurfacing date unknown</p> | <p>46 (593)</p> | <p>Major M&R <i>Localized structural patching + cold mill and overlay <u>or</u> reconstruction</i></p> |




| | | | |
|--|---|--|---|
|  | <p>55th Ct. (Section 70) Last resurfacing date 2004</p> | <p>34 (435)</p> | <p>Major M&R Localized structural patching + cold mill and overlay <u>or</u> reconstruction</p> |
|  | <p>61st Ct. (Section 150) Last resurfacing date unknown</p> | <p>24 (546)</p> | <p>Major M&R Reconstruction</p> |
|  | <p>12th Pl. (Section 20) Last resurfacing date unknown</p> | <p>10 (705)</p> | <p>Major M&R Reconstruction</p> |

Figure 11. Pavement conditions observed during PCI inspection.

A distress observed on some of the Town’s pavements was unsealed paving lane seams (cracks), as shown in several of the photos above. If left unsealed, paving lane seams can deteriorate rapidly and significantly reduce the life of the pavement. By sealing paving lane seams immediately following paving and routinely resealing them, this type of deterioration may be minimized or prevented.

5.8 Summary

This section presented an overview of the methodology used to perform the 2019/2020 pavement condition survey and the results of the survey. A state-of-the-art PathRunner pavement condition survey system was deployed to collect pavement imagery and profile data on the Town’s roadways. The collected data were analyzed, and PCI values and IRI values were determined for each of the roadways surveyed. The Town’s roadways were found to be in overall “fair” condition with an average PCI of 61. Furthermore, the Town’s roadways were found to be in overall “marginally rough” condition, with an average IRI of 326 inches/mile.

6 MAINTENANCE AND REHABILITATION FUNDING ANALYSES

6.1 Foreword

This section discusses the third task of this project: M&R needs analyses. This section discusses the results of the analyses performed for the Town’s consideration, assumptions which shaped the analyses, and results of the analyses. The recommendations of these analyses are provided in this section and in Appendixes A through D.

6.2 Objective

The M&R Planning module in PAVER provides *raw recommendations* of when and where pavement M&R activities are needed and approximately how much they will cost. The Town should use these raw recommendations to develop programmatic M&R plans for the Town’s roadway network. These programmatic plans may be generated based on anticipated annual funding or with the goal of maintaining or achieving a desired pavement condition.

For the Town’s roadways, two preliminary M&R analyses were performed:

- A series of **five-year analyses** was performed to determine the impact of several funding levels on overall roadway conditions. The analyses included:
 - Assessing the impact of the Town’s existing funding level.
 - Determining the annual funding level needed to modestly increase the Town’s overall average roadway condition to approximately 70.
 - Determining the annual funding level needed to eliminate the Town’s major M&R backlog over a five-year period.
- A **one-year analysis** was performed to identify pavements that may benefit from preventive maintenance activities, such as crack sealing or localized patching. Only pavements with a PCI of 65 or better were considered in this analysis.

The purpose of these analyses is to determine the appropriate funding level needed to manage the Town’s roadways and provide general recommendations that will assist the Town in developing and evolving its M&R program. Additional analyses may be performed to assess either the impact of anticipated funding levels or to determine the funding levels needed to achieve a desired overall, network-average condition.

6.3 Assumptions

The M&R analyses were based on the results of the fall of 2019 and spring of 2020 Pavement Condition Index (PCI) survey and the pavement inventory and historical work records provided by the Town and stored in the Town’s PAVER database. The following assumptions were made in our analyses.

- Pavements considered candidates for preventive maintenance were determined based on their overall PCI values and the distresses observed on the pavement at the time of inspection. Pavements with PCI values of 65 or better were considered candidates for preventive maintenance.
- Recommended preventive maintenance policies for asphalt and concrete pavements are shown in Appendix D Tables D-1 and D-2, respectively. The policy tables show what type of repair activity should be applied to each distress type and severity combination. Table D-3 presents estimated unit costs for the maintenance activities recommended in tables D-1 and D-2.

- A pavement deterioration rate of roughly 2.5 points per year was used based on the performance of the Town’s resurfaced roads, which equates to a pavement life between resurfacings or reconstruction of approximately eighteen years. This deterioration rate will be refined as more historical work records are entered in PAVER and more PCI inspection data become available over time.
- A Critical PCI value (the PCI value below which a pavement is considered a candidate for major M&R) of 55 was assumed for all pavement sections. Pavements at or below the Critical PCI during the five-year analysis period triggered major M&R recommendations. *(Note: A PCI value of 55 has been initially chosen for all the Town’s roadways as this numerical value straddles the “Fair” to “Poor” condition categories in the Town’s PCI scale. Performing major M&R on pavements that are closer to a PCI of 55, rather than waiting for these pavements to deteriorate further is generally more cost effective.)*
- Unit costs used in these analyses were based on bid tabs provided by the Town and by costs reported by nearby municipalities.
 - ✓ Asphalt resurfacing ranged from approximately \$1.50 to more than \$5.00 a square foot depending roadway condition (i.e., lower PCI values may result in more patching and thicker resurfacing). Reconstruction was set at \$6.50 a square foot.
 - ✓ Concrete slab replacement costs ranged from \$5.00 to \$15.00 a square foot depending on roadway condition (i.e., lower PCI values result in more slab replacement). Reconstruction was set at \$20.00 a square foot.
- All analyses began in the fall of 2020 (November 1 start date), and an inflation rate of 3% was assumed.

6.4 Results

The results of the PAVER M&R analyses are shown in the following two figures. Figure 12 illustrates the estimated five-year change in pavement condition resulting from the analyzed funding scenarios, and Figure 13 depicts the estimated change in the Town’s major M&R backlog for each funding scenario.

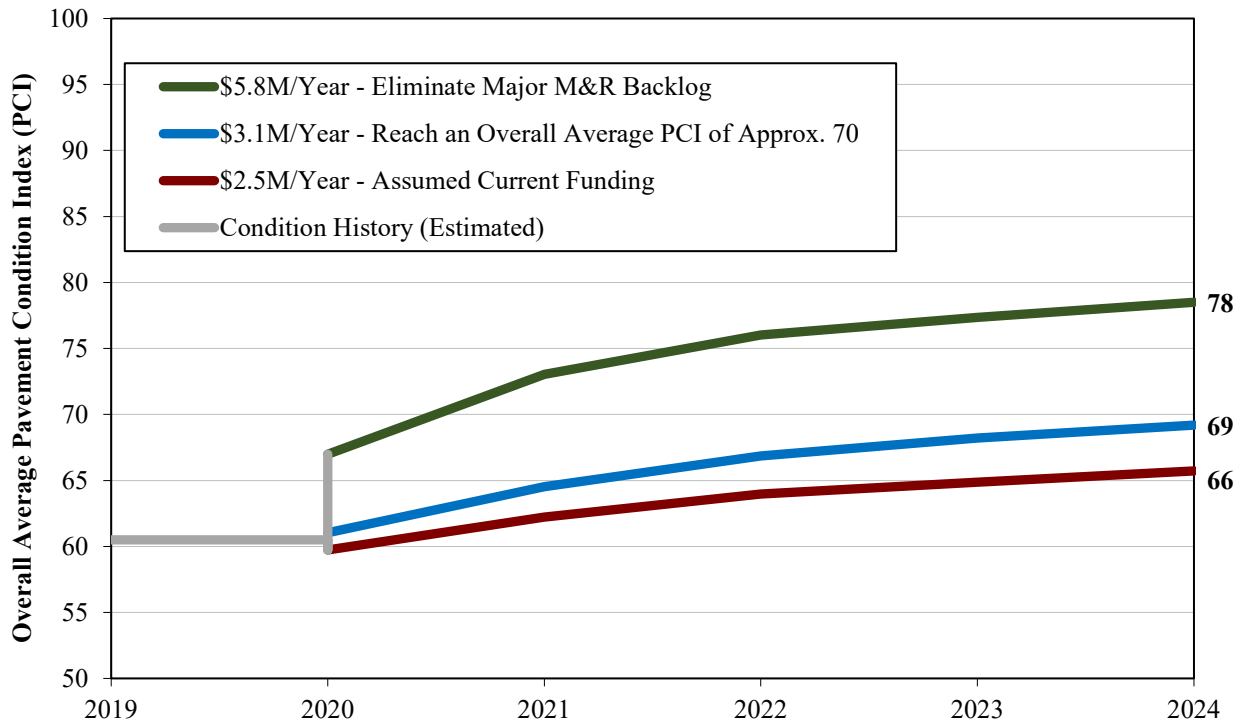


Figure 12: Impact of funding levels on overall pavement conditions by year.

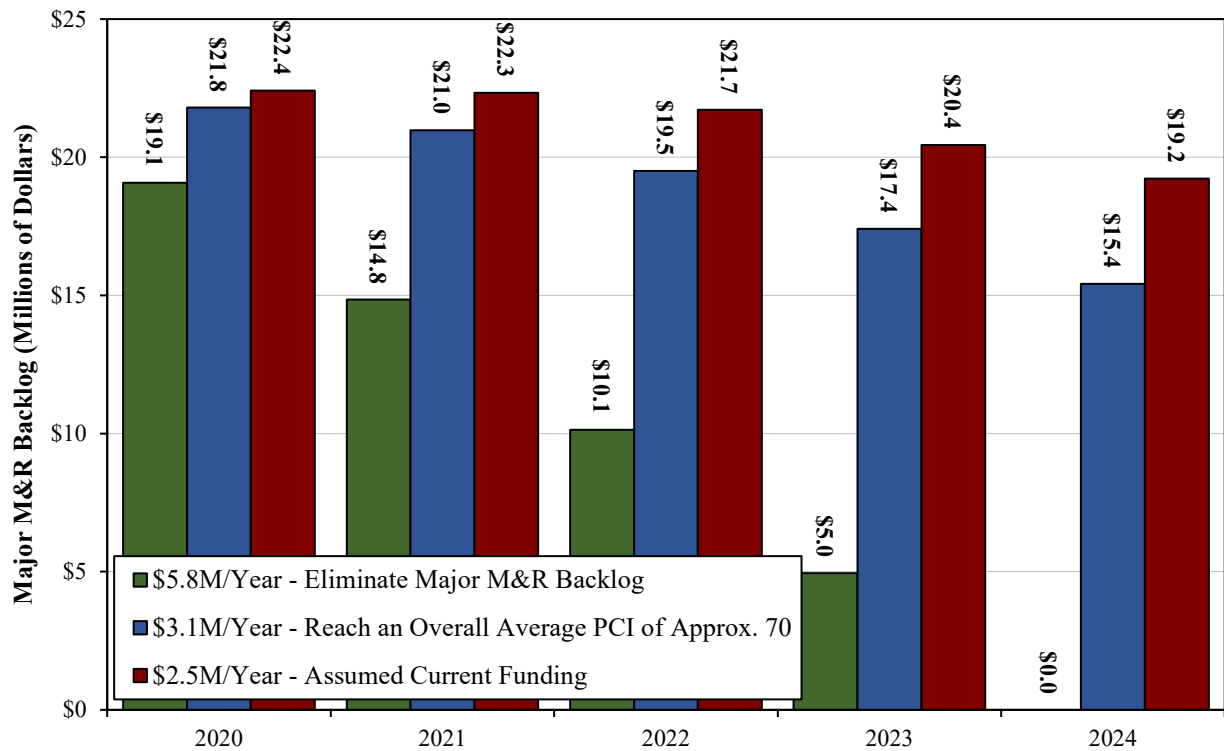


Figure 13: Impact of funding levels on major M&R backlog by year.

The consequences of the annual funding scenarios are shown in Table 9. This table illustrates the concept of “total cost.” By treating both the total annual M&R expenditures and the remaining major M&R backlog at the end of the five-year period as costs to the Town, the benefit of increasing annual funding – which results in a smaller major M&R backlog – is clearly illustrated. Consequently, eliminating the major M&R backlog over a five-year period results in the lowest total cost to the Town.

Table 9. Estimated Five-Year Pavement M&R Costs

| Funding Scenario | Total Five-Year M&R Costs (2020-2024) | Remaining M&R Backlog ¹⁾ (2024) | Total Five-Year Cost ²⁾ | Projected PCI (2024) |
|--|---------------------------------------|--|------------------------------------|----------------------|
| \$2.5M/YR (Assumed Current Funding) | \$12.5M | \$19.2M | \$31.7M | 66 |
| Increase Overall Average PCI to Approximately 70 (\$3.1M/YR) | \$15.6M | \$15.4M | \$31.0M | 69 |
| Backlog Elimination (\$5.8M/YR) | \$29.1M | \$0 | \$29.1M | 78 |

- 1) “M&R Backlog” equals the lump-sum cost to resurface/reconstruct all pavements at or below their critical PCI value.
- 2) “Total five-year cost” equals the sum of the five-year major M&R expenditures plus the remaining major M&R backlog at the end of the five-year analysis period.

Appendix A maps A-5 and A-6 present major M&R recommendations. Map A-5 shows all roadways recommended for major M&R over the upcoming five years based on the Town’s existing funding level. Map A-6 shows all roadways recommended for major M&R over the upcoming five years given an unlimited budget. The maps show which roadways are recommended each year by PAVER. These recommendations do not consider geographic proximity. Consequently, these recommendations should be grouped into practical projects during the Town’s planning process.

Map A-7 shows all roads that are candidates for preventive maintenance, such as crack sealing and localized patching. While crack sealing can be an effective treatment for preserving roadways in good condition, its utility diminishes when applied to roadways that are already in poor condition or are exhibiting signs of structural failure.

Appendix B presents tabular data showing the estimated cost to repair each of the roads recommended for major M&R over the next five years based on the Town’s existing funding level. Appendix C presents similar data assuming unlimited funding. *The costs presented in Appendixes B and C should be considered rough estimates only and should not be considered engineering estimates.* These costs are based on a simple relationship between predicted PCI value and typical level of major M&R. Unit costs used in developing these relationships were based on bid tabs provided by the Town and by costs reported by neighboring municipalities.

Appendix E presents tabular data showing one-year estimated costs to apply preventive maintenance to each of the candidate roadways (i.e., roadways with PCI values of 65 or better). The total one-year preventive maintenance cost is estimated to be approximately \$419,000, as shown in Table 10. *The estimated costs presented in Appendix E should be considered rough estimates based on the assumed unit costs only and should not be considered engineering estimates.*

Table 10. Preventive Maintenance Summary

| Maintenance Type | Quantity | Units | Est. Cost |
|------------------------------|----------|-------|------------------|
| Patching - AC Shallow | 2,562 | SF | \$14,092 |
| Crack Sealing - AC | 115,086 | FT | \$115,084 |
| Patching - AC Deep | 18,823 | SF | \$207,055 |
| Patching - PCC Full Depth | 1,686 | SF | \$50,566 |
| Joint Seal (Localized) | 20,048 | FT | \$30,073 |
| Patching - PCC Partial Depth | 163 | SF | \$1,141 |
| Crack Sealing - PCC | 308 | FT | \$462 |
| Total: | | | \$418,474 |

7 SUMMARY AND RECOMMENDATIONS

7.1 Summary

A pavement condition survey was performed in the fall of 2019 and spring of 2020 on the Town’s roadways. The results of the survey provide a snapshot of roadway conditions at the time of the survey. PAVER was implemented for the Town’s roadways and was populated with collected pavement condition data and available M&R history data provided by the Town.

For the Town to get the most return on investment out of PAVER, the system must be considered a living entity and be updated regularly with M&R activities as they are performed, M&R unit cost data, and routinely collected pavement condition data. With such attention, PAVER becomes a repository of accurate, up-to-date data and can aid the Town in more cost-effectively programming M&R funding and objectively analyzing the true cost-effectiveness of presently employed M&R activities.

Five-year M&R funding analyses were performed on the Town’s roadways using PAVER to: 1) evaluate the adequacy of the Town’s existing funding level, 2) estimate the funding level needed to maintain the Town’s existing roadway conditions, 3) estimate the funding level needed to modestly raise the overall condition of the Town’s roadways, and 4) estimate the funding level needed to eliminate the Town’s backlog of major M&R.

It was determined that the Town’s existing funding level for major M&R is adequate to maintain and gradually improve the condition of the Town’s roadway pavements.

Based on this initial set of PCI data collection and analysis on the Town’s roadways, G&AI respectfully offers the following broad recommendations.

7.2 Recommendations

7.2.1 Implement pavement preservation techniques

As discussed in Section 2.6, preventive maintenance activities, such as crack sealing, localized patching, and surface treatments, can cost-effectively extend the life of a pavement. The Town should incorporate these strategies into its M&R planning.

The Town does not appear to have an active crack sealing program for its roadways. Moisture penetrates unsealed cracks and compromises the base structure of the pavement. Freeze/thaw cycling exacerbates the damage. Sealing cracks on roadways that are in relatively good condition is a simple, cost-effective method for pavement preservation. Crack sealing is a preventive maintenance activity and should not be applied on roadways that require major M&R.

Furthermore, the Town should focus on applying routine preventive maintenance to newly resurfaced or reconstructed roadways. It was observed that some paving lane seams throughout the Town had not been sealed. Like crack sealing, sealing the paving lane seams is a simple method for pavement preservation, and it may be included in construction specifications.

7.2.2 Determine when pavements should be reconstructed rather than resurfaced

As the Town’s asphalt-surfaced pavements age and are resurfaced multiple times, the performance of successive resurfacing projects will diminish. These “diminishing returns” occur because the sublayers of the pavement (the pavement structure below the asphalt surface) continue to deteriorate due to moisture

infiltration, freeze-thaw damage, and damage due to vehicular loading. The M&R history and performance of resurfaced roadways should be closely tracked to determine the optimal number of resurfacing projects that may be performed prior to reconstructing the pavement.

7.2.3 Perform regular pavement condition inspections – every three years

To capitalize on the pavement condition survey and better track the condition of its pavements, the Town should continue to perform PCI surveys on a regular, three-year cycle. Doing so will enable the Town to:

1. Better track the deterioration of its pavements over time,
2. Identify pavement deterioration trends and use these trends to better predict future pavement conditions and then strategically apply M&R funding, and
3. Assess and track the effectiveness of its pavement preservation and major M&R activities.

The deterioration trends developed for this project were based on only one set of inspection data. Additional inspection data will help validate these trends and will improve forecasts, which may impact forecasted pavement conditions and recommended future M&R funding needs.

7.2.4 Routinely update PAVER

PAVER should be updated annually following the paving season to capture major M&R activities, routine maintenance activities, and pavement inventory changes (new roadways, jurisdictional changes, realignments). PAVER relies on updated inventory and work history data in order to generate meaningful recommendations.

7.2.5 Increase funding for pavement M&R

Based on the results of the pavement condition survey and forecasts of future pavement condition, the Town’s current level of funding is likely adequate to maintain the overall current condition of the Town’s roadways. Managing a pavement network at an overall average PCI between 70 and 80 is more cost effective since funding is spent on less costly preventive maintenance and preservation activities rather than more expensive major M&R. As the Town moves forward, it is recommended that additional funding be allocated for M&R to improve the overall condition of the roadways so that they may be managed more cost-effectively.

7.2.6 Prioritize existing M&R funding to maximize shared benefit

Currently, the Town’s roadway M&R funding needs exceed available funding. The Town should focus major M&R activities on its most trafficked roadways. Doing so will maximize the overall shared benefit of the funds spent.

APPENDIX A – PAVEMENT INVENTORY, CONDITION, AND RECOMMENDED M&R MAPS

Map A-1: Pavement Ranks

Map A-2: Pavement Surface Types

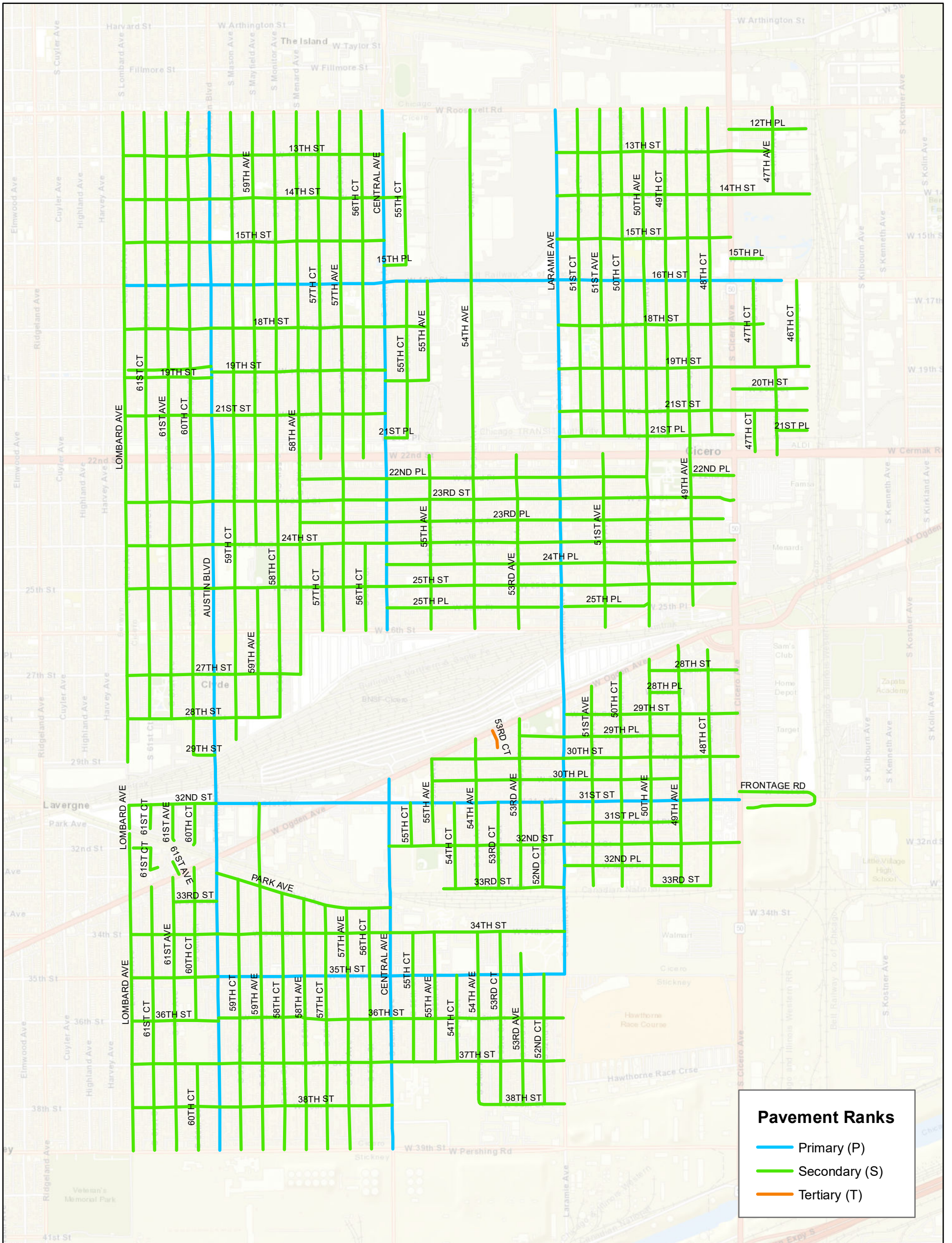
Map A-3: Pavement Condition Index (PCI) values

Map A-4: International Roughness Index (IRI) values

Map A-5: Five-year major M&R recommendations – *Recommendations assuming current funding*

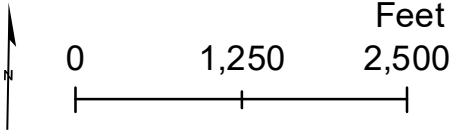
Map A-6: Five-year major M&R recommendations – *Recommendations assuming unlimited funding*

Map A-7: Pavement preservation candidates – *Current recommendations*



Pavement Ranks

- Primary (P)
- Secondary (S)
- Tertiary (T)

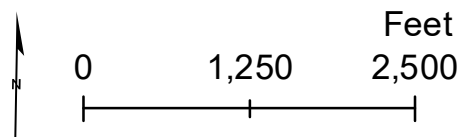
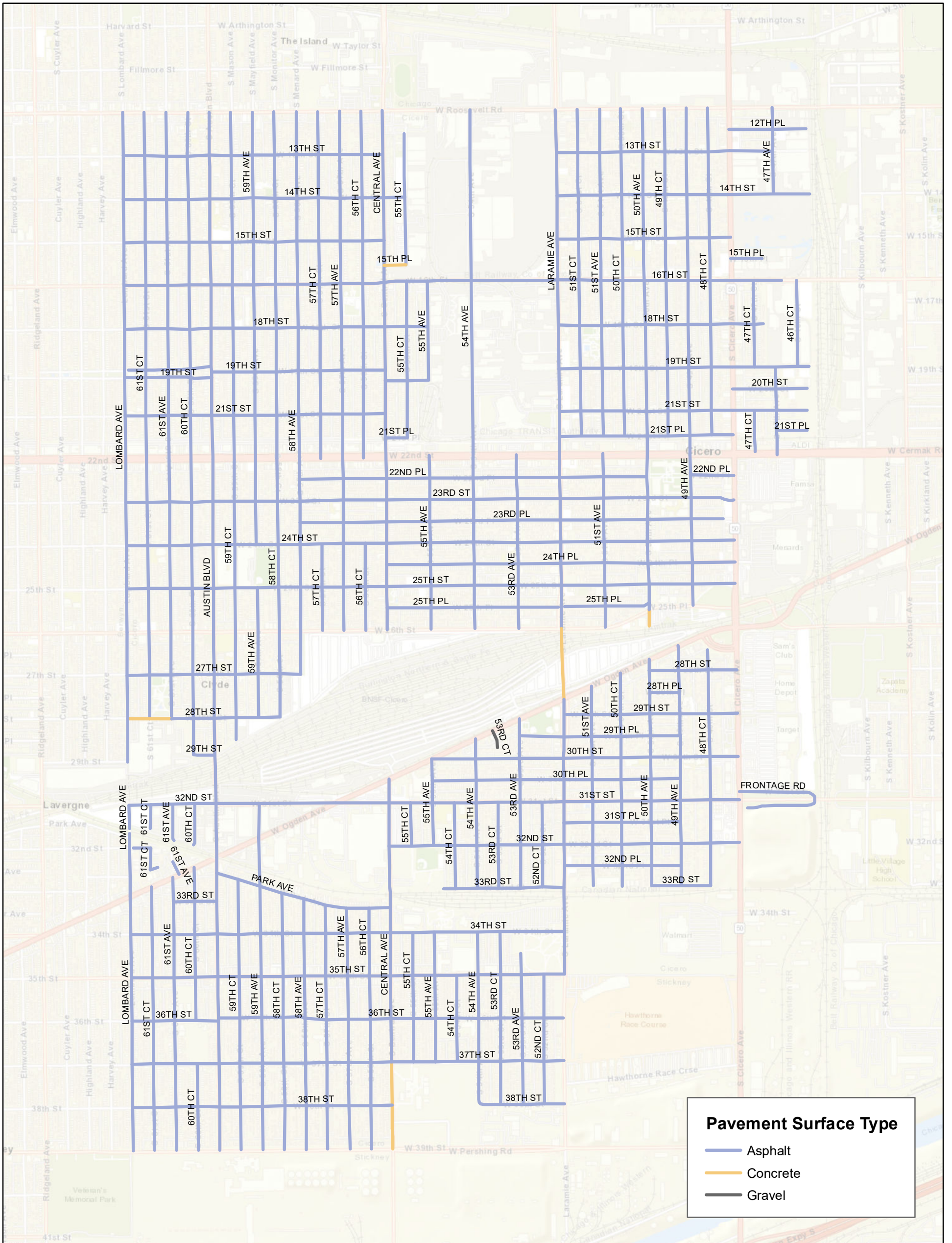


Map A-1:
Pavement Ranks

Cicero, Illinois

Pavement Management Program





Map A-2:
Pavement Surface Types

Cicero, Illinois

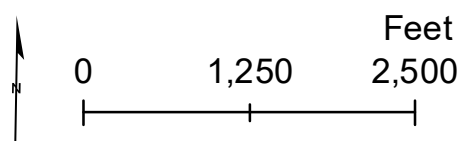
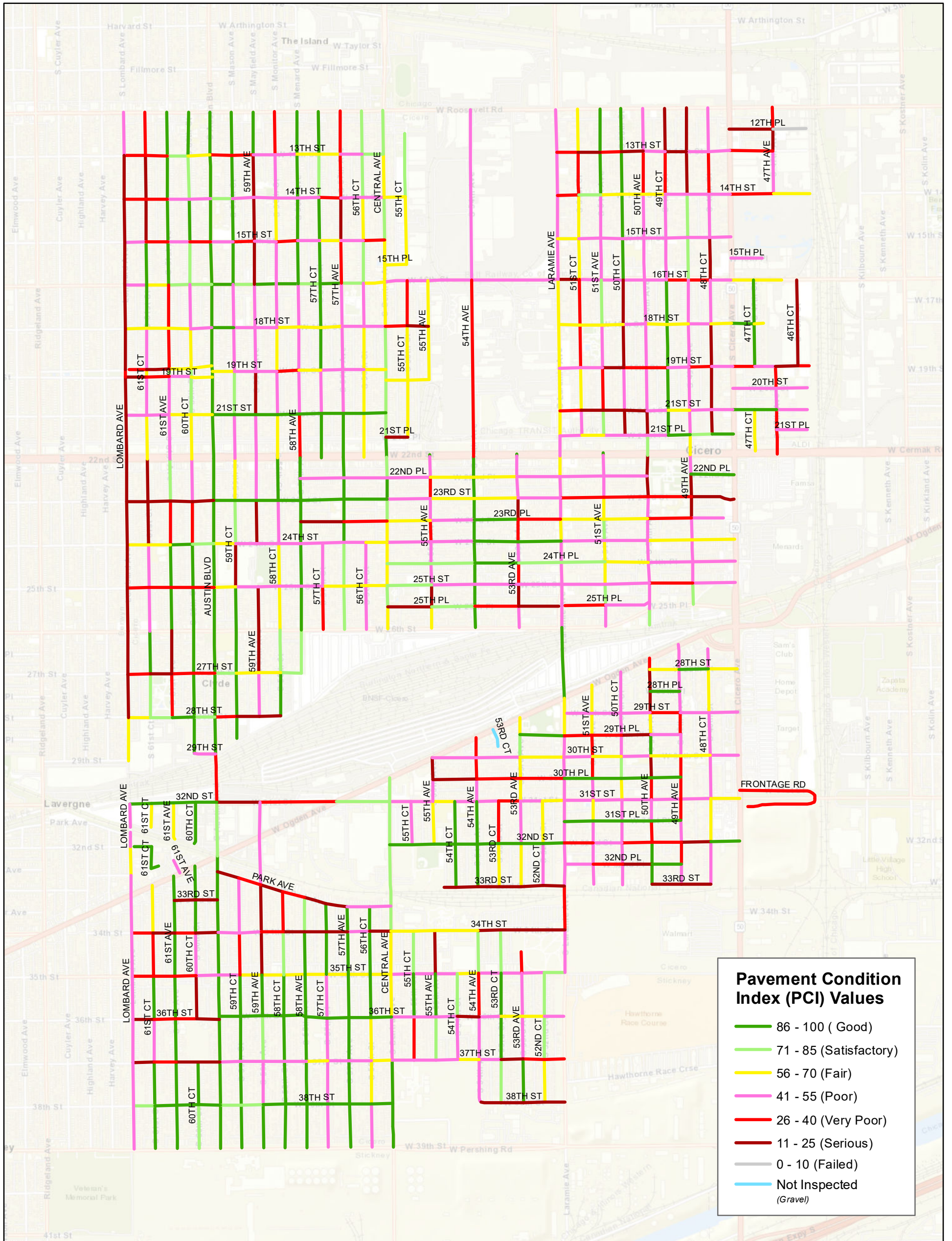
Pavement Management Program



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Map A-3:
Pavement Condition Index
(PCI) Values

Cicero, Illinois

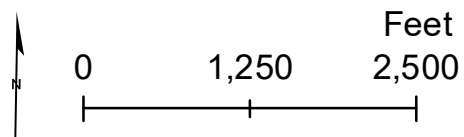
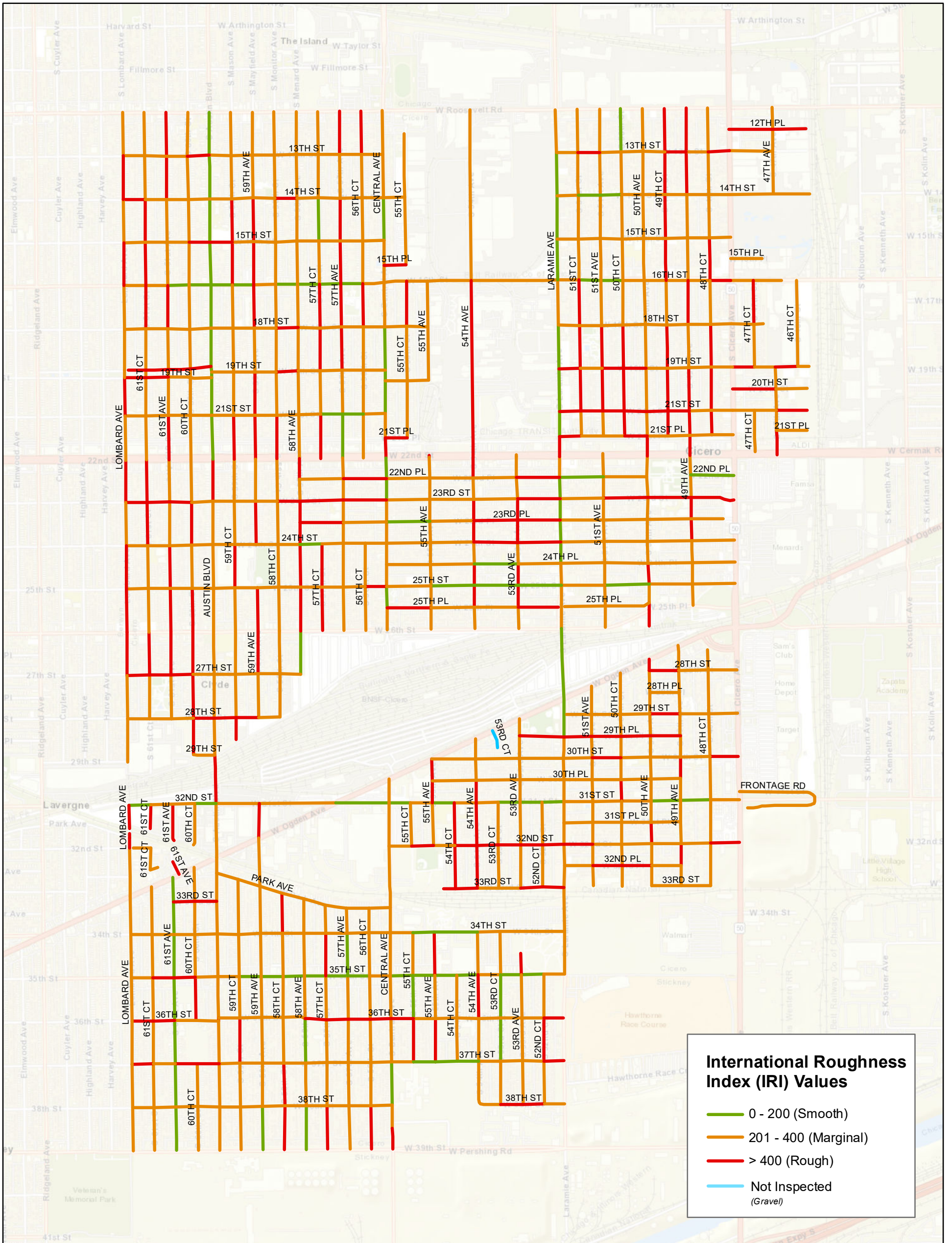
Pavement Management Program



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Map A-4:
International Roughness
Index (IRI) Values

Cicero, Illinois

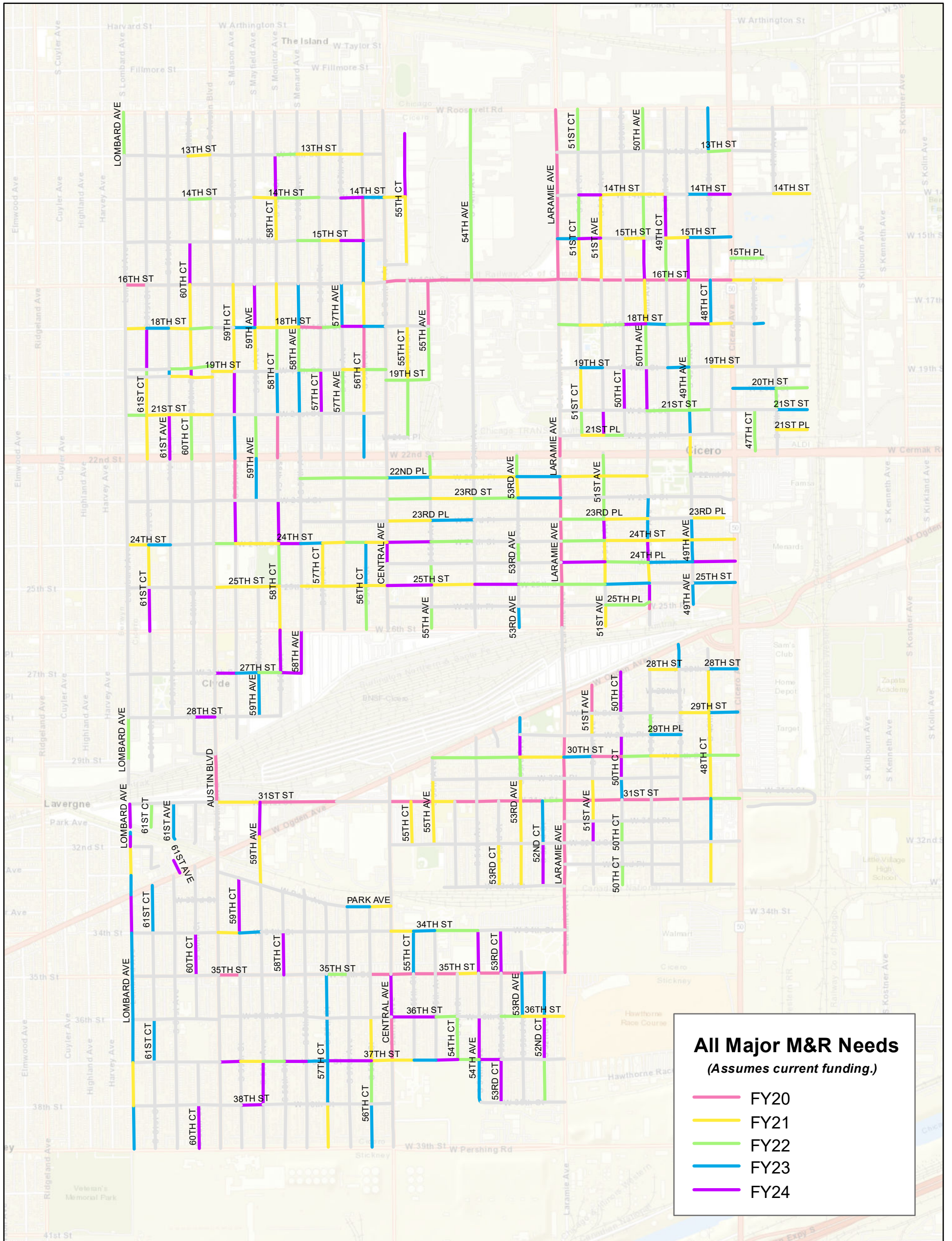
Pavement Management Program



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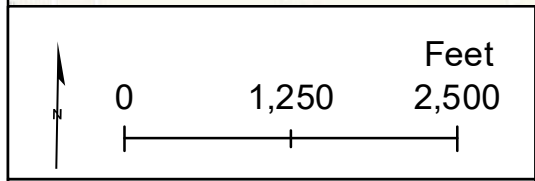


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All Major M&R Needs
(Assumes current funding.)

- FY20
- FY21
- FY22
- FY23
- FY24

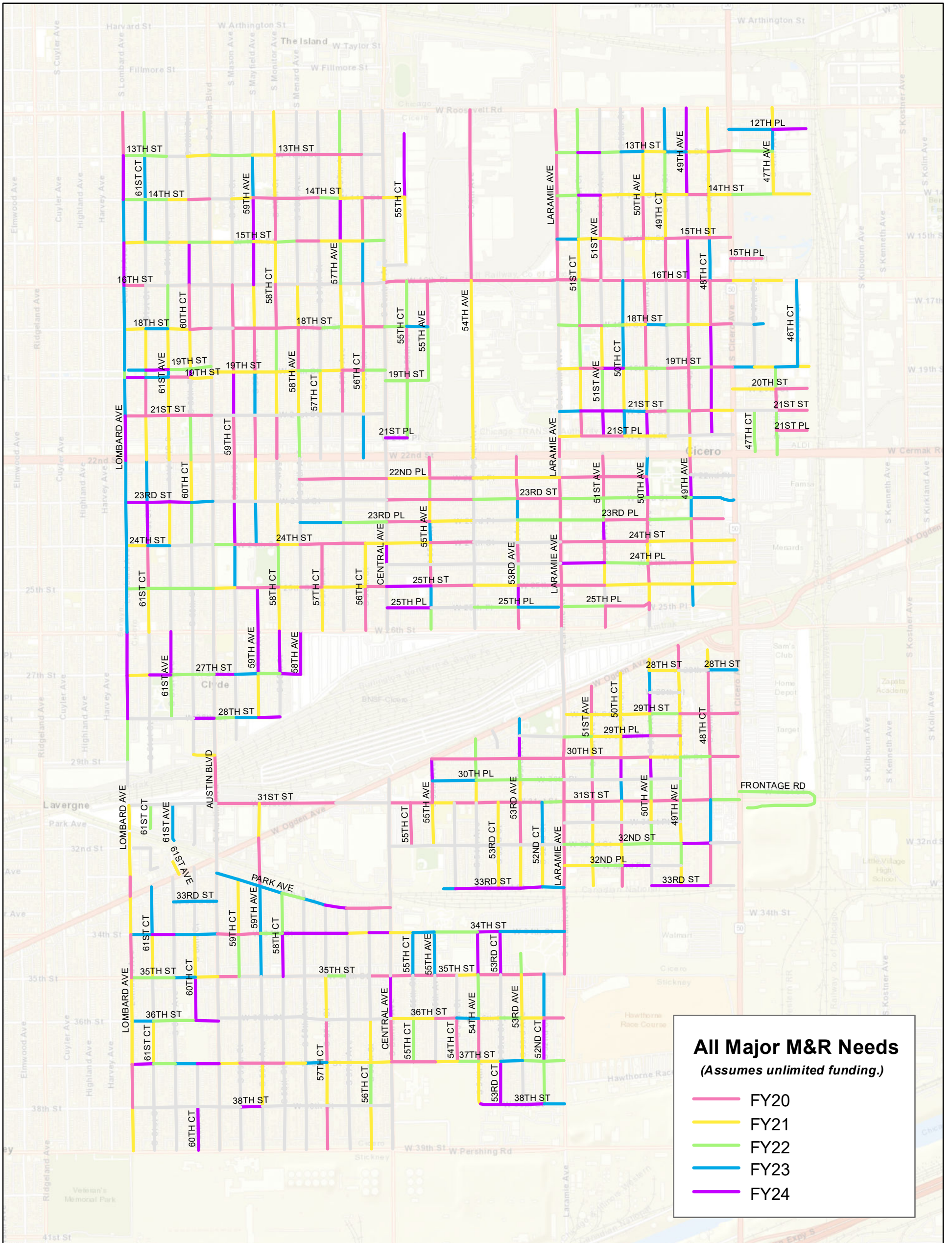


Map A-5:
All Major M&R Needs
(Assumes current funding.)

Cicero, Illinois
 Pavement Management Program

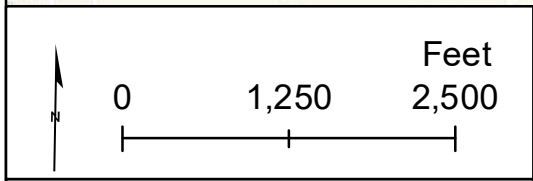
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All Major M&R Needs
(Assumes unlimited funding.)

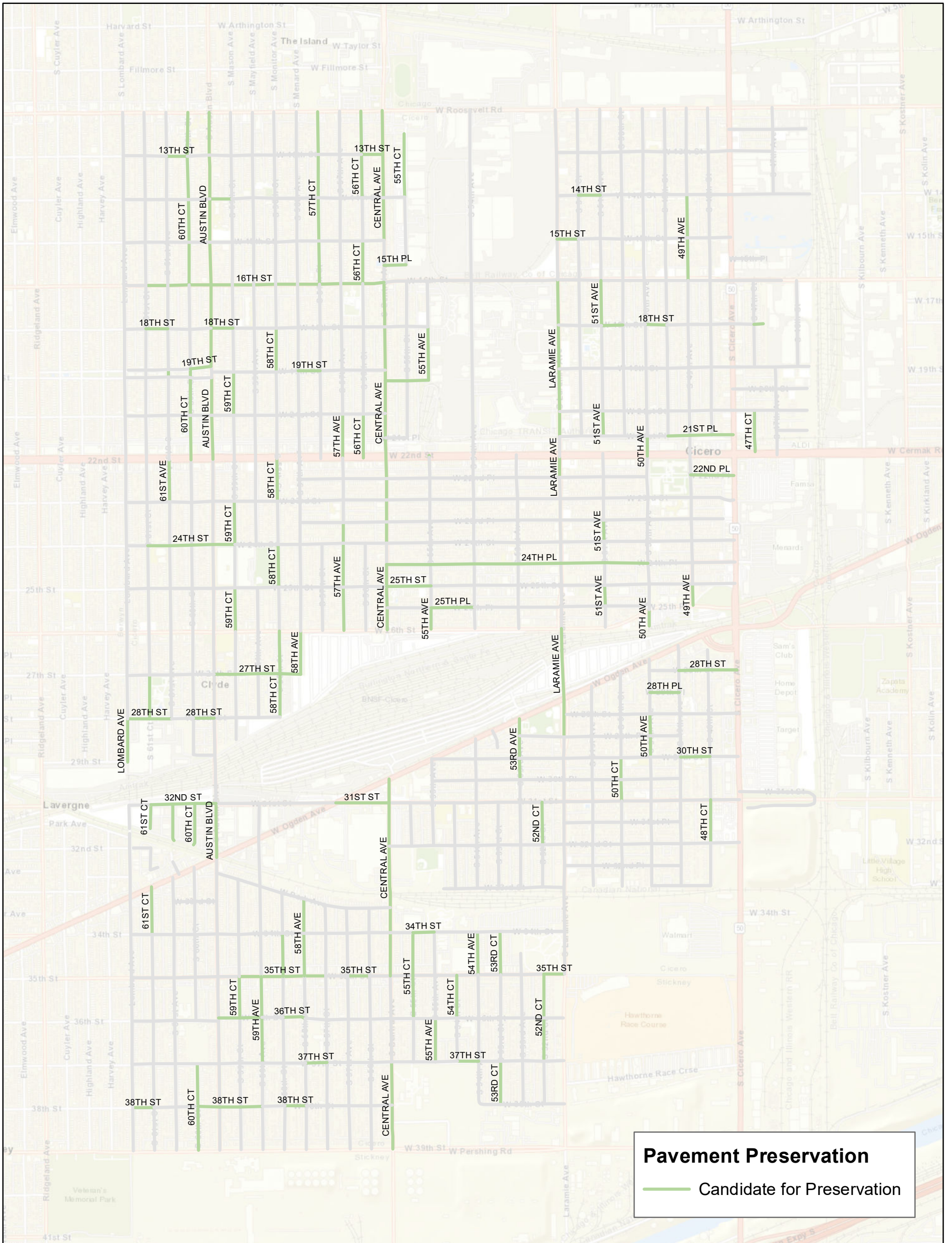
- FY20
- FY21
- FY22
- FY23
- FY24



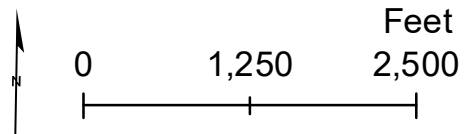
Map A-6:
All Major M&R Needs
(Assumes unlimited funding.)

Cicero, Illinois
 Pavement Management Program





Pavement Preservation
 — Candidate for Preservation



Map A-7:
 Pavement Preservation
 Candidates

Cicero, Illinois

Pavement Management Program



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**APPENDIX B – TABULATED FIVE-YEAR MAJOR M&R RECOMMENDATIONS AND
ESTIMATED COSTS – ASSUMING CURRENT FUNDING**

| Pavement ID | Road Name | From | To | Area | PCI | Year | Cost |
|----------------------|------------------|------------------|----------------|--------|-----|------|-----------|
| CCRO::16TH ST::10 | 16TH STREET | LOMBARD AVENUE | 61ST COURT | 13,415 | 51 | 2020 | \$19,433 |
| CCRO::16TH ST::130 | 16TH STREET | CENTRAL AVENUE | 55TH COURT | 13,713 | 50 | 2020 | \$21,089 |
| CCRO::16TH ST::140 | 16TH STREET | 55TH COURT | 55TH AVENUE | 13,039 | 44 | 2020 | \$30,513 |
| CCRO::16TH ST::150 | 16TH STREET | 55TH AVENUE | 54TH AVENUE | 27,060 | 51 | 2020 | \$39,200 |
| CCRO::16TH ST::160 | 16TH STREET | 54TH AVENUE | LARAMIE AVENUE | 53,153 | 44 | 2020 | \$124,383 |
| CCRO::16TH ST::170 | 16TH STREET | LARAMIE AVENUE | 51ST COURT | 13,172 | 24 | 2020 | \$78,293 |
| CCRO::16TH ST::180 | 16TH STREET | 51ST COURT | 51ST AVENUE | 13,473 | 46 | 2020 | \$27,569 |
| CCRO::16TH ST::190 | 16TH STREET | 51ST AVENUE | 50TH COURT | 13,316 | 48 | 2020 | \$24,012 |
| CCRO::16TH ST::200 | 16TH STREET | 50TH COURT | 50TH AVENUE | 13,174 | 48 | 2020 | \$23,757 |
| CCRO::16TH ST::210 | 16TH STREET | 50TH AVENUE | 49TH COURT | 13,457 | 41 | 2020 | \$38,937 |
| CCRO::16TH ST::220 | 16TH STREET | 49TH COURT | 49TH AVENUE | 13,604 | 36 | 2020 | \$52,484 |
| CCRO::16TH ST::230 | 16TH STREET | 49TH AVENUE | 48TH COURT | 13,599 | 33 | 2020 | \$60,623 |
| CCRO::16TH ST::240 | 16TH STREET | 48TH COURT | CICERO AVENUE | 12,881 | 52 | 2020 | \$17,741 |
| CCRO::18TH ST::90 | 18TH STREET | 58TH AVENUE | 57TH COURT | 8,802 | 55 | 2020 | \$10,180 |
| CCRO::30TH ST::50 | 30TH STREET | 51ST AVENUE | 50TH COURT | 11,646 | 55 | 2020 | \$13,470 |
| CCRO::31ST ST::100 | 31ST STREET | 53RD AVENUE | 52ND COURT | 13,607 | 45 | 2020 | \$30,719 |
| CCRO::31ST ST::120 | 31ST STREET | LARAMIE AVENUE | 51ST AVENUE | 17,772 | 51 | 2020 | \$25,744 |
| CCRO::31ST ST::130 | 31ST STREET | 51ST AVENUE | 50TH COURT | 17,735 | 45 | 2020 | \$40,038 |
| CCRO::31ST ST::140 | 31ST STREET | 50TH COURT | 50TH AVENUE | 18,265 | 46 | 2020 | \$37,374 |
| CCRO::31ST ST::150 | 31ST STREET | 50TH AVENUE | 49TH AVENUE | 18,173 | 53 | 2020 | \$23,679 |
| CCRO::31ST ST::160 | 31ST STREET | 49TH AVENUE | 48TH COURT | 18,426 | 53 | 2020 | \$24,009 |
| CCRO::31ST ST::20 | 31ST STREET | 59TH AVENUE | OGDEN AVENUE | 46,955 | 33 | 2020 | \$209,324 |
| CCRO::31ST ST::40 | 31ST STREET | CENTRAL AVENUE | 55TH COURT | 13,486 | 44 | 2020 | \$31,558 |
| CCRO::31ST ST::50 | 31ST STREET | 55TH COURT | 55TH AVENUE | 13,454 | 52 | 2020 | \$18,530 |
| CCRO::31ST ST::70 | 31ST STREET | 54TH COURT | 54TH AVENUE | 13,462 | 49 | 2020 | \$22,519 |
| CCRO::31ST ST::80 | 31ST STREET | 54TH AVENUE | 53RD COURT | 13,336 | 41 | 2020 | \$38,587 |
| CCRO::31ST ST::90 | 31ST STREET | 53RD COURT | 53RD AVENUE | 13,736 | 39 | 2020 | \$44,749 |
| CCRO::35TH ST::120 | 35TH STREET | 56TH COURT | CENTRAL AVENUE | 8,231 | 55 | 2020 | \$9,458 |
| CCRO::35TH ST::130 | 35TH STREET | CENTRAL AVENUE | 55TH COURT | 9,048 | 46 | 2020 | \$18,514 |
| CCRO::35TH ST::140 | 35TH STREET | 55TH COURT | 55TH AVENUE | 8,776 | 46 | 2020 | \$17,957 |
| CCRO::35TH ST::150 | 35TH STREET | 55TH AVENUE | 54TH COURT | 8,776 | 46 | 2020 | \$18,931 |
| CCRO::35TH ST::170 | 35TH STREET | 54TH AVENUE | 53RD COURT | 8,914 | 44 | 2020 | \$20,859 |
| CCRO::35TH ST::180 | 35TH STREET | 53RD COURT | 53RD AVENUE | 8,352 | 46 | 2020 | \$18,017 |
| CCRO::35TH ST::190 | 35TH STREET | 53RD AVENUE | 53RD COURT | 9,134 | 50 | 2020 | \$14,047 |
| CCRO::35TH ST::50 | 35TH STREET | AUSTIN BOULEVARD | 59TH COURT | 8,633 | 50 | 2020 | \$13,277 |
| CCRO::50TH AVE::130 | 50TH AVENUE | 26TH STREET | 25TH PLACE | 2,754 | 51 | 2020 | \$3,918 |
| CCRO::51ST AVE::90 | 51ST AVENUE | 29TH STREET | OGDEN AVENUE | 11,276 | 55 | 2020 | \$13,042 |
| CCRO::55TH AVE::170 | 55TH AVENUE | 18TH STREET | 16TH STREET | 18,349 | 55 | 2020 | \$21,222 |
| CCRO::56TH CT::110 | 56TH COURT | 19TH STREET | 18TH STREET | 17,414 | 55 | 2020 | \$20,141 |
| CCRO::56TH CT::140 | 56TH COURT | 15TH STREET | 14TH STREET | 17,374 | 55 | 2020 | \$20,095 |
| CCRO::58TH CT::130 | 58TH COURT | 22ND STREET | 21ST STREET | 17,410 | 55 | 2020 | \$20,136 |
| CCRO::59TH CT::130 | 59TH COURT | 23RD STREET | 22ND STREET | 16,897 | 55 | 2020 | \$19,543 |
| CCRO::ASTN BLVD::110 | AUSTIN BOULEVARD | 32ND STREET | 29TH STREET | 29,932 | 31 | 2020 | \$145,410 |
| CCRO::CNTRL AVE::40 | CENTRAL AVENUE | 37TH STREET | 36TH STREET | 31,138 | 52 | 2020 | \$42,886 |
| CCRO::LRM AVE::10 | LARAMIE AVENUE | 35TH STREET | 34TH STREET | 32,742 | 46 | 2020 | \$70,629 |
| CCRO::LRM AVE::120 | LARAMIE AVENUE | 26TH STREET | 25TH STREET | 33,254 | 49 | 2020 | \$55,628 |
| CCRO::LRM AVE::130 | LARAMIE AVENUE | 25TH STREET | 24TH PLACE | 16,744 | 53 | 2020 | \$21,817 |
| CCRO::LRM AVE::140 | LARAMIE AVENUE | 24TH PLACE | 24TH STREET | 13,335 | 47 | 2020 | \$25,707 |
| CCRO::LRM AVE::150 | LARAMIE AVENUE | 24TH STREET | 23RD PLACE | 13,454 | 54 | 2020 | \$16,511 |
| CCRO::LRM AVE::160 | LARAMIE AVENUE | 23RD PLACE | 23RD STREET | 13,068 | 44 | 2020 | \$30,580 |
| CCRO::LRM AVE::170 | LARAMIE AVENUE | 23RD STREET | 22ND PLACE | 13,301 | 43 | 2020 | \$33,440 |
| CCRO::LRM AVE::190 | LARAMIE AVENUE | 22ND STREET | 21ST PLACE | 15,345 | 52 | 2020 | \$21,134 |
| CCRO::LRM AVE::20 | LARAMIE AVENUE | 34TH STREET | 33RD STREET | 33,758 | 35 | 2020 | \$136,990 |
| CCRO::LRM AVE::240 | LARAMIE AVENUE | 16TH STREET | 15TH STREET | 28,612 | 52 | 2020 | \$39,406 |
| CCRO::LRM AVE::250 | LARAMIE AVENUE | 15TH STREET | 14TH STREET | 29,567 | 46 | 2020 | \$60,499 |
| CCRO::LRM AVE::260 | LARAMIE AVENUE | 14TH STREET | 13TH STREET | 30,146 | 45 | 2020 | \$68,058 |
| CCRO::LRM AVE::270 | LARAMIE AVENUE | 13TH STREET | ROOSEVELT ROAD | 29,785 | 53 | 2020 | \$38,809 |
| CCRO::LRM AVE::30 | LARAMIE AVENUE | 33RD STREET | 32ND PLACE | 20,177 | 45 | 2020 | \$45,551 |
| CCRO::LRM AVE::40 | LARAMIE AVENUE | 32ND PLACE | 32ND STREET | 19,569 | 44 | 2020 | \$45,793 |
| CCRO::LRM AVE::50 | LARAMIE AVENUE | 32ND STREET | 31ST PLACE | 20,203 | 44 | 2020 | \$47,277 |

| Pavement ID | Road Name | From | To | Area | PCI | Year | Cost |
|--------------------|----------------|------------------|------------------|--------|-----|------|----------|
| CCRO::LRM AVE::60 | LARAMIE AVENUE | 31ST PLACE | 31ST STREET | 19,452 | 47 | 2020 | \$37,499 |
| CCRO::LRM AVE::70 | LARAMIE AVENUE | 31ST STREET | 30TH PLACE | 20,513 | 44 | 2020 | \$48,002 |
| CCRO::LRM AVE::80 | LARAMIE AVENUE | 30TH PLACE | 30TH STREET | 19,556 | 41 | 2020 | \$56,582 |
| CCRO::LRM AVE::90 | LARAMIE AVENUE | 30TH STREET | 29TH PLACE | 23,166 | 51 | 2020 | \$33,559 |
| CCRO::13TH ST::100 | 13TH STREET | 57TH COURT | 57TH AVENUE | 8,606 | 50 | 2021 | \$13,592 |
| CCRO::13TH ST::110 | 13TH STREET | 57TH AVENUE | 56TH COURT | 8,777 | 49 | 2021 | \$14,878 |
| CCRO::13TH ST::40 | 13TH STREET | 60TH COURT | AUSTIN BOULEVARD | 8,848 | 54 | 2021 | \$10,602 |
| CCRO::13TH ST::90 | 13TH STREET | 58TH AVENUE | 57TH COURT | 8,664 | 50 | 2021 | \$13,684 |
| CCRO::14TH ST::130 | 14TH STREET | CENTRAL AVENUE | 55TH COURT | 8,437 | 54 | 2021 | \$10,110 |
| CCRO::14TH ST::160 | 14TH STREET | 51ST AVENUE | 50TH COURT | 8,565 | 54 | 2021 | \$10,263 |
| CCRO::14TH ST::170 | 14TH STREET | 50TH COURT | 50TH AVENUE | 8,633 | 53 | 2021 | \$11,419 |
| CCRO::14TH ST::180 | 14TH STREET | 50TH AVENUE | 49TH COURT | 8,765 | 54 | 2021 | \$11,052 |
| CCRO::14TH ST::230 | 14TH STREET | 47TH AVENUE | END | 14,723 | 52 | 2021 | \$20,374 |
| CCRO::14TH ST::70 | 14TH STREET | 59TH AVENUE | 58TH COURT | 8,797 | 48 | 2021 | \$16,883 |
| CCRO::15TH ST::100 | 15TH STREET | 57TH COURT | 57TH AVENUE | 8,664 | 54 | 2021 | \$10,925 |
| CCRO::15TH ST::160 | 15TH STREET | 50TH COURT | 50TH AVENUE | 8,599 | 48 | 2021 | \$16,502 |
| CCRO::15TH ST::180 | 15TH STREET | 49TH COURT | 49TH AVENUE | 8,808 | 48 | 2021 | \$16,904 |
| CCRO::16TH ST::250 | 16TH STREET | CICERO AVENUE | 47TH COURT | 13,620 | 53 | 2021 | \$17,710 |
| CCRO::16TH ST::260 | 16TH STREET | 47TH COURT | END | 17,488 | 53 | 2021 | \$22,740 |
| CCRO::18TH ST::10 | 18TH STREET | LOMBARD AVENUE | 61ST COURT | 8,612 | 52 | 2021 | \$11,917 |
| CCRO::18TH ST::160 | 18TH STREET | 51ST COURT | 51ST AVENUE | 8,768 | 50 | 2021 | \$13,848 |
| CCRO::18TH ST::220 | 18TH STREET | 48TH COURT | CICERO AVENUE | 8,444 | 50 | 2021 | \$13,336 |
| CCRO::18TH ST::30 | 18TH STREET | 61ST AVENUE | 60TH COURT | 8,660 | 52 | 2021 | \$11,983 |
| CCRO::18TH ST::70 | 18TH STREET | 59TH AVENUE | 58TH COURT | 8,704 | 48 | 2021 | \$16,705 |
| CCRO::18TH ST::80 | 18TH STREET | 58TH COURT | 58TH AVENUE | 8,732 | 51 | 2021 | \$12,610 |
| CCRO::19TH ST::150 | 19TH STREET | 57TH AVENUE | 56TH COURT | 8,596 | 48 | 2021 | \$16,497 |
| CCRO::19TH ST::260 | 19TH STREET | 48TH COURT | CICERO AVENUE | 8,534 | 49 | 2021 | \$14,468 |
| CCRO::19TH ST::30 | 19TH STREET | 61ST AVENUE | 60TH COURT | 5,065 | 47 | 2021 | \$10,276 |
| CCRO::19TH ST::40 | 19TH STREET | 60TH COURT | AUSTIN BOULEVARD | 4,960 | 54 | 2021 | \$5,943 |
| CCRO::19TH ST::90 | 19TH STREET | AUSTIN BOULEVARD | 59TH COURT | 8,748 | 54 | 2021 | \$11,030 |
| CCRO::21ST PL::100 | 21ST PLACE | 47TH AVENUE | END | 12,172 | 49 | 2021 | \$20,634 |
| CCRO::21ST PL::30 | 21ST PLACE | 51ST COURT | 51ST AVENUE | 8,794 | 51 | 2021 | \$13,224 |
| CCRO::21ST ST::20 | 21ST STREET | 61ST COURT | 61ST AVENUE | 8,993 | 48 | 2021 | \$17,259 |
| CCRO::21ST ST::30 | 21ST STREET | 61ST AVENUE | 60TH COURT | 8,834 | 51 | 2021 | \$13,283 |
| CCRO::21ST ST::40 | 21ST STREET | 60TH COURT | AUSTIN BOULEVARD | 8,507 | 51 | 2021 | \$12,285 |
| CCRO::22ND PL::40 | 22ND PLACE | 55TH AVENUE | 54TH AVENUE | 17,492 | 51 | 2021 | \$26,302 |
| CCRO::22ND PL::50 | 22ND PLACE | 54TH AVENUE | 53RD AVENUE | 17,256 | 54 | 2021 | \$21,757 |
| CCRO::22ND PL::70 | 22ND PLACE | LARAMIE AVENUE | 51ST AVENUE | 17,340 | 49 | 2021 | \$29,395 |
| CCRO::22ND PL::80 | 22ND PLACE | 51ST AVENUE | 50TH AVENUE | 17,460 | 48 | 2021 | \$31,566 |
| CCRO::23RD PL::100 | 23RD PLACE | 49TH AVENUE | END | 12,798 | 49 | 2021 | \$21,696 |
| CCRO::23RD PL::30 | 23RD PLACE | CENTRAL AVENUE | 55TH AVENUE | 17,394 | 51 | 2021 | \$26,155 |
| CCRO::23RD PL::80 | 23RD PLACE | 51ST AVENUE | 50TH AVENUE | 17,511 | 50 | 2021 | \$27,656 |
| CCRO::23RD ST::120 | 23RD STREET | 55TH AVENUE | 54TH AVENUE | 17,483 | 50 | 2021 | \$27,611 |
| CCRO::24TH ST::10 | 24TH STREET | LOMBARD AVENUE | 61ST COURT | 8,584 | 54 | 2021 | \$10,285 |
| CCRO::24TH ST::100 | 24TH STREET | 57TH COURT | 57TH AVENUE | 8,652 | 48 | 2021 | \$16,605 |
| CCRO::24TH ST::120 | 24TH STREET | 56TH COURT | CENTRAL AVENUE | 8,675 | 52 | 2021 | \$12,005 |
| CCRO::24TH ST::180 | 24TH STREET | 51ST AVENUE | 50TH AVENUE | 17,366 | 48 | 2021 | \$33,328 |
| CCRO::24TH ST::190 | 24TH STREET | 50TH AVENUE | 49TH AVENUE | 17,747 | 52 | 2021 | \$24,558 |
| CCRO::24TH ST::200 | 24TH STREET | 49TH AVENUE | CICERO AVENUE | 17,092 | 53 | 2021 | \$22,608 |
| CCRO::24TH ST::60 | 24TH STREET | 59TH COURT | 59TH AVENUE | 8,526 | 49 | 2021 | \$14,453 |
| CCRO::24TH ST::70 | 24TH STREET | 59TH AVENUE | 58TH COURT | 8,890 | 49 | 2021 | \$15,071 |
| CCRO::25TH ST::100 | 25TH STREET | 57TH COURT | 57TH AVENUE | 8,609 | 54 | 2021 | \$10,316 |
| CCRO::25TH ST::110 | 25TH STREET | 57TH AVENUE | 56TH COURT | 8,697 | 54 | 2021 | \$10,421 |
| CCRO::25TH ST::120 | 25TH STREET | 56TH COURT | CENTRAL AVENUE | 8,476 | 50 | 2021 | \$13,387 |
| CCRO::25TH ST::140 | 25TH STREET | 55TH AVENUE | 54TH AVENUE | 17,443 | 49 | 2021 | \$29,569 |
| CCRO::25TH ST::50 | 25TH STREET | AUSTIN BOULEVARD | 59TH COURT | 8,632 | 53 | 2021 | \$11,417 |
| CCRO::25TH ST::60 | 25TH STREET | 59TH COURT | 59TH AVENUE | 9,001 | 48 | 2021 | \$17,275 |
| CCRO::25TH ST::70 | 25TH STREET | 59TH AVENUE | 58TH COURT | 8,556 | 48 | 2021 | \$16,419 |
| CCRO::25TH ST::90 | 25TH STREET | 58TH AVENUE | 57TH COURT | 8,667 | 49 | 2021 | \$14,692 |
| CCRO::28TH ST::80 | 28TH STREET | 50TH AVENUE | 49TH AVENUE | 11,807 | 53 | 2021 | \$15,617 |

| Pavement ID | Road Name | From | To | Area | PCI | Year | Cost |
|----------------------|----------------|------------------|----------------|--------|-----|------|-----------|
| CCRO::29TH ST::60 | 29TH STREET | 49TH AVENUE | 48TH COURT | 11,638 | 48 | 2021 | \$21,041 |
| CCRO::30TH ST::30 | 30TH STREET | 53RD AVENUE | LARAMIE AVENUE | 17,563 | 51 | 2021 | \$26,409 |
| CCRO::30TH ST::70 | 30TH STREET | 50TH AVENUE | 49TH AVENUE | 11,928 | 53 | 2021 | \$15,777 |
| CCRO::31ST ST::10 | 31ST STREET | AUSTIN BOULEVARD | 59TH AVENUE | 26,634 | 22 | 2021 | \$168,491 |
| CCRO::31ST ST::60 | 31ST STREET | 55TH AVENUE | 54TH COURT | 13,474 | 53 | 2021 | \$17,521 |
| CCRO::34TH ST::130 | 34TH STREET | CENTRAL AVENUE | 55TH COURT | 8,820 | 52 | 2021 | \$12,205 |
| CCRO::34TH ST::50 | 34TH STREET | AUSTIN BOULEVARD | 59TH COURT | 8,714 | 48 | 2021 | \$16,724 |
| CCRO::35TH ST::160 | 35TH STREET | 54TH COURT | 54TH AVENUE | 8,500 | 54 | 2021 | \$10,303 |
| CCRO::36TH ST::190 | 36TH STREET | 53RD AVENUE | 52ND COURT | 8,883 | 52 | 2021 | \$12,293 |
| CCRO::36TH ST::200 | 36TH STREET | 52ND COURT | LARAMIE AVENUE | 7,725 | 49 | 2021 | \$13,095 |
| CCRO::37TH ST::120 | 37TH STREET | 56TH COURT | CENTRAL AVENUE | 8,381 | 52 | 2021 | \$11,597 |
| CCRO::37TH ST::130 | 37TH STREET | CENTRAL AVENUE | 55TH COURT | 8,886 | 48 | 2021 | \$17,054 |
| CCRO::37TH ST::60 | 37TH STREET | 59TH COURT | 59TH AVENUE | 8,977 | 49 | 2021 | \$15,218 |
| CCRO::48TH CT::10 | 48TH COURT | 33RD STREET | 32ND STREET | 17,046 | 48 | 2021 | \$32,715 |
| CCRO::48TH CT::30 | 48TH COURT | 31ST STREET | 30TH STREET | 17,306 | 48 | 2021 | \$33,213 |
| CCRO::48TH CT::40 | 48TH COURT | 30TH STREET | 29TH STREET | 17,184 | 49 | 2021 | \$29,130 |
| CCRO::48TH CT::50 | 48TH COURT | 29TH STREET | 28TH STREET | 17,427 | 50 | 2021 | \$27,524 |
| CCRO::49TH AVE::190 | 49TH AVENUE | 22ND STREET | 21ST PLACE | 8,143 | 52 | 2021 | \$11,269 |
| CCRO::50TH AVE::150 | 50TH AVENUE | 25TH STREET | 24TH PLACE | 12,926 | 48 | 2021 | \$24,808 |
| CCRO::50TH AVE::250 | 50TH AVENUE | 18TH STREET | 16TH STREET | 27,375 | 48 | 2021 | \$52,537 |
| CCRO::51ST AVE::100 | 51ST AVENUE | 26TH STREET | 25TH PLACE | 8,702 | 48 | 2021 | \$16,700 |
| CCRO::51ST AVE::120 | 51ST AVENUE | 25TH STREET | 24TH PLACE | 8,879 | 54 | 2021 | \$11,195 |
| CCRO::51ST AVE::130 | 51ST AVENUE | 24TH PLACE | 24TH STREET | 8,654 | 51 | 2021 | \$13,013 |
| CCRO::51ST AVE::230 | 51ST AVENUE | 16TH STREET | 15TH STREET | 16,620 | 49 | 2021 | \$28,174 |
| CCRO::51ST AVE::240 | 51ST AVENUE | 15TH STREET | 14TH STREET | 17,416 | 48 | 2021 | \$31,488 |
| CCRO::51ST AVE::40 | 51ST AVENUE | 31ST PLACE | 31ST STREET | 8,318 | 50 | 2021 | \$13,137 |
| CCRO::51ST AVE::50 | 51ST AVENUE | 31ST STREET | 30TH PLACE | 8,755 | 49 | 2021 | \$14,841 |
| CCRO::51ST AVE::80 | 51ST AVENUE | 29TH PLACE | 29TH STREET | 8,726 | 54 | 2021 | \$10,456 |
| CCRO::51ST CT::20 | 51ST COURT | 21ST STREET | 19TH STREET | 17,065 | 54 | 2021 | \$21,517 |
| CCRO::51ST CT::50 | 51ST COURT | 16TH STREET | 15TH STREET | 16,530 | 54 | 2021 | \$19,807 |
| CCRO::53RD AVE::170 | 53RD AVENUE | 23RD STREET | 22ND PLACE | 8,614 | 48 | 2021 | \$16,531 |
| CCRO::53RD AVE::50 | 53RD AVENUE | 33RD STREET | 32ND STREET | 17,123 | 54 | 2021 | \$20,517 |
| CCRO::53RD AVE::60 | 53RD AVENUE | 32ND STREET | 31ST STREET | 17,200 | 48 | 2021 | \$31,096 |
| CCRO::53RD AVE::70 | 53RD AVENUE | 31ST STREET | 30TH PLACE | 9,069 | 54 | 2021 | \$10,867 |
| CCRO::53RD CT::50 | 53RD COURT | 33RD STREET | 32ND STREET | 17,306 | 52 | 2021 | \$23,949 |
| CCRO::55TH AVE::40 | 55TH AVENUE | 32ND STREET | 31ST STREET | 17,143 | 53 | 2021 | \$22,675 |
| CCRO::55TH AVE::50 | 55TH AVENUE | 31ST STREET | 30TH PLACE | 8,736 | 49 | 2021 | \$14,809 |
| CCRO::55TH CT::40 | 55TH COURT | 32ND STREET | 31ST STREET | 17,447 | 48 | 2021 | \$31,542 |
| CCRO::55TH CT::60 | 55TH COURT | 19TH STREET | 18TH STREET | 21,725 | 50 | 2021 | \$34,312 |
| CCRO::55TH CT::80 | 55TH COURT | 15TH PLACE | 14TH STREET | 27,062 | 54 | 2021 | \$34,122 |
| CCRO::56TH CT::100 | 56TH COURT | 21ST STREET | 19TH STREET | 17,367 | 54 | 2021 | \$20,810 |
| CCRO::56TH CT::120 | 56TH COURT | 18TH STREET | 16TH STREET | 17,266 | 48 | 2021 | \$33,136 |
| CCRO::56TH CT::30 | 56TH COURT | 37TH STREET | 36TH STREET | 17,237 | 54 | 2021 | \$21,734 |
| CCRO::57TH CT::10 | 57TH COURT | 39TH STREET | 38TH STREET | 17,293 | 48 | 2021 | \$33,189 |
| CCRO::57TH CT::80 | 57TH COURT | 25TH STREET | 24TH STREET | 17,283 | 48 | 2021 | \$31,246 |
| CCRO::58TH CT::160 | 58TH COURT | 18TH STREET | 16TH STREET | 17,449 | 49 | 2021 | \$29,579 |
| CCRO::58TH CT::180 | 58TH COURT | 15TH STREET | 14TH STREET | 17,454 | 50 | 2021 | \$27,566 |
| CCRO::58TH CT::90 | 58TH COURT | 26TH STREET | 25TH STREET | 17,479 | 53 | 2021 | \$23,120 |
| CCRO::59TH AVE::160 | 59TH AVENUE | 19TH STREET | 18TH STREET | 17,479 | 49 | 2021 | \$29,630 |
| CCRO::59TH AVE::70 | 59TH AVENUE | PARK AVENUE | OGDEN AVENUE | 19,402 | 48 | 2021 | \$37,235 |
| CCRO::59TH CT::160 | 59TH COURT | 19TH STREET | 18TH STREET | 17,417 | 48 | 2021 | \$31,489 |
| CCRO::59TH CT::170 | 59TH COURT | 18TH STREET | 16TH STREET | 17,412 | 48 | 2021 | \$33,417 |
| CCRO::60TH CT::180 | 60TH COURT | 19TH STREET | 18TH STREET | 15,790 | 53 | 2021 | \$20,886 |
| CCRO::60TH CT::190 | 60TH COURT | 18TH STREET | 16TH STREET | 17,700 | 48 | 2021 | \$33,969 |
| CCRO::61ST CT::130 | 61ST COURT | 25TH STREET | 24TH STREET | 17,177 | 48 | 2021 | \$31,055 |
| CCRO::61ST CT::160 | 61ST COURT | 22ND STREET | 21ST STREET | 17,650 | 52 | 2021 | \$24,425 |
| CCRO::61ST CT::170 | 61ST COURT | 21ST STREET | 19TH STREET | 15,207 | 48 | 2021 | \$29,184 |
| CCRO::CNTRL AVE::250 | CENTRAL AVENUE | 16TH STREET | 15TH PLACE | 10,230 | 53 | 2021 | \$13,302 |
| CCRO::LMBRD AVE::20 | LOMBARD AVENUE | 38TH STREET | 37TH STREET | 17,112 | 48 | 2021 | \$30,937 |
| CCRO::LMBRD AVE::80 | LOMBARD AVENUE | 33RD STREET | 32ND STREET | 16,357 | 53 | 2021 | \$21,635 |

| Pavement ID | Road Name | From | To | Area | PCI | Year | Cost |
|---------------------|-------------|------------------|------------------|--------|-----|------|----------|
| CCRO::PRK AV::80 | PARK AVENUE | 56TH COURT | CENTRAL AVENUE | 8,566 | 48 | 2021 | \$16,439 |
| CCRO::13TH ST::200 | 13TH STREET | 48TH COURT | CICERO AVENUE | 8,458 | 45 | 2022 | \$19,545 |
| CCRO::13TH ST::80 | 13TH STREET | 58TH COURT | 58TH AVENUE | 8,741 | 46 | 2022 | \$19,405 |
| CCRO::14TH ST::40 | 14TH STREET | 60TH COURT | AUSTIN BOULEVARD | 8,645 | 46 | 2022 | \$19,193 |
| CCRO::14TH ST::80 | 14TH STREET | 58TH COURT | 58TH AVENUE | 8,715 | 45 | 2022 | \$20,915 |
| CCRO::14TH ST::90 | 14TH STREET | 58TH AVENUE | 57TH COURT | 8,678 | 46 | 2022 | \$19,266 |
| CCRO::15TH PL::20 | 15TH PLACE | CICERO AVENUE | END | 12,463 | 45 | 2022 | \$29,911 |
| CCRO::15TH ST::90 | 15TH STREET | 58TH AVENUE | 57TH COURT | 8,689 | 45 | 2022 | \$20,852 |
| CCRO::18TH ST::100 | 18TH STREET | 57TH COURT | 57TH AVENUE | 8,621 | 53 | 2022 | \$11,896 |
| CCRO::18TH ST::150 | 18TH STREET | LARAMIE AVENUE | 51ST COURT | 8,575 | 53 | 2022 | \$11,833 |
| CCRO::18TH ST::170 | 18TH STREET | 51ST AVENUE | 50TH COURT | 8,631 | 55 | 2022 | \$10,377 |
| CCRO::18TH ST::200 | 18TH STREET | 49TH COURT | 49TH AVENUE | 8,557 | 53 | 2022 | \$11,808 |
| CCRO::18TH ST::40 | 18TH STREET | 60TH COURT | AUSTIN BOULEVARD | 8,567 | 45 | 2022 | \$20,559 |
| CCRO::19TH ST::130 | 19TH STREET | 58TH AVENUE | 57TH COURT | 8,743 | 53 | 2022 | \$11,556 |
| CCRO::19TH ST::160 | 19TH STREET | 56TH COURT | CENTRAL AVENUE | 8,720 | 52 | 2022 | \$12,533 |
| CCRO::19TH ST::170 | 19TH STREET | CENTRAL AVENUE | 55TH COURT | 8,752 | 53 | 2022 | \$11,568 |
| CCRO::19TH ST::180 | 19TH STREET | 55TH COURT | 55TH AVENUE | 8,634 | 55 | 2022 | \$10,381 |
| CCRO::19TH ST::50 | 19TH STREET | 60TH COURT | AUSTIN BOULEVARD | 4,924 | 54 | 2022 | \$6,219 |
| CCRO::19TH ST::60 | 19TH STREET | 61ST AVENUE | 60TH COURT | 5,048 | 53 | 2022 | \$6,966 |
| CCRO::20TH ST::20 | 20TH STREET | 47TH AVENUE | END | 12,580 | 45 | 2022 | \$30,189 |
| CCRO::21ST PL::40 | 21ST PLACE | 51ST AVENUE | 50TH COURT | 8,568 | 46 | 2022 | \$19,022 |
| CCRO::21ST ST::10 | 21ST STREET | LOMBARD AVENUE | 61ST COURT | 8,396 | 45 | 2022 | \$20,150 |
| CCRO::21ST ST::170 | 21ST STREET | 50TH AVENUE | 49TH COURT | 8,489 | 45 | 2022 | \$20,373 |
| CCRO::21ST ST::180 | 21ST STREET | 49TH COURT | 49TH AVENUE | 8,663 | 52 | 2022 | \$12,451 |
| CCRO::21ST ST::190 | 21ST STREET | 49TH AVENUE | 48TH COURT | 8,825 | 45 | 2022 | \$21,178 |
| CCRO::22ND PL::10 | 22ND PLACE | 58TH AVENUE | 57TH AVENUE | 17,389 | 44 | 2022 | \$43,197 |
| CCRO::22ND PL::20 | 22ND PLACE | 57TH AVENUE | CENTRAL AVENUE | 17,383 | 44 | 2022 | \$43,183 |
| CCRO::23RD PL::70 | 23RD PLACE | LARAMIE AVENUE | 51ST AVENUE | 17,382 | 53 | 2022 | \$23,986 |
| CCRO::23RD ST::110 | 23RD STREET | CENTRAL AVENUE | 55TH AVENUE | 17,347 | 44 | 2022 | \$43,093 |
| CCRO::23RD ST::130 | 23RD STREET | 54TH AVENUE | 53RD AVENUE | 17,510 | 52 | 2022 | \$25,166 |
| CCRO::24TH PL::60 | 24TH PLACE | 51ST AVENUE | 50TH AVENUE | 17,456 | 54 | 2022 | \$22,045 |
| CCRO::24TH ST::110 | 24TH STREET | 57TH AVENUE | 56TH COURT | 8,640 | 45 | 2022 | \$20,735 |
| CCRO::24TH ST::140 | 24TH STREET | 55TH AVENUE | 54TH AVENUE | 17,462 | 44 | 2022 | \$44,797 |
| CCRO::24TH ST::170 | 24TH STREET | LARAMIE AVENUE | 51ST AVENUE | 17,408 | 45 | 2022 | \$41,776 |
| CCRO::24TH ST::50 | 24TH STREET | AUSTIN BOULEVARD | 59TH COURT | 8,634 | 53 | 2022 | \$11,412 |
| CCRO::25TH PL::60 | 25TH PLACE | 51ST AVENUE | 50TH AVENUE | 17,956 | 45 | 2022 | \$43,092 |
| CCRO::25TH ST::160 | 25TH STREET | 53RD AVENUE | LARAMIE AVENUE | 17,242 | 44 | 2022 | \$44,231 |
| CCRO::25TH ST::170 | 25TH STREET | LARAMIE AVENUE | 51ST AVENUE | 17,459 | 45 | 2022 | \$41,900 |
| CCRO::27TH ST::70 | 27TH STREET | 59TH AVENUE | 58TH COURT | 8,680 | 54 | 2022 | \$10,962 |
| CCRO::30TH ST::10 | 30TH STREET | 55TH AVENUE | 54TH AVENUE | 17,516 | 45 | 2022 | \$42,036 |
| CCRO::30TH ST::20 | 30TH STREET | 54TH AVENUE | 53RD AVENUE | 17,618 | 45 | 2022 | \$42,281 |
| CCRO::30TH ST::60 | 30TH STREET | 50TH COURT | 50TH AVENUE | 11,834 | 45 | 2022 | \$28,400 |
| CCRO::30TH ST::80 | 30TH STREET | 49TH AVENUE | 48TH COURT | 11,752 | 53 | 2022 | \$15,534 |
| CCRO::30TH ST::90 | 30TH STREET | 48TH COURT | CICERO AVENUE | 11,369 | 44 | 2022 | \$29,165 |
| CCRO::31ST ST::110 | 31ST STREET | 52ND COURT | LARAMIE AVENUE | 13,275 | 54 | 2022 | \$16,596 |
| CCRO::31ST ST::170 | 31ST STREET | 48TH COURT | CICERO AVENUE | 17,792 | 53 | 2022 | \$24,141 |
| CCRO::34TH ST::150 | 34TH STREET | 55TH AVENUE | 54TH AVENUE | 17,528 | 52 | 2022 | \$25,192 |
| CCRO::35TH ST::100 | 35TH STREET | 57TH COURT | 57TH AVENUE | 8,629 | 53 | 2022 | \$11,708 |
| CCRO::36TH ST::150 | 36TH STREET | 55TH AVENUE | 54TH COURT | 8,705 | 46 | 2022 | \$19,325 |
| CCRO::36TH ST::180 | 36TH STREET | 53RD COURT | 53RD AVENUE | 8,521 | 52 | 2022 | \$12,247 |
| CCRO::37TH ST::160 | 37TH STREET | 54TH COURT | 54TH AVENUE | 8,530 | 54 | 2022 | \$10,772 |
| CCRO::37TH ST::70 | 37TH STREET | 59TH AVENUE | 58TH COURT | 8,683 | 46 | 2022 | \$19,277 |
| CCRO::47TH AVE::30 | 47TH AVENUE | 21ST STREET | 20TH STREET | 8,645 | 45 | 2022 | \$20,747 |
| CCRO::47TH CT::10 | 47TH COURT | 22ND STREET | 21ST STREET | 16,574 | 54 | 2022 | \$20,931 |
| CCRO::49TH AVE::180 | 49TH AVENUE | 22ND PLACE | 22ND STREET | 7,801 | 45 | 2022 | \$18,721 |
| CCRO::49TH AVE::210 | 49TH AVENUE | 21ST STREET | 19TH STREET | 17,366 | 44 | 2022 | \$44,549 |
| CCRO::49TH AVE::220 | 49TH AVENUE | 19TH STREET | 18TH STREET | 17,147 | 44 | 2022 | \$43,986 |
| CCRO::49TH AVE::230 | 49TH AVENUE | 18TH STREET | 16TH STREET | 17,775 | 45 | 2022 | \$42,659 |
| CCRO::49TH CT::50 | 49TH COURT | 16TH STREET | 15TH STREET | 16,710 | 45 | 2022 | \$40,102 |
| CCRO::50TH AVE::240 | 50TH AVENUE | 19TH STREET | 18TH STREET | 26,783 | 44 | 2022 | \$66,533 |

| Pavement ID | Road Name | From | To | Area | PCI | Year | Cost |
|----------------------|----------------|----------------|----------------|--------|-----|------|-----------|
| CCRO::50TH AVE::270 | 50TH AVENUE | 15TH STREET | 14TH STREET | 26,861 | 45 | 2022 | \$64,464 |
| CCRO::50TH AVE::290 | 50TH AVENUE | 13TH STREET | ROOSEVELT ROAD | 26,443 | 45 | 2022 | \$63,459 |
| CCRO::50TH AVE::80 | 50TH AVENUE | 29TH PLACE | 29TH STREET | 8,781 | 53 | 2022 | \$11,607 |
| CCRO::50TH CT::10 | 50TH COURT | 32ND PLACE | END | 8,121 | 45 | 2022 | \$18,765 |
| CCRO::50TH CT::30 | 50TH COURT | 32ND STREET | 31ST PLACE | 8,655 | 45 | 2022 | \$20,771 |
| CCRO::51ST AVE::150 | 51ST AVENUE | 23RD PLACE | 23RD STREET | 8,580 | 52 | 2022 | \$12,332 |
| CCRO::51ST AVE::160 | 51ST AVENUE | 23RD STREET | 22ND PLACE | 8,658 | 45 | 2022 | \$20,779 |
| CCRO::51ST AVE::170 | 51ST AVENUE | 22ND PLACE | 22ND STREET | 8,238 | 45 | 2022 | \$19,037 |
| CCRO::51ST CT::10 | 51ST COURT | 21ST PLACE | 21ST STREET | 9,940 | 45 | 2022 | \$23,855 |
| CCRO::51ST CT::60 | 51ST COURT | 15TH STREET | 14TH STREET | 17,349 | 53 | 2022 | \$23,940 |
| CCRO::51ST CT::80 | 51ST COURT | 13TH STREET | ROOSEVELT ROAD | 17,219 | 53 | 2022 | \$23,761 |
| CCRO::52ND CT::10 | 52ND COURT | 38TH STREET | 37TH STREET | 17,261 | 53 | 2022 | \$23,819 |
| CCRO::53RD AVE::140 | 53RD AVENUE | 24TH PLACE | 24TH STREET | 8,489 | 45 | 2022 | \$19,616 |
| CCRO::53RD AVE::180 | 53RD AVENUE | 22ND PLACE | 22ND STREET | 8,110 | 45 | 2022 | \$18,741 |
| CCRO::53RD AVE::80 | 53RD AVENUE | 30TH PLACE | 30TH STREET | 8,534 | 55 | 2022 | \$10,260 |
| CCRO::54TH AVE::190 | 54TH AVENUE | 16TH STREET | ROOSEVELT ROAD | 68,326 | 45 | 2022 | \$163,974 |
| CCRO::54TH CT::10 | 54TH COURT | 37TH STREET | 36TH STREET | 17,301 | 44 | 2022 | \$44,383 |
| CCRO::55TH AVE::140 | 55TH AVENUE | 22ND PLACE | 22ND STREET | 8,168 | 45 | 2022 | \$18,874 |
| CCRO::55TH AVE::160 | 55TH AVENUE | 19TH STREET | 18TH STREET | 21,389 | 54 | 2022 | \$27,012 |
| CCRO::55TH AVE::70 | 55TH AVENUE | 26TH STREET | 25TH PLACE | 8,658 | 54 | 2022 | \$10,934 |
| CCRO::55TH AVE::90 | 55TH AVENUE | 25TH STREET | 24TH PLACE | 8,597 | 45 | 2022 | \$20,631 |
| CCRO::55TH CT::50 | 55TH COURT | 21ST PLACE | 19TH STREET | 22,807 | 53 | 2022 | \$31,472 |
| CCRO::56TH CT::20 | 56TH COURT | 38TH STREET | 37TH STREET | 17,243 | 52 | 2022 | \$24,782 |
| CCRO::56TH CT::70 | 56TH COURT | 26TH STREET | 25TH STREET | 17,332 | 44 | 2022 | \$44,463 |
| CCRO::57TH AVE::140 | 57TH AVENUE | 21ST STREET | 19TH STREET | 17,650 | 44 | 2022 | \$43,847 |
| CCRO::58TH AVE::170 | 58TH AVENUE | 19TH STREET | 18TH STREET | 17,365 | 44 | 2022 | \$43,137 |
| CCRO::58TH CT::100 | 58TH COURT | 25TH STREET | 24TH STREET | 17,204 | 55 | 2022 | \$20,685 |
| CCRO::58TH CT::150 | 58TH COURT | 19TH STREET | 18TH STREET | 17,576 | 54 | 2022 | \$22,196 |
| CCRO::59TH AVE::140 | 59TH AVENUE | 22ND STREET | 21ST STREET | 17,582 | 45 | 2022 | \$42,194 |
| CCRO::60TH CT::150 | 60TH COURT | 22ND STREET | 21ST STREET | 17,760 | 55 | 2022 | \$21,354 |
| CCRO::61ST CT::180 | 61ST COURT | 19TH STREET | 19TH STREET | 3,142 | 52 | 2022 | \$4,516 |
| CCRO::61ST CT::80 | 61ST COURT | 31ST STREET | END | 9,618 | 55 | 2022 | \$11,564 |
| CCRO::LMBRD AVE::130 | LOMBARD AVENUE | 28TH STREET | 29TH STREET | 21,892 | 54 | 2022 | \$27,646 |
| CCRO::LMBRD AVE::280 | LOMBARD AVENUE | 13TH STREET | ROOSEVELT ROAD | 19,952 | 45 | 2022 | \$47,881 |
| CCRO::14TH ST::120 | 14TH STREET | 56TH COURT | CENTRAL AVENUE | 8,578 | 43 | 2023 | \$23,818 |
| CCRO::14TH ST::200 | 14TH STREET | 49TH AVENUE | 48TH COURT | 8,695 | 43 | 2023 | \$24,142 |
| CCRO::15TH ST::130 | 15TH STREET | LARAMIE AVENUE | 51ST COURT | 8,547 | 52 | 2023 | \$12,443 |
| CCRO::15TH ST::190 | 15TH STREET | 49TH AVENUE | 48TH COURT | 8,767 | 43 | 2023 | \$24,860 |
| CCRO::15TH ST::200 | 15TH STREET | 48TH COURT | CICERO AVENUE | 8,411 | 43 | 2023 | \$23,353 |
| CCRO::18TH ST::120 | 18TH STREET | 56TH COURT | CENTRAL AVENUE | 8,703 | 43 | 2023 | \$24,165 |
| CCRO::18TH ST::190 | 18TH STREET | 50TH AVENUE | 49TH COURT | 8,719 | 52 | 2023 | \$12,693 |
| CCRO::18TH ST::20 | 18TH STREET | 61ST COURT | 61ST AVENUE | 8,797 | 54 | 2023 | \$11,811 |
| CCRO::18TH ST::240 | 18TH STREET | 47TH COURT | END | 3,819 | 53 | 2023 | \$5,343 |
| CCRO::18TH ST::60 | 18TH STREET | 59TH COURT | 59TH AVENUE | 8,568 | 43 | 2023 | \$23,790 |
| CCRO::19TH ST::200 | 19TH STREET | 51ST COURT | 51ST AVENUE | 8,778 | 43 | 2023 | \$24,372 |
| CCRO::19TH ST::240 | 19TH STREET | 49TH COURT | 49TH AVENUE | 8,482 | 43 | 2023 | \$23,551 |
| CCRO::20TH ST::10 | 20TH STREET | CICERO AVENUE | 47TH AVENUE | 17,396 | 42 | 2023 | \$52,522 |
| CCRO::21ST ST::230 | 21ST STREET | 47TH AVENUE | END | 12,319 | 43 | 2023 | \$34,933 |
| CCRO::22ND PL::30 | 22ND PLACE | CENTRAL AVENUE | 55TH AVENUE | 17,304 | 42 | 2023 | \$52,244 |
| CCRO::22ND PL::60 | 22ND PLACE | 53RD AVENUE | LARAMIE AVENUE | 17,385 | 42 | 2023 | \$52,489 |
| CCRO::23RD PL::40 | 23RD PLACE | 55TH AVENUE | 54TH AVENUE | 17,472 | 42 | 2023 | \$52,751 |
| CCRO::23RD ST::140 | 23RD STREET | 53RD AVENUE | LARAMIE AVENUE | 17,098 | 43 | 2023 | \$47,474 |
| CCRO::24TH PL::70 | 24TH PLACE | 50TH AVENUE | 49TH AVENUE | 17,667 | 42 | 2023 | \$51,051 |
| CCRO::24TH ST::20 | 24TH STREET | 61ST COURT | 61ST AVENUE | 8,862 | 52 | 2023 | \$12,901 |
| CCRO::24TH ST::90 | 24TH STREET | 58TH AVENUE | 57TH COURT | 8,670 | 43 | 2023 | \$24,584 |
| CCRO::25TH ST::180 | 25TH STREET | 51ST AVENUE | 50TH AVENUE | 17,676 | 42 | 2023 | \$51,948 |
| CCRO::25TH ST::200 | 25TH STREET | 49TH AVENUE | CICERO AVENUE | 17,107 | 42 | 2023 | \$51,002 |
| CCRO::27TH ST::60 | 27TH STREET | 59TH COURT | 59TH AVENUE | 8,969 | 54 | 2023 | \$12,042 |
| CCRO::28TH ST::100 | 28TH STREET | 48TH COURT | CICERO AVENUE | 11,602 | 53 | 2023 | \$16,233 |
| CCRO::29TH PL::50 | 29TH PLACE | 50TH AVENUE | 49TH AVENUE | 11,982 | 43 | 2023 | \$33,976 |

| Pavement ID | Road Name | From | To | Area | PCI | Year | Cost |
|----------------------|----------------|----------------|----------------|--------|-----|------|----------|
| CCRO::29TH ST::70 | 29TH STREET | 48TH COURT | CICERO AVENUE | 11,381 | 43 | 2023 | \$31,600 |
| CCRO::30TH ST::40 | 30TH STREET | LARAMIE AVENUE | 51ST AVENUE | 11,342 | 43 | 2023 | \$31,494 |
| CCRO::34TH ST::140 | 34TH STREET | 55TH COURT | 55TH AVENUE | 8,549 | 53 | 2023 | \$11,961 |
| CCRO::34TH ST::60 | 34TH STREET | 59TH COURT | 59TH AVENUE | 8,712 | 43 | 2023 | \$24,191 |
| CCRO::37TH ST::140 | 37TH STREET | 55TH COURT | 55TH AVENUE | 8,705 | 43 | 2023 | \$24,685 |
| CCRO::37TH ST::90 | 37TH STREET | 58TH AVENUE | 57TH COURT | 8,487 | 52 | 2023 | \$12,355 |
| CCRO::48TH CT::100 | 48TH COURT | 18TH STREET | 16TH STREET | 17,548 | 42 | 2023 | \$50,708 |
| CCRO::48TH CT::140 | 48TH COURT | 13TH STREET | ROOSEVELT ROAD | 17,500 | 42 | 2023 | \$52,174 |
| CCRO::48TH CT::20 | 48TH COURT | 32ND STREET | 31ST STREET | 17,256 | 53 | 2023 | \$24,144 |
| CCRO::49TH AVE::110 | 49TH AVENUE | 28TH STREET | OGDEN AVENUE | 9,998 | 43 | 2023 | \$27,760 |
| CCRO::49TH AVE::120 | 49TH AVENUE | 25TH PLACE | 25TH STREET | 8,640 | 54 | 2023 | \$11,096 |
| CCRO::49TH AVE::140 | 49TH AVENUE | 24TH PLACE | 24TH STREET | 8,728 | 43 | 2023 | \$24,234 |
| CCRO::49TH AVE::150 | 49TH AVENUE | 24TH STREET | 23RD PLACE | 8,805 | 43 | 2023 | \$24,969 |
| CCRO::49TH AVE::200 | 49TH AVENUE | 21ST PLACE | 21ST STREET | 9,726 | 43 | 2023 | \$27,579 |
| CCRO::50TH AVE::170 | 50TH AVENUE | 24TH STREET | 23RD PLACE | 13,599 | 43 | 2023 | \$37,760 |
| CCRO::50TH AVE::180 | 50TH AVENUE | 23RD PLACE | 23RD STREET | 13,241 | 43 | 2023 | \$37,545 |
| CCRO::50TH CT::50 | 50TH COURT | 31ST STREET | 30TH PLACE | 8,547 | 52 | 2023 | \$12,443 |
| CCRO::52ND CT::30 | 52ND COURT | 36TH STREET | 35TH STREET | 17,451 | 54 | 2023 | \$22,414 |
| CCRO::52ND CT::50 | 52ND COURT | 32ND STREET | 31ST STREET | 17,104 | 53 | 2023 | \$23,932 |
| CCRO::53RD AVE::100 | 53RD AVENUE | 29TH PLACE | OGDEN AVENUE | 6,733 | 52 | 2023 | \$9,802 |
| CCRO::53RD AVE::110 | 53RD AVENUE | 26TH STREET | 25TH PLACE | 8,518 | 43 | 2023 | \$23,652 |
| CCRO::53RD AVE::30 | 53RD AVENUE | 36TH STREET | 35TH STREET | 17,297 | 42 | 2023 | \$50,834 |
| CCRO::54TH AVE::10 | 54TH AVENUE | 38TH STREET | 37TH STREET | 17,271 | 42 | 2023 | \$52,142 |
| CCRO::55TH CT::30 | 55TH COURT | 35TH STREET | 34TH STREET | 17,150 | 54 | 2023 | \$22,027 |
| CCRO::56TH CT::10 | 56TH COURT | 39TH STREET | 38TH STREET | 17,371 | 42 | 2023 | \$51,049 |
| CCRO::56TH CT::130 | 56TH COURT | 16TH STREET | 15TH STREET | 17,109 | 54 | 2023 | \$21,974 |
| CCRO::56TH CT::80 | 56TH COURT | 25TH STREET | 24TH STREET | 17,378 | 42 | 2023 | \$51,070 |
| CCRO::56TH CT::90 | 56TH COURT | 22ND STREET | 21ST STREET | 17,748 | 52 | 2023 | \$25,837 |
| CCRO::57TH AVE::160 | 57TH AVENUE | 18TH STREET | 16TH STREET | 17,377 | 42 | 2023 | \$52,464 |
| CCRO::57TH CT::20 | 57TH COURT | 38TH STREET | 37TH STREET | 17,316 | 42 | 2023 | \$51,626 |
| CCRO::57TH CT::30 | 57TH COURT | 37TH STREET | 36TH STREET | 17,275 | 43 | 2023 | \$47,966 |
| CCRO::57TH CT::40 | 57TH COURT | 36TH STREET | 35TH STREET | 17,460 | 42 | 2023 | \$52,056 |
| CCRO::58TH AVE::160 | 58TH AVENUE | 21ST STREET | 19TH STREET | 17,438 | 42 | 2023 | \$51,248 |
| CCRO::58TH AVE::180 | 58TH AVENUE | 18TH STREET | 16TH STREET | 17,389 | 42 | 2023 | \$52,500 |
| CCRO::58TH CT::140 | 58TH COURT | 21ST STREET | 19TH STREET | 17,326 | 42 | 2023 | \$51,658 |
| CCRO::59TH AVE::130 | 59TH AVENUE | 23RD STREET | 22ND STREET | 16,863 | 42 | 2023 | \$50,912 |
| CCRO::59TH AVE::90 | 59TH AVENUE | 28TH STREET | 27TH STREET | 17,366 | 42 | 2023 | \$51,036 |
| CCRO::59TH CT::140 | 59TH COURT | 22ND STREET | 21ST STREET | 17,610 | 42 | 2023 | \$50,888 |
| CCRO::61ST AVE::180 | 61ST AVENUE | 19TH STREET | 19TH STREET | 2,984 | 54 | 2023 | \$3,832 |
| CCRO::61ST AVE::90 | 61ST AVENUE | 31ST STREET | END | 14,467 | 52 | 2023 | \$21,060 |
| CCRO::61ST CT::30 | 61ST COURT | 37TH STREET | 36TH STREET | 17,183 | 42 | 2023 | \$51,230 |
| CCRO::61ST CT::60 | 61ST COURT | 34TH STREET | OGDEN AVENUE | 18,899 | 53 | 2023 | \$26,443 |
| CCRO::LMBRD AVE::10 | LOMBARD AVENUE | 39TH STREET | 38TH STREET | 17,278 | 42 | 2023 | \$51,514 |
| CCRO::LMBRD AVE::100 | LOMBARD AVENUE | PARK AVENUE | END | 2,340 | 42 | 2023 | \$6,762 |
| CCRO::LMBRD AVE::30 | LOMBARD AVENUE | 37TH STREET | 36TH STREET | 17,472 | 42 | 2023 | \$51,347 |
| CCRO::LMBRD AVE::40 | LOMBARD AVENUE | 36TH STREET | 35TH STREET | 17,337 | 42 | 2023 | \$52,344 |
| CCRO::LMBRD AVE::50 | LOMBARD AVENUE | 35TH STREET | 34TH STREET | 19,850 | 42 | 2023 | \$57,360 |
| CCRO::LMBRD AVE::70 | LOMBARD AVENUE | OGDEN AVENUE | 33RD STREET | 11,282 | 43 | 2023 | \$31,326 |
| CCRO::PRK AV::70 | PARK AVENUE | 57TH AVENUE | 56TH COURT | 8,870 | 43 | 2023 | \$24,630 |
| CCRO::14TH ST::110 | 14TH STREET | 57TH AVENUE | 56TH COURT | 8,809 | 42 | 2024 | \$27,185 |
| CCRO::14TH ST::150 | 14TH STREET | 51ST COURT | 51ST AVENUE | 8,836 | 52 | 2024 | \$13,644 |
| CCRO::14TH ST::210 | 14TH STREET | 48TH COURT | CICERO AVENUE | 8,435 | 42 | 2024 | \$26,273 |
| CCRO::15TH ST::110 | 15TH STREET | 57TH AVENUE | 56TH COURT | 8,838 | 42 | 2024 | \$27,529 |
| CCRO::15TH ST::140 | 15TH STREET | 51ST COURT | 51ST AVENUE | 8,795 | 42 | 2024 | \$27,396 |
| CCRO::18TH ST::110 | 18TH STREET | 57TH AVENUE | 56TH COURT | 8,719 | 42 | 2024 | \$27,158 |
| CCRO::18TH ST::180 | 18TH STREET | 50TH COURT | 50TH AVENUE | 8,793 | 42 | 2024 | \$27,387 |
| CCRO::18TH ST::210 | 18TH STREET | 49TH AVENUE | 48TH COURT | 8,902 | 42 | 2024 | \$27,729 |
| CCRO::24TH PL::50 | 24TH PLACE | LARAMIE AVENUE | 51ST AVENUE | 17,429 | 52 | 2024 | \$25,972 |
| CCRO::24TH PL::80 | 24TH PLACE | 49TH AVENUE | CICERO AVENUE | 17,118 | 42 | 2024 | \$52,827 |
| CCRO::24TH ST::130 | 24TH STREET | CENTRAL AVENUE | 55TH AVENUE | 17,512 | 41 | 2024 | \$55,403 |

| Pavement ID | Road Name | From | To | Area | PCI | Year | Cost |
|----------------------|----------------|------------------|------------------|--------|-----|------|----------|
| CCRO::24TH ST::80 | 24TH STREET | 58TH COURT | 58TH AVENUE | 8,755 | 42 | 2024 | \$27,019 |
| CCRO::25TH ST::130 | 25TH STREET | CENTRAL AVENUE | 55TH AVENUE | 17,747 | 53 | 2024 | \$25,471 |
| CCRO::25TH ST::150 | 25TH STREET | 54TH AVENUE | 53RD AVENUE | 17,506 | 41 | 2024 | \$55,946 |
| CCRO::27TH ST::50 | 27TH STREET | AUSTIN BOULEVARD | 59TH COURT | 8,698 | 54 | 2024 | \$11,984 |
| CCRO::27TH ST::80 | 27TH STREET | 58TH COURT | 58TH AVENUE | 8,302 | 52 | 2024 | \$12,820 |
| CCRO::28TH ST::40 | 28TH STREET | 60TH COURT | AUSTIN BOULEVARD | 8,390 | 52 | 2024 | \$12,503 |
| CCRO::36TH ST::130 | 36TH STREET | CENTRAL AVENUE | 55TH COURT | 8,901 | 42 | 2024 | \$27,470 |
| CCRO::36TH ST::140 | 36TH STREET | 55TH COURT | 55TH AVENUE | 8,724 | 42 | 2024 | \$27,173 |
| CCRO::37TH ST::100 | 37TH STREET | 57TH COURT | 57TH AVENUE | 8,498 | 42 | 2024 | \$26,470 |
| CCRO::37TH ST::110 | 37TH STREET | 57TH AVENUE | 56TH COURT | 9,067 | 42 | 2024 | \$27,980 |
| CCRO::37TH ST::150 | 37TH STREET | 55TH AVENUE | 54TH COURT | 8,722 | 42 | 2024 | \$27,166 |
| CCRO::37TH ST::170 | 37TH STREET | 54TH AVENUE | 53RD COURT | 8,829 | 42 | 2024 | \$27,499 |
| CCRO::37TH ST::50 | 37TH STREET | AUSTIN BOULEVARD | 59TH COURT | 8,279 | 42 | 2024 | \$25,548 |
| CCRO::37TH ST::80 | 37TH STREET | 58TH COURT | 58TH AVENUE | 8,672 | 42 | 2024 | \$26,764 |
| CCRO::38TH ST::60 | 38TH STREET | 59TH COURT | 59TH AVENUE | 9,018 | 54 | 2024 | \$12,425 |
| CCRO::49TH AVE::240 | 49TH AVENUE | 16TH STREET | 15TH STREET | 16,887 | 52 | 2024 | \$25,164 |
| CCRO::49TH CT::60 | 49TH COURT | 15TH STREET | 14TH STREET | 17,525 | 41 | 2024 | \$57,040 |
| CCRO::50TH AVE::140 | 50TH AVENUE | 25TH PLACE | 25TH STREET | 21,273 | 41 | 2024 | \$67,408 |
| CCRO::50TH AVE::160 | 50TH AVENUE | 24TH PLACE | 24TH STREET | 13,441 | 42 | 2024 | \$41,481 |
| CCRO::50TH AVE::230 | 50TH AVENUE | 21ST STREET | 19TH STREET | 26,695 | 41 | 2024 | \$84,456 |
| CCRO::50TH AVE::260 | 50TH AVENUE | 16TH STREET | 15TH STREET | 25,744 | 41 | 2024 | \$81,447 |
| CCRO::50TH CT::110 | 50TH COURT | 21ST STREET | 19TH STREET | 17,116 | 41 | 2024 | \$55,711 |
| CCRO::50TH CT::60 | 50TH COURT | 30TH PLACE | 30TH STREET | 8,694 | 53 | 2024 | \$12,478 |
| CCRO::50TH CT::70 | 50TH COURT | 30TH STREET | 29TH PLACE | 8,651 | 42 | 2024 | \$26,699 |
| CCRO::50TH CT::90 | 50TH COURT | 29TH STREET | OGDEN AVENUE | 16,621 | 42 | 2024 | \$51,293 |
| CCRO::51ST AVE::140 | 51ST AVENUE | 24TH STREET | 23RD PLACE | 8,635 | 53 | 2024 | \$12,393 |
| CCRO::51ST AVE::190 | 51ST AVENUE | 21ST PLACE | 21ST STREET | 10,003 | 52 | 2024 | \$15,447 |
| CCRO::51ST AVE::30 | 51ST AVENUE | 32ND STREET | 31ST PLACE | 8,800 | 42 | 2024 | \$27,410 |
| CCRO::52ND CT::20 | 52ND COURT | 37TH STREET | 36TH STREET | 17,250 | 52 | 2024 | \$26,639 |
| CCRO::52ND CT::40 | 52ND COURT | 33RD STREET | 32ND STREET | 16,950 | 42 | 2024 | \$52,797 |
| CCRO::53RD AVE::90 | 53RD AVENUE | 30TH STREET | 29TH PLACE | 8,907 | 52 | 2024 | \$13,273 |
| CCRO::53RD CT::10 | 53RD COURT | 38TH STREET | 37TH STREET | 17,349 | 54 | 2024 | \$23,903 |
| CCRO::53RD CT::40 | 53RD COURT | 35TH STREET | 34TH STREET | 17,247 | 52 | 2024 | \$25,700 |
| CCRO::54TH AVE::20 | 54TH AVENUE | 37TH STREET | 36TH STREET | 17,175 | 42 | 2024 | \$53,002 |
| CCRO::54TH AVE::40 | 54TH AVENUE | 35TH STREET | 34TH STREET | 17,156 | 54 | 2024 | \$22,651 |
| CCRO::55TH CT::90 | 55TH COURT | 14TH STREET | 13TH STREET | 25,473 | 52 | 2024 | \$37,959 |
| CCRO::57TH CT::100 | 57TH COURT | 21ST STREET | 19TH STREET | 17,381 | 41 | 2024 | \$55,544 |
| CCRO::58TH AVE::80 | 58TH AVENUE | 27TH STREET | 26TH STREET | 17,313 | 54 | 2024 | \$23,854 |
| CCRO::58TH CT::110 | 58TH COURT | 24TH STREET | 23RD STREET | 17,338 | 41 | 2024 | \$54,854 |
| CCRO::58TH CT::190 | 58TH COURT | 14TH STREET | 13TH STREET | 17,448 | 42 | 2024 | \$53,846 |
| CCRO::58TH CT::50 | 58TH COURT | 35TH STREET | 34TH STREET | 17,232 | 54 | 2024 | \$22,752 |
| CCRO::58TH CT::80 | 58TH COURT | 27TH STREET | 26TH STREET | 17,130 | 52 | 2024 | \$25,527 |
| CCRO::59TH AVE::170 | 59TH AVENUE | 18TH STREET | 16TH STREET | 17,420 | 41 | 2024 | \$55,112 |
| CCRO::59TH AVE::20 | 59TH AVENUE | 38TH STREET | 37TH STREET | 17,377 | 42 | 2024 | \$53,627 |
| CCRO::59TH AVE::80 | 59TH AVENUE | OGDEN AVENUE | 31ST STREET | 13,759 | 42 | 2024 | \$42,461 |
| CCRO::59TH CT::120 | 59TH COURT | 24TH STREET | 23RD STREET | 17,300 | 52 | 2024 | \$26,715 |
| CCRO::59TH CT::150 | 59TH COURT | 21ST STREET | 19TH STREET | 17,310 | 54 | 2024 | \$22,855 |
| CCRO::59TH CT::60 | 59TH COURT | 34TH STREET | PARK AVENUE | 21,935 | 41 | 2024 | \$69,506 |
| CCRO::60TH CT::10 | 60TH COURT | 39TH STREET | 38TH STREET | 17,443 | 54 | 2024 | \$23,030 |
| CCRO::60TH CT::170 | 60TH COURT | 19TH STREET | 19TH STREET | 3,668 | 52 | 2024 | \$5,663 |
| CCRO::60TH CT::200 | 60TH COURT | 16TH STREET | 15TH STREET | 17,199 | 53 | 2024 | \$24,684 |
| CCRO::60TH CT::40 | 60TH COURT | 35TH STREET | 34TH STREET | 17,031 | 42 | 2024 | \$52,559 |
| CCRO::61ST AVE::160 | 61ST AVENUE | 22ND STREET | 21ST STREET | 17,805 | 41 | 2024 | \$56,331 |
| CCRO::61ST AVE::80 | 61ST AVENUE | OGDEN AVENUE | END | 5,792 | 41 | 2024 | \$18,853 |
| CCRO::61ST CT::120 | 61ST COURT | 26TH STREET | 25TH STREET | 17,475 | 41 | 2024 | \$55,375 |
| CCRO::61ST CT::190 | 61ST COURT | 19TH STREET | 18TH STREET | 16,436 | 42 | 2024 | \$51,196 |
| CCRO::CNTRL AVE::140 | CENTRAL AVENUE | 24TH PLACE | 24TH STREET | 15,147 | 53 | 2024 | \$22,315 |
| CCRO::CNTRL AVE::50 | CENTRAL AVENUE | 36TH STREET | 35TH STREET | 31,206 | 53 | 2024 | \$45,974 |
| CCRO::LMBRD AVE::110 | LOMBARD AVENUE | 31ST STREET | END | 13,322 | 42 | 2024 | \$41,496 |
| CCRO::LMBRD AVE::60 | LOMBARD AVENUE | 34TH STREET | OGDEN AVENUE | 15,260 | 42 | 2024 | \$47,093 |

| Pavement ID | Road Name | From | To | Area | PCI | Year | Cost |
|---------------------|----------------|-------------|-------------|-------|-----|------|----------|
| CCRO::LMBRD AVE::90 | LOMBARD AVENUE | 32ND STREET | PARK AVENUE | 5,883 | 42 | 2024 | \$18,324 |

**APPENDIX C – TABULATED FIVE-YEAR MAJOR M&R RECOMMENDATIONS AND
ESTIMATED COSTS – ASSUMING UNLIMITED FUNDING**

| Pavement ID | Road Name | From | To | Area | PCI | Year | Cost |
|--------------------|-------------|----------------|------------------|--------|-----|------|-----------|
| CCRO::13TH ST::100 | 13TH STREET | 57TH COURT | 57TH AVENUE | 8,606 | 53 | 2020 | \$11,103 |
| CCRO::13TH ST::110 | 13TH STREET | 57TH AVENUE | 56TH COURT | 8,777 | 52 | 2020 | \$11,903 |
| CCRO::13TH ST::200 | 13TH STREET | 48TH COURT | CICERO AVENUE | 8,458 | 50 | 2020 | \$12,576 |
| CCRO::13TH ST::80 | 13TH STREET | 58TH COURT | 58TH AVENUE | 8,741 | 51 | 2020 | \$12,432 |
| CCRO::13TH ST::90 | 13TH STREET | 58TH AVENUE | 57TH COURT | 8,664 | 53 | 2020 | \$11,179 |
| CCRO::14TH ST::120 | 14TH STREET | 56TH COURT | CENTRAL AVENUE | 8,578 | 48 | 2020 | \$14,851 |
| CCRO::14TH ST::200 | 14TH STREET | 49TH AVENUE | 48TH COURT | 8,695 | 48 | 2020 | \$15,053 |
| CCRO::14TH ST::40 | 14TH STREET | 60TH COURT | AUSTIN BOULEVARD | 8,645 | 51 | 2020 | \$12,296 |
| CCRO::14TH ST::70 | 14TH STREET | 59TH AVENUE | 58TH COURT | 8,797 | 50 | 2020 | \$13,080 |
| CCRO::14TH ST::80 | 14TH STREET | 58TH COURT | 58TH AVENUE | 8,715 | 49 | 2020 | \$13,977 |
| CCRO::14TH ST::90 | 14TH STREET | 58TH AVENUE | 57TH COURT | 8,678 | 51 | 2020 | \$12,342 |
| CCRO::15TH PL::20 | 15TH PLACE | CICERO AVENUE | END | 12,463 | 49 | 2020 | \$19,989 |
| CCRO::15TH ST::160 | 15TH STREET | 50TH COURT | 50TH AVENUE | 8,599 | 50 | 2020 | \$12,785 |
| CCRO::15TH ST::180 | 15TH STREET | 49TH COURT | 49TH AVENUE | 8,808 | 50 | 2020 | \$13,096 |
| CCRO::15TH ST::190 | 15TH STREET | 49TH AVENUE | 48TH COURT | 8,767 | 48 | 2020 | \$16,283 |
| CCRO::15TH ST::200 | 15TH STREET | 48TH COURT | CICERO AVENUE | 8,411 | 48 | 2020 | \$14,561 |
| CCRO::15TH ST::90 | 15TH STREET | 58TH AVENUE | 57TH COURT | 8,689 | 49 | 2020 | \$13,936 |
| CCRO::16TH ST::10 | 16TH STREET | LOMBARD AVENUE | 61ST COURT | 13,415 | 51 | 2020 | \$19,433 |
| CCRO::16TH ST::130 | 16TH STREET | CENTRAL AVENUE | 55TH COURT | 13,713 | 50 | 2020 | \$21,089 |
| CCRO::16TH ST::140 | 16TH STREET | 55TH COURT | 55TH AVENUE | 13,039 | 44 | 2020 | \$30,513 |
| CCRO::16TH ST::150 | 16TH STREET | 55TH AVENUE | 54TH AVENUE | 27,060 | 51 | 2020 | \$39,200 |
| CCRO::16TH ST::160 | 16TH STREET | 54TH AVENUE | LARAMIE AVENUE | 53,153 | 44 | 2020 | \$124,383 |
| CCRO::16TH ST::170 | 16TH STREET | LARAMIE AVENUE | 51ST COURT | 13,172 | 24 | 2020 | \$78,293 |
| CCRO::16TH ST::180 | 16TH STREET | 51ST COURT | 51ST AVENUE | 13,473 | 46 | 2020 | \$27,569 |
| CCRO::16TH ST::190 | 16TH STREET | 51ST AVENUE | 50TH COURT | 13,316 | 48 | 2020 | \$24,012 |
| CCRO::16TH ST::200 | 16TH STREET | 50TH COURT | 50TH AVENUE | 13,174 | 48 | 2020 | \$23,757 |
| CCRO::16TH ST::210 | 16TH STREET | 50TH AVENUE | 49TH COURT | 13,457 | 41 | 2020 | \$38,937 |
| CCRO::16TH ST::220 | 16TH STREET | 49TH COURT | 49TH AVENUE | 13,604 | 36 | 2020 | \$52,484 |
| CCRO::16TH ST::230 | 16TH STREET | 49TH AVENUE | 48TH COURT | 13,599 | 33 | 2020 | \$60,623 |
| CCRO::16TH ST::240 | 16TH STREET | 48TH COURT | CICERO AVENUE | 12,881 | 52 | 2020 | \$17,741 |
| CCRO::18TH ST::120 | 18TH STREET | 56TH COURT | CENTRAL AVENUE | 8,703 | 48 | 2020 | \$15,068 |
| CCRO::18TH ST::160 | 18TH STREET | 51ST COURT | 51ST AVENUE | 8,768 | 53 | 2020 | \$11,313 |
| CCRO::18TH ST::220 | 18TH STREET | 48TH COURT | CICERO AVENUE | 8,444 | 53 | 2020 | \$10,894 |
| CCRO::18TH ST::40 | 18TH STREET | 60TH COURT | AUSTIN BOULEVARD | 8,567 | 49 | 2020 | \$13,740 |
| CCRO::18TH ST::60 | 18TH STREET | 59TH COURT | 59TH AVENUE | 8,568 | 48 | 2020 | \$14,834 |
| CCRO::18TH ST::70 | 18TH STREET | 59TH AVENUE | 58TH COURT | 8,704 | 50 | 2020 | \$12,942 |
| CCRO::18TH ST::80 | 18TH STREET | 58TH COURT | 58TH AVENUE | 8,732 | 55 | 2020 | \$10,100 |
| CCRO::18TH ST::90 | 18TH STREET | 58TH AVENUE | 57TH COURT | 8,802 | 55 | 2020 | \$10,180 |
| CCRO::19TH ST::150 | 19TH STREET | 57TH AVENUE | 56TH COURT | 8,596 | 50 | 2020 | \$12,781 |
| CCRO::19TH ST::200 | 19TH STREET | 51ST COURT | 51ST AVENUE | 8,778 | 48 | 2020 | \$15,197 |
| CCRO::19TH ST::240 | 19TH STREET | 49TH COURT | 49TH AVENUE | 8,482 | 48 | 2020 | \$14,685 |
| CCRO::19TH ST::260 | 19TH STREET | 48TH COURT | CICERO AVENUE | 8,534 | 52 | 2020 | \$11,575 |
| CCRO::19TH ST::30 | 19TH STREET | 61ST AVENUE | 60TH COURT | 5,065 | 49 | 2020 | \$8,124 |
| CCRO::20TH ST::20 | 20TH STREET | 47TH AVENUE | END | 12,580 | 49 | 2020 | \$20,176 |
| CCRO::21ST PL::100 | 21ST PLACE | 47TH AVENUE | END | 12,172 | 52 | 2020 | \$16,508 |
| CCRO::21ST PL::30 | 21ST PLACE | 51ST COURT | 51ST AVENUE | 8,794 | 54 | 2020 | \$10,759 |
| CCRO::21ST PL::40 | 21ST PLACE | 51ST AVENUE | 50TH COURT | 8,568 | 51 | 2020 | \$12,187 |
| CCRO::21ST ST::10 | 21ST STREET | LOMBARD AVENUE | 61ST COURT | 8,396 | 49 | 2020 | \$13,466 |
| CCRO::21ST ST::170 | 21ST STREET | 50TH AVENUE | 49TH COURT | 8,489 | 49 | 2020 | \$13,615 |
| CCRO::21ST ST::190 | 21ST STREET | 49TH AVENUE | 48TH COURT | 8,825 | 49 | 2020 | \$14,153 |
| CCRO::21ST ST::20 | 21ST STREET | 61ST COURT | 61ST AVENUE | 8,993 | 50 | 2020 | \$13,372 |
| CCRO::21ST ST::230 | 21ST STREET | 47TH AVENUE | END | 12,319 | 48 | 2020 | \$22,880 |
| CCRO::21ST ST::30 | 21ST STREET | 61ST AVENUE | 60TH COURT | 8,834 | 54 | 2020 | \$10,807 |
| CCRO::21ST ST::40 | 21ST STREET | 60TH COURT | AUSTIN BOULEVARD | 8,507 | 55 | 2020 | \$9,839 |
| CCRO::22ND PL::10 | 22ND PLACE | 58TH AVENUE | 57TH AVENUE | 17,389 | 48 | 2020 | \$30,105 |
| CCRO::22ND PL::20 | 22ND PLACE | 57TH AVENUE | CENTRAL AVENUE | 17,383 | 48 | 2020 | \$30,095 |
| CCRO::22ND PL::40 | 22ND PLACE | 55TH AVENUE | 54TH AVENUE | 17,492 | 54 | 2020 | \$21,400 |
| CCRO::22ND PL::70 | 22ND PLACE | LARAMIE AVENUE | 51ST AVENUE | 17,340 | 52 | 2020 | \$23,517 |
| CCRO::22ND PL::80 | 22ND PLACE | 51ST AVENUE | 50TH AVENUE | 17,460 | 51 | 2020 | \$24,833 |
| CCRO::23RD PL::100 | 23RD PLACE | 49TH AVENUE | END | 12,798 | 52 | 2020 | \$17,358 |
| CCRO::23RD PL::30 | 23RD PLACE | CENTRAL AVENUE | 55TH AVENUE | 17,394 | 54 | 2020 | \$21,280 |

| Pavement ID | Road Name | From | To | Area | PCI | Year | Cost |
|--------------------|-------------|------------------|----------------|--------|-----|------|-----------|
| CCRO::23RD PL::80 | 23RD PLACE | 51ST AVENUE | 50TH AVENUE | 17,511 | 53 | 2020 | \$22,592 |
| CCRO::23RD ST::110 | 23RD STREET | CENTRAL AVENUE | 55TH AVENUE | 17,347 | 48 | 2020 | \$30,033 |
| CCRO::23RD ST::120 | 23RD STREET | 55TH AVENUE | 54TH AVENUE | 17,483 | 53 | 2020 | \$22,556 |
| CCRO::23RD ST::140 | 23RD STREET | 53RD AVENUE | LARAMIE AVENUE | 17,098 | 48 | 2020 | \$29,601 |
| CCRO::24TH PL::70 | 24TH PLACE | 50TH AVENUE | 49TH AVENUE | 17,667 | 47 | 2020 | \$35,010 |
| CCRO::24TH ST::100 | 24TH STREET | 57TH COURT | 57TH AVENUE | 8,652 | 50 | 2020 | \$12,865 |
| CCRO::24TH ST::110 | 24TH STREET | 57TH AVENUE | 56TH COURT | 8,640 | 49 | 2020 | \$13,858 |
| CCRO::24TH ST::140 | 24TH STREET | 55TH AVENUE | 54TH AVENUE | 17,462 | 48 | 2020 | \$32,432 |
| CCRO::24TH ST::170 | 24TH STREET | LARAMIE AVENUE | 51ST AVENUE | 17,408 | 49 | 2020 | \$27,919 |
| CCRO::24TH ST::180 | 24TH STREET | 51ST AVENUE | 50TH AVENUE | 17,366 | 50 | 2020 | \$25,821 |
| CCRO::24TH ST::60 | 24TH STREET | 59TH COURT | 59TH AVENUE | 8,526 | 52 | 2020 | \$11,563 |
| CCRO::24TH ST::70 | 24TH STREET | 59TH AVENUE | 58TH COURT | 8,890 | 52 | 2020 | \$12,057 |
| CCRO::24TH ST::90 | 24TH STREET | 58TH AVENUE | 57TH COURT | 8,670 | 48 | 2020 | \$16,102 |
| CCRO::25TH PL::60 | 25TH PLACE | 51ST AVENUE | 50TH AVENUE | 17,956 | 49 | 2020 | \$28,799 |
| CCRO::25TH ST::120 | 25TH STREET | 56TH COURT | CENTRAL AVENUE | 8,476 | 53 | 2020 | \$10,936 |
| CCRO::25TH ST::140 | 25TH STREET | 55TH AVENUE | 54TH AVENUE | 17,443 | 52 | 2020 | \$23,657 |
| CCRO::25TH ST::160 | 25TH STREET | 53RD AVENUE | LARAMIE AVENUE | 17,242 | 48 | 2020 | \$32,023 |
| CCRO::25TH ST::170 | 25TH STREET | LARAMIE AVENUE | 51ST AVENUE | 17,459 | 49 | 2020 | \$28,002 |
| CCRO::25TH ST::60 | 25TH STREET | 59TH COURT | 59TH AVENUE | 9,001 | 50 | 2020 | \$13,384 |
| CCRO::25TH ST::70 | 25TH STREET | 59TH AVENUE | 58TH COURT | 8,556 | 50 | 2020 | \$12,721 |
| CCRO::25TH ST::90 | 25TH STREET | 58TH AVENUE | 57TH COURT | 8,667 | 52 | 2020 | \$11,754 |
| CCRO::29TH PL::50 | 29TH PLACE | 50TH AVENUE | 49TH AVENUE | 11,982 | 48 | 2020 | \$22,253 |
| CCRO::29TH ST::60 | 29TH STREET | 49TH AVENUE | 48TH COURT | 11,638 | 51 | 2020 | \$16,553 |
| CCRO::29TH ST::70 | 29TH STREET | 48TH COURT | CICERO AVENUE | 11,381 | 48 | 2020 | \$19,704 |
| CCRO::30TH ST::10 | 30TH STREET | 55TH AVENUE | 54TH AVENUE | 17,516 | 49 | 2020 | \$28,093 |
| CCRO::30TH ST::20 | 30TH STREET | 54TH AVENUE | 53RD AVENUE | 17,618 | 49 | 2020 | \$28,257 |
| CCRO::30TH ST::30 | 30TH STREET | 53RD AVENUE | LARAMIE AVENUE | 17,563 | 54 | 2020 | \$21,487 |
| CCRO::30TH ST::40 | 30TH STREET | LARAMIE AVENUE | 51ST AVENUE | 11,342 | 48 | 2020 | \$19,637 |
| CCRO::30TH ST::50 | 30TH STREET | 51ST AVENUE | 50TH COURT | 11,646 | 55 | 2020 | \$13,470 |
| CCRO::30TH ST::60 | 30TH STREET | 50TH COURT | 50TH AVENUE | 11,834 | 49 | 2020 | \$18,980 |
| CCRO::30TH ST::90 | 30TH STREET | 48TH COURT | CICERO AVENUE | 11,369 | 48 | 2020 | \$21,115 |
| CCRO::31ST ST::10 | 31ST STREET | AUSTIN BOULEVARD | 59TH AVENUE | 26,634 | 23 | 2020 | \$162,305 |
| CCRO::31ST ST::100 | 31ST STREET | 53RD AVENUE | 52ND COURT | 13,607 | 45 | 2020 | \$30,719 |
| CCRO::31ST ST::120 | 31ST STREET | LARAMIE AVENUE | 51ST AVENUE | 17,772 | 51 | 2020 | \$25,744 |
| CCRO::31ST ST::130 | 31ST STREET | 51ST AVENUE | 50TH COURT | 17,735 | 45 | 2020 | \$40,038 |
| CCRO::31ST ST::140 | 31ST STREET | 50TH COURT | 50TH AVENUE | 18,265 | 46 | 2020 | \$37,374 |
| CCRO::31ST ST::150 | 31ST STREET | 50TH AVENUE | 49TH AVENUE | 18,173 | 53 | 2020 | \$23,679 |
| CCRO::31ST ST::160 | 31ST STREET | 49TH AVENUE | 48TH COURT | 18,426 | 53 | 2020 | \$24,009 |
| CCRO::31ST ST::20 | 31ST STREET | 59TH AVENUE | OGDEN AVENUE | 46,955 | 33 | 2020 | \$209,324 |
| CCRO::31ST ST::40 | 31ST STREET | CENTRAL AVENUE | 55TH COURT | 13,486 | 44 | 2020 | \$31,558 |
| CCRO::31ST ST::50 | 31ST STREET | 55TH COURT | 55TH AVENUE | 13,454 | 52 | 2020 | \$18,530 |
| CCRO::31ST ST::70 | 31ST STREET | 54TH COURT | 54TH AVENUE | 13,462 | 49 | 2020 | \$22,519 |
| CCRO::31ST ST::80 | 31ST STREET | 54TH AVENUE | 53RD COURT | 13,336 | 41 | 2020 | \$38,587 |
| CCRO::31ST ST::90 | 31ST STREET | 53RD COURT | 53RD AVENUE | 13,736 | 39 | 2020 | \$44,749 |
| CCRO::34TH ST::50 | 34TH STREET | AUSTIN BOULEVARD | 59TH COURT | 8,714 | 50 | 2020 | \$12,957 |
| CCRO::34TH ST::60 | 34TH STREET | 59TH COURT | 59TH AVENUE | 8,712 | 48 | 2020 | \$15,084 |
| CCRO::35TH ST::120 | 35TH STREET | 56TH COURT | CENTRAL AVENUE | 8,231 | 55 | 2020 | \$9,458 |
| CCRO::35TH ST::130 | 35TH STREET | CENTRAL AVENUE | 55TH COURT | 9,048 | 46 | 2020 | \$18,514 |
| CCRO::35TH ST::140 | 35TH STREET | 55TH COURT | 55TH AVENUE | 8,776 | 46 | 2020 | \$17,957 |
| CCRO::35TH ST::150 | 35TH STREET | 55TH AVENUE | 54TH COURT | 8,776 | 46 | 2020 | \$18,931 |
| CCRO::35TH ST::170 | 35TH STREET | 54TH AVENUE | 53RD COURT | 8,914 | 44 | 2020 | \$20,859 |
| CCRO::35TH ST::180 | 35TH STREET | 53RD COURT | 53RD AVENUE | 8,352 | 46 | 2020 | \$18,017 |
| CCRO::35TH ST::190 | 35TH STREET | 53RD AVENUE | 53RD COURT | 9,134 | 50 | 2020 | \$14,047 |
| CCRO::35TH ST::50 | 35TH STREET | AUSTIN BOULEVARD | 59TH COURT | 8,633 | 50 | 2020 | \$13,277 |
| CCRO::36TH ST::150 | 36TH STREET | 55TH AVENUE | 54TH COURT | 8,705 | 51 | 2020 | \$12,381 |
| CCRO::36TH ST::200 | 36TH STREET | 52ND COURT | LARAMIE AVENUE | 7,725 | 52 | 2020 | \$10,477 |
| CCRO::37TH ST::130 | 37TH STREET | CENTRAL AVENUE | 55TH COURT | 8,886 | 50 | 2020 | \$13,212 |
| CCRO::37TH ST::140 | 37TH STREET | 55TH COURT | 55TH AVENUE | 8,705 | 48 | 2020 | \$16,168 |
| CCRO::37TH ST::60 | 37TH STREET | 59TH COURT | 59TH AVENUE | 8,977 | 52 | 2020 | \$12,176 |
| CCRO::37TH ST::70 | 37TH STREET | 59TH AVENUE | 58TH COURT | 8,683 | 51 | 2020 | \$12,350 |
| CCRO::47TH AVE::30 | 47TH AVENUE | 21ST STREET | 20TH STREET | 8,645 | 49 | 2020 | \$13,865 |

| Pavement ID | Road Name | From | To | Area | PCI | Year | Cost |
|---------------------|-------------|-------------|----------------|--------|-----|------|-----------|
| CCRO::48TH CT::10 | 48TH COURT | 33RD STREET | 32ND STREET | 17,046 | 50 | 2020 | \$25,346 |
| CCRO::48TH CT::100 | 48TH COURT | 18TH STREET | 16TH STREET | 17,548 | 47 | 2020 | \$34,775 |
| CCRO::48TH CT::30 | 48TH COURT | 31ST STREET | 30TH STREET | 17,306 | 50 | 2020 | \$25,732 |
| CCRO::48TH CT::40 | 48TH COURT | 30TH STREET | 29TH STREET | 17,184 | 52 | 2020 | \$23,305 |
| CCRO::48TH CT::50 | 48TH COURT | 29TH STREET | 28TH STREET | 17,427 | 53 | 2020 | \$22,484 |
| CCRO::49TH AVE::110 | 49TH AVENUE | 28TH STREET | OGDEN AVENUE | 9,998 | 48 | 2020 | \$17,309 |
| CCRO::49TH AVE::140 | 49TH AVENUE | 24TH PLACE | 24TH STREET | 8,728 | 48 | 2020 | \$15,110 |
| CCRO::49TH AVE::150 | 49TH AVENUE | 24TH STREET | 23RD PLACE | 8,805 | 48 | 2020 | \$16,354 |
| CCRO::49TH AVE::180 | 49TH AVENUE | 22ND PLACE | 22ND STREET | 7,801 | 49 | 2020 | \$12,511 |
| CCRO::49TH AVE::200 | 49TH AVENUE | 21ST PLACE | 21ST STREET | 9,726 | 48 | 2020 | \$18,064 |
| CCRO::49TH AVE::210 | 49TH AVENUE | 21ST STREET | 19TH STREET | 17,366 | 48 | 2020 | \$32,253 |
| CCRO::49TH AVE::220 | 49TH AVENUE | 19TH STREET | 18TH STREET | 17,147 | 48 | 2020 | \$31,846 |
| CCRO::49TH AVE::230 | 49TH AVENUE | 18TH STREET | 16TH STREET | 17,775 | 49 | 2020 | \$28,509 |
| CCRO::49TH CT::50 | 49TH COURT | 16TH STREET | 15TH STREET | 16,710 | 49 | 2020 | \$26,801 |
| CCRO::50TH AVE::130 | 50TH AVENUE | 26TH STREET | 25TH PLACE | 2,754 | 51 | 2020 | \$3,918 |
| CCRO::50TH AVE::150 | 50TH AVENUE | 25TH STREET | 24TH PLACE | 12,926 | 50 | 2020 | \$19,220 |
| CCRO::50TH AVE::170 | 50TH AVENUE | 24TH STREET | 23RD PLACE | 13,599 | 48 | 2020 | \$23,544 |
| CCRO::50TH AVE::180 | 50TH AVENUE | 23RD PLACE | 23RD STREET | 13,241 | 48 | 2020 | \$24,591 |
| CCRO::50TH AVE::240 | 50TH AVENUE | 19TH STREET | 18TH STREET | 26,783 | 48 | 2020 | \$46,369 |
| CCRO::50TH AVE::250 | 50TH AVENUE | 18TH STREET | 16TH STREET | 27,375 | 50 | 2020 | \$40,703 |
| CCRO::50TH AVE::270 | 50TH AVENUE | 15TH STREET | 14TH STREET | 26,861 | 49 | 2020 | \$43,082 |
| CCRO::50TH AVE::290 | 50TH AVENUE | 13TH STREET | ROOSEVELT ROAD | 26,443 | 49 | 2020 | \$42,410 |
| CCRO::50TH CT::10 | 50TH COURT | 32ND PLACE | END | 8,121 | 50 | 2020 | \$12,074 |
| CCRO::50TH CT::30 | 50TH COURT | 32ND STREET | 31ST PLACE | 8,655 | 49 | 2020 | \$13,881 |
| CCRO::51ST AVE::100 | 51ST AVENUE | 26TH STREET | 25TH PLACE | 8,702 | 50 | 2020 | \$12,938 |
| CCRO::51ST AVE::130 | 51ST AVENUE | 24TH PLACE | 24TH STREET | 8,654 | 54 | 2020 | \$10,588 |
| CCRO::51ST AVE::160 | 51ST AVENUE | 23RD STREET | 22ND PLACE | 8,658 | 49 | 2020 | \$13,887 |
| CCRO::51ST AVE::170 | 51ST AVENUE | 22ND PLACE | 22ND STREET | 8,238 | 50 | 2020 | \$12,249 |
| CCRO::51ST AVE::230 | 51ST AVENUE | 16TH STREET | 15TH STREET | 16,620 | 52 | 2020 | \$22,541 |
| CCRO::51ST AVE::240 | 51ST AVENUE | 15TH STREET | 14TH STREET | 17,416 | 51 | 2020 | \$24,772 |
| CCRO::51ST AVE::40 | 51ST AVENUE | 31ST PLACE | 31ST STREET | 8,318 | 53 | 2020 | \$10,731 |
| CCRO::51ST AVE::50 | 51ST AVENUE | 31ST STREET | 30TH PLACE | 8,755 | 52 | 2020 | \$11,874 |
| CCRO::51ST AVE::90 | 51ST AVENUE | 29TH STREET | OGDEN AVENUE | 11,276 | 55 | 2020 | \$13,042 |
| CCRO::51ST CT::10 | 51ST COURT | 21ST PLACE | 21ST STREET | 9,940 | 49 | 2020 | \$15,942 |
| CCRO::53RD AVE::110 | 53RD AVENUE | 26TH STREET | 25TH PLACE | 8,518 | 48 | 2020 | \$14,748 |
| CCRO::53RD AVE::140 | 53RD AVENUE | 24TH PLACE | 24TH STREET | 8,489 | 50 | 2020 | \$12,622 |
| CCRO::53RD AVE::170 | 53RD AVENUE | 23RD STREET | 22ND PLACE | 8,614 | 50 | 2020 | \$12,807 |
| CCRO::53RD AVE::180 | 53RD AVENUE | 22ND PLACE | 22ND STREET | 8,110 | 50 | 2020 | \$12,059 |
| CCRO::53RD AVE::60 | 53RD AVENUE | 32ND STREET | 31ST STREET | 17,200 | 51 | 2020 | \$24,463 |
| CCRO::54TH AVE::190 | 54TH AVENUE | 16TH STREET | ROOSEVELT ROAD | 68,326 | 49 | 2020 | \$109,585 |
| CCRO::54TH AVE::20 | 54TH AVENUE | 37TH STREET | 36TH STREET | 17,175 | 47 | 2020 | \$34,035 |
| CCRO::54TH CT::10 | 54TH COURT | 37TH STREET | 36TH STREET | 17,301 | 48 | 2020 | \$32,132 |
| CCRO::55TH AVE::140 | 55TH AVENUE | 22ND PLACE | 22ND STREET | 8,168 | 50 | 2020 | \$12,144 |
| CCRO::55TH AVE::170 | 55TH AVENUE | 18TH STREET | 16TH STREET | 18,349 | 55 | 2020 | \$21,222 |
| CCRO::55TH AVE::50 | 55TH AVENUE | 31ST STREET | 30TH PLACE | 8,736 | 52 | 2020 | \$11,848 |
| CCRO::55TH AVE::90 | 55TH AVENUE | 25TH STREET | 24TH PLACE | 8,597 | 49 | 2020 | \$13,788 |
| CCRO::55TH CT::40 | 55TH COURT | 32ND STREET | 31ST STREET | 17,447 | 51 | 2020 | \$24,815 |
| CCRO::55TH CT::60 | 55TH COURT | 19TH STREET | 18TH STREET | 21,725 | 53 | 2020 | \$28,030 |
| CCRO::56TH CT::110 | 56TH COURT | 19TH STREET | 18TH STREET | 17,414 | 55 | 2020 | \$20,141 |
| CCRO::56TH CT::120 | 56TH COURT | 18TH STREET | 16TH STREET | 17,266 | 50 | 2020 | \$25,672 |
| CCRO::56TH CT::140 | 56TH COURT | 15TH STREET | 14TH STREET | 17,374 | 55 | 2020 | \$20,095 |
| CCRO::56TH CT::70 | 56TH COURT | 26TH STREET | 25TH STREET | 17,332 | 48 | 2020 | \$32,190 |
| CCRO::57TH AVE::140 | 57TH AVENUE | 21ST STREET | 19TH STREET | 17,650 | 48 | 2020 | \$30,558 |
| CCRO::57TH CT::10 | 57TH COURT | 39TH STREET | 38TH STREET | 17,293 | 50 | 2020 | \$25,713 |
| CCRO::57TH CT::30 | 57TH COURT | 37TH STREET | 36TH STREET | 17,275 | 48 | 2020 | \$29,908 |
| CCRO::57TH CT::80 | 57TH COURT | 25TH STREET | 24TH STREET | 17,283 | 51 | 2020 | \$24,581 |
| CCRO::58TH AVE::170 | 58TH AVENUE | 19TH STREET | 18TH STREET | 17,365 | 48 | 2020 | \$30,064 |
| CCRO::58TH CT::130 | 58TH COURT | 22ND STREET | 21ST STREET | 17,410 | 55 | 2020 | \$20,136 |
| CCRO::58TH CT::160 | 58TH COURT | 18TH STREET | 16TH STREET | 17,449 | 52 | 2020 | \$23,665 |
| CCRO::58TH CT::180 | 58TH COURT | 15TH STREET | 14TH STREET | 17,454 | 53 | 2020 | \$22,519 |
| CCRO::59TH AVE::140 | 59TH AVENUE | 22ND STREET | 21ST STREET | 17,582 | 49 | 2020 | \$28,199 |

| Pavement ID | Road Name | From | To | Area | PCI | Year | Cost |
|----------------------|------------------|------------------|------------------|--------|-----|------|-----------|
| CCRO::59TH AVE::160 | 59TH AVENUE | 19TH STREET | 18TH STREET | 17,479 | 52 | 2020 | \$23,705 |
| CCRO::59TH AVE::70 | 59TH AVENUE | PARK AVENUE | OGDEN AVENUE | 19,402 | 50 | 2020 | \$28,848 |
| CCRO::59TH CT::130 | 59TH COURT | 23RD STREET | 22ND STREET | 16,897 | 55 | 2020 | \$19,543 |
| CCRO::59TH CT::140 | 59TH COURT | 22ND STREET | 21ST STREET | 17,610 | 47 | 2020 | \$34,898 |
| CCRO::59TH CT::160 | 59TH COURT | 19TH STREET | 18TH STREET | 17,417 | 51 | 2020 | \$24,772 |
| CCRO::59TH CT::170 | 59TH COURT | 18TH STREET | 16TH STREET | 17,412 | 50 | 2020 | \$25,890 |
| CCRO::60TH CT::190 | 60TH COURT | 18TH STREET | 16TH STREET | 17,700 | 50 | 2020 | \$26,318 |
| CCRO::61ST CT::130 | 61ST COURT | 25TH STREET | 24TH STREET | 17,177 | 51 | 2020 | \$24,431 |
| CCRO::61ST CT::170 | 61ST COURT | 21ST STREET | 19TH STREET | 15,207 | 50 | 2020 | \$22,611 |
| CCRO::ASTN BLVD::110 | AUSTIN BOULEVARD | 32ND STREET | 29TH STREET | 29,932 | 31 | 2020 | \$145,410 |
| CCRO::CNTRL AVE::40 | CENTRAL AVENUE | 37TH STREET | 36TH STREET | 31,138 | 52 | 2020 | \$42,886 |
| CCRO::LMBRD AVE::20 | LOMBARD AVENUE | 38TH STREET | 37TH STREET | 17,112 | 51 | 2020 | \$24,338 |
| CCRO::LMBRD AVE::280 | LOMBARD AVENUE | 13TH STREET | ROOSEVELT ROAD | 19,952 | 49 | 2020 | \$31,999 |
| CCRO::LMBRD AVE::50 | LOMBARD AVENUE | 35TH STREET | 34TH STREET | 19,850 | 47 | 2020 | \$39,336 |
| CCRO::LMBRD AVE::70 | LOMBARD AVENUE | OGDEN AVENUE | 33RD STREET | 11,282 | 48 | 2020 | \$19,533 |
| CCRO::LRM AVE::10 | LARAMIE AVENUE | 35TH STREET | 34TH STREET | 32,742 | 46 | 2020 | \$70,629 |
| CCRO::LRM AVE::120 | LARAMIE AVENUE | 26TH STREET | 25TH STREET | 33,254 | 49 | 2020 | \$55,628 |
| CCRO::LRM AVE::130 | LARAMIE AVENUE | 25TH STREET | 24TH PLACE | 16,744 | 53 | 2020 | \$21,817 |
| CCRO::LRM AVE::140 | LARAMIE AVENUE | 24TH PLACE | 24TH STREET | 13,335 | 47 | 2020 | \$25,707 |
| CCRO::LRM AVE::150 | LARAMIE AVENUE | 24TH STREET | 23RD PLACE | 13,454 | 54 | 2020 | \$16,511 |
| CCRO::LRM AVE::160 | LARAMIE AVENUE | 23RD PLACE | 23RD STREET | 13,068 | 44 | 2020 | \$30,580 |
| CCRO::LRM AVE::170 | LARAMIE AVENUE | 23RD STREET | 22ND PLACE | 13,301 | 43 | 2020 | \$33,440 |
| CCRO::LRM AVE::190 | LARAMIE AVENUE | 22ND STREET | 21ST PLACE | 15,345 | 52 | 2020 | \$21,134 |
| CCRO::LRM AVE::20 | LARAMIE AVENUE | 34TH STREET | 33RD STREET | 33,758 | 35 | 2020 | \$136,990 |
| CCRO::LRM AVE::240 | LARAMIE AVENUE | 16TH STREET | 15TH STREET | 28,612 | 52 | 2020 | \$39,406 |
| CCRO::LRM AVE::250 | LARAMIE AVENUE | 15TH STREET | 14TH STREET | 29,567 | 46 | 2020 | \$60,499 |
| CCRO::LRM AVE::260 | LARAMIE AVENUE | 14TH STREET | 13TH STREET | 30,146 | 45 | 2020 | \$68,058 |
| CCRO::LRM AVE::270 | LARAMIE AVENUE | 13TH STREET | ROOSEVELT ROAD | 29,785 | 53 | 2020 | \$38,809 |
| CCRO::LRM AVE::30 | LARAMIE AVENUE | 33RD STREET | 32ND PLACE | 20,177 | 45 | 2020 | \$45,551 |
| CCRO::LRM AVE::40 | LARAMIE AVENUE | 32ND PLACE | 32ND STREET | 19,569 | 44 | 2020 | \$45,793 |
| CCRO::LRM AVE::50 | LARAMIE AVENUE | 32ND STREET | 31ST PLACE | 20,203 | 44 | 2020 | \$47,277 |
| CCRO::LRM AVE::60 | LARAMIE AVENUE | 31ST PLACE | 31ST STREET | 19,452 | 47 | 2020 | \$37,499 |
| CCRO::LRM AVE::70 | LARAMIE AVENUE | 31ST STREET | 30TH PLACE | 20,513 | 44 | 2020 | \$48,002 |
| CCRO::LRM AVE::80 | LARAMIE AVENUE | 30TH PLACE | 30TH STREET | 19,556 | 41 | 2020 | \$56,582 |
| CCRO::LRM AVE::90 | LARAMIE AVENUE | 30TH STREET | 29TH PLACE | 23,166 | 51 | 2020 | \$33,559 |
| CCRO::PRK AV::70 | PARK AVENUE | 57TH AVENUE | 56TH COURT | 8,870 | 48 | 2020 | \$15,357 |
| CCRO::PRK AV::80 | PARK AVENUE | 56TH COURT | CENTRAL AVENUE | 8,566 | 50 | 2020 | \$12,736 |
| CCRO::13TH ST::170 | 13TH STREET | 50TH AVENUE | 49TH COURT | 8,777 | 42 | 2021 | \$24,949 |
| CCRO::13TH ST::190 | 13TH STREET | 49TH AVENUE | 48TH COURT | 8,623 | 42 | 2021 | \$24,514 |
| CCRO::13TH ST::40 | 13TH STREET | 60TH COURT | AUSTIN BOULEVARD | 8,848 | 54 | 2021 | \$10,602 |
| CCRO::13TH ST::70 | 13TH STREET | 59TH AVENUE | 58TH COURT | 8,815 | 42 | 2021 | \$25,058 |
| CCRO::14TH ST::110 | 14TH STREET | 57TH AVENUE | 56TH COURT | 8,809 | 45 | 2021 | \$20,606 |
| CCRO::14TH ST::130 | 14TH STREET | CENTRAL AVENUE | 55TH COURT | 8,437 | 54 | 2021 | \$10,110 |
| CCRO::14TH ST::160 | 14TH STREET | 51ST AVENUE | 50TH COURT | 8,565 | 54 | 2021 | \$10,263 |
| CCRO::14TH ST::170 | 14TH STREET | 50TH COURT | 50TH AVENUE | 8,633 | 53 | 2021 | \$11,419 |
| CCRO::14TH ST::180 | 14TH STREET | 50TH AVENUE | 49TH COURT | 8,765 | 54 | 2021 | \$11,052 |
| CCRO::14TH ST::190 | 14TH STREET | 49TH COURT | 49TH AVENUE | 8,773 | 42 | 2021 | \$24,940 |
| CCRO::14TH ST::210 | 14TH STREET | 48TH COURT | CICERO AVENUE | 8,435 | 44 | 2021 | \$20,565 |
| CCRO::14TH ST::230 | 14TH STREET | 47TH AVENUE | END | 14,723 | 52 | 2021 | \$20,374 |
| CCRO::14TH ST::30 | 14TH STREET | 61ST AVENUE | 60TH COURT | 8,650 | 43 | 2021 | \$22,666 |
| CCRO::14TH ST::60 | 14TH STREET | 59TH COURT | 59TH AVENUE | 8,534 | 42 | 2021 | \$23,707 |
| CCRO::15TH ST::10 | 15TH STREET | LOMBARD AVENUE | 61ST COURT | 8,681 | 44 | 2021 | \$21,970 |
| CCRO::15TH ST::100 | 15TH STREET | 57TH COURT | 57TH AVENUE | 8,664 | 54 | 2021 | \$10,925 |
| CCRO::15TH ST::110 | 15TH STREET | 57TH AVENUE | 56TH COURT | 8,838 | 44 | 2021 | \$21,549 |
| CCRO::15TH ST::140 | 15TH STREET | 51ST COURT | 51ST AVENUE | 8,795 | 44 | 2021 | \$21,444 |
| CCRO::15TH ST::150 | 15TH STREET | 51ST AVENUE | 50TH COURT | 8,615 | 42 | 2021 | \$23,931 |
| CCRO::15TH ST::170 | 15TH STREET | 50TH AVENUE | 49TH COURT | 8,756 | 43 | 2021 | \$23,661 |
| CCRO::15TH ST::30 | 15TH STREET | 61ST AVENUE | 60TH COURT | 8,659 | 43 | 2021 | \$22,688 |
| CCRO::15TH ST::50 | 15TH STREET | AUSTIN BOULEVARD | 59TH COURT | 8,889 | 41 | 2021 | \$26,242 |
| CCRO::15TH ST::70 | 15TH STREET | 59TH AVENUE | 58TH COURT | 8,775 | 43 | 2021 | \$23,710 |
| CCRO::16TH ST::250 | 16TH STREET | CICERO AVENUE | 47TH COURT | 13,620 | 53 | 2021 | \$17,710 |

| Pavement ID | Road Name | From | To | Area | PCI | Year | Cost |
|--------------------|-------------|------------------|------------------|--------|-----|------|----------|
| CCRO::16TH ST::260 | 16TH STREET | 47TH COURT | END | 17,488 | 53 | 2021 | \$22,740 |
| CCRO::18TH ST::10 | 18TH STREET | LOMBARD AVENUE | 61ST COURT | 8,612 | 52 | 2021 | \$11,917 |
| CCRO::18TH ST::110 | 18TH STREET | 57TH AVENUE | 56TH COURT | 8,719 | 44 | 2021 | \$21,258 |
| CCRO::18TH ST::180 | 18TH STREET | 50TH COURT | 50TH AVENUE | 8,793 | 44 | 2021 | \$21,437 |
| CCRO::18TH ST::210 | 18TH STREET | 49TH AVENUE | 48TH COURT | 8,902 | 44 | 2021 | \$21,705 |
| CCRO::18TH ST::30 | 18TH STREET | 61ST AVENUE | 60TH COURT | 8,660 | 52 | 2021 | \$11,983 |
| CCRO::19TH ST::100 | 19TH STREET | 59TH COURT | 59TH AVENUE | 8,569 | 42 | 2021 | \$24,360 |
| CCRO::19TH ST::110 | 19TH STREET | 59TH AVENUE | 58TH COURT | 8,659 | 42 | 2021 | \$24,053 |
| CCRO::19TH ST::120 | 19TH STREET | 58TH COURT | 58TH AVENUE | 8,806 | 39 | 2021 | \$29,222 |
| CCRO::19TH ST::140 | 19TH STREET | 57TH COURT | 57TH AVENUE | 8,673 | 43 | 2021 | \$23,435 |
| CCRO::19TH ST::190 | 19TH STREET | LARAMIE AVENUE | 51ST COURT | 8,564 | 44 | 2021 | \$21,674 |
| CCRO::19TH ST::280 | 19TH STREET | 47TH COURT | 47TH AVENUE | 8,224 | 43 | 2021 | \$22,222 |
| CCRO::19TH ST::300 | 19TH STREET | 46TH COURT | END | 4,612 | 40 | 2021 | \$14,355 |
| CCRO::19TH ST::40 | 19TH STREET | 60TH COURT | AUSTIN BOULEVARD | 4,960 | 54 | 2021 | \$5,943 |
| CCRO::19TH ST::90 | 19TH STREET | AUSTIN BOULEVARD | 59TH COURT | 8,748 | 54 | 2021 | \$11,030 |
| CCRO::20TH ST::10 | 20TH STREET | CICERO AVENUE | 47TH AVENUE | 17,396 | 43 | 2021 | \$45,584 |
| CCRO::21ST PL::20 | 21ST PLACE | LARAMIE AVENUE | 51ST COURT | 8,725 | 44 | 2021 | \$22,081 |
| CCRO::21ST PL::50 | 21ST PLACE | 50TH COURT | 50TH AVENUE | 8,988 | 43 | 2021 | \$23,552 |
| CCRO::21ST PL::60 | 21ST PLACE | 50TH AVENUE | 49TH COURT | 8,382 | 44 | 2021 | \$21,213 |
| CCRO::21ST ST::200 | 21ST STREET | 48TH COURT | CICERO AVENUE | 8,543 | 43 | 2021 | \$23,084 |
| CCRO::22ND PL::30 | 22ND PLACE | CENTRAL AVENUE | 55TH AVENUE | 17,304 | 43 | 2021 | \$45,343 |
| CCRO::22ND PL::50 | 22ND PLACE | 54TH AVENUE | 53RD AVENUE | 17,256 | 54 | 2021 | \$21,757 |
| CCRO::22ND PL::60 | 22ND PLACE | 53RD AVENUE | LARAMIE AVENUE | 17,385 | 43 | 2021 | \$45,555 |
| CCRO::23RD PL::40 | 23RD PLACE | 55TH AVENUE | 54TH AVENUE | 17,472 | 43 | 2021 | \$45,783 |
| CCRO::24TH PL::80 | 24TH PLACE | 49TH AVENUE | CICERO AVENUE | 17,118 | 45 | 2021 | \$40,043 |
| CCRO::24TH ST::10 | 24TH STREET | LOMBARD AVENUE | 61ST COURT | 8,584 | 54 | 2021 | \$10,285 |
| CCRO::24TH ST::120 | 24TH STREET | 56TH COURT | CENTRAL AVENUE | 8,675 | 52 | 2021 | \$12,005 |
| CCRO::24TH ST::130 | 24TH STREET | CENTRAL AVENUE | 55TH AVENUE | 17,512 | 43 | 2021 | \$47,321 |
| CCRO::24TH ST::190 | 24TH STREET | 50TH AVENUE | 49TH AVENUE | 17,747 | 52 | 2021 | \$24,558 |
| CCRO::24TH ST::200 | 24TH STREET | 49TH AVENUE | CICERO AVENUE | 17,092 | 53 | 2021 | \$22,608 |
| CCRO::24TH ST::80 | 24TH STREET | 58TH COURT | 58TH AVENUE | 8,755 | 45 | 2021 | \$20,480 |
| CCRO::25TH ST::100 | 25TH STREET | 57TH COURT | 57TH AVENUE | 8,609 | 54 | 2021 | \$10,316 |
| CCRO::25TH ST::110 | 25TH STREET | 57TH AVENUE | 56TH COURT | 8,697 | 54 | 2021 | \$10,421 |
| CCRO::25TH ST::150 | 25TH STREET | 54TH AVENUE | 53RD AVENUE | 17,506 | 41 | 2021 | \$50,577 |
| CCRO::25TH ST::180 | 25TH STREET | 51ST AVENUE | 50TH AVENUE | 17,676 | 44 | 2021 | \$43,096 |
| CCRO::25TH ST::190 | 25TH STREET | 50TH AVENUE | 49TH AVENUE | 17,449 | 40 | 2021 | \$54,309 |
| CCRO::25TH ST::200 | 25TH STREET | 49TH AVENUE | CICERO AVENUE | 17,107 | 44 | 2021 | \$43,293 |
| CCRO::25TH ST::50 | 25TH STREET | AUSTIN BOULEVARD | 59TH COURT | 8,632 | 53 | 2021 | \$11,417 |
| CCRO::25TH ST::80 | 25TH STREET | 58TH COURT | 58TH AVENUE | 8,661 | 42 | 2021 | \$24,622 |
| CCRO::27TH ST::10 | 27TH STREET | LOMBARD AVENUE | 61ST COURT | 8,881 | 40 | 2021 | \$27,642 |
| CCRO::28TH ST::80 | 28TH STREET | 50TH AVENUE | 49TH AVENUE | 11,807 | 53 | 2021 | \$15,617 |
| CCRO::29TH PL::30 | 29TH PLACE | 51ST AVENUE | 50TH COURT | 11,642 | 40 | 2021 | \$36,235 |
| CCRO::29TH ST::10 | 29TH STREET | 60TH COURT | AUSTIN BOULEVARD | 8,485 | 42 | 2021 | \$24,122 |
| CCRO::29TH ST::20 | 29TH STREET | LARAMIE AVENUE | END | 9,946 | 41 | 2021 | \$29,365 |
| CCRO::29TH ST::30 | 29TH STREET | 51ST AVENUE | 50TH COURT | 11,628 | 43 | 2021 | \$31,421 |
| CCRO::29TH ST::40 | 29TH STREET | 50TH COURT | 50TH AVENUE | 11,803 | 42 | 2021 | \$32,788 |
| CCRO::30TH ST::70 | 30TH STREET | 50TH AVENUE | 49TH AVENUE | 11,928 | 53 | 2021 | \$15,777 |
| CCRO::31ST ST::60 | 31ST STREET | 55TH AVENUE | 54TH COURT | 13,474 | 53 | 2021 | \$17,521 |
| CCRO::32ND PL::10 | 32ND PLACE | LARAMIE AVENUE | 51ST AVENUE | 11,405 | 42 | 2021 | \$32,421 |
| CCRO::32ND ST::150 | 32ND STREET | 51ST AVENUE | 50TH COURT | 12,469 | 42 | 2021 | \$35,446 |
| CCRO::34TH ST::110 | 34TH STREET | 57TH AVENUE | 56TH COURT | 8,882 | 43 | 2021 | \$24,001 |
| CCRO::34TH ST::130 | 34TH STREET | CENTRAL AVENUE | 55TH COURT | 8,820 | 52 | 2021 | \$12,205 |
| CCRO::35TH ST::160 | 35TH STREET | 54TH COURT | 54TH AVENUE | 8,500 | 54 | 2021 | \$10,303 |
| CCRO::35TH ST::40 | 35TH STREET | 60TH COURT | AUSTIN BOULEVARD | 8,958 | 41 | 2021 | \$25,880 |
| CCRO::36TH ST::130 | 36TH STREET | CENTRAL AVENUE | 55TH COURT | 8,901 | 45 | 2021 | \$20,823 |
| CCRO::36TH ST::140 | 36TH STREET | 55TH COURT | 55TH AVENUE | 8,724 | 44 | 2021 | \$21,270 |
| CCRO::36TH ST::170 | 36TH STREET | 54TH AVENUE | 53RD COURT | 8,817 | 43 | 2021 | \$23,103 |
| CCRO::36TH ST::190 | 36TH STREET | 53RD AVENUE | 52ND COURT | 8,883 | 52 | 2021 | \$12,293 |
| CCRO::37TH ST::100 | 37TH STREET | 57TH COURT | 57TH AVENUE | 8,498 | 44 | 2021 | \$20,719 |
| CCRO::37TH ST::110 | 37TH STREET | 57TH AVENUE | 56TH COURT | 9,067 | 45 | 2021 | \$21,209 |
| CCRO::37TH ST::120 | 37TH STREET | 56TH COURT | CENTRAL AVENUE | 8,381 | 52 | 2021 | \$11,597 |

| Pavement ID | Road Name | From | To | Area | PCI | Year | Cost |
|---------------------|-------------|------------------|----------------|--------|-----|------|-----------|
| CCRO::37TH ST::150 | 37TH STREET | 55TH AVENUE | 54TH COURT | 8,722 | 44 | 2021 | \$21,265 |
| CCRO::37TH ST::170 | 37TH STREET | 54TH AVENUE | 53RD COURT | 8,829 | 44 | 2021 | \$21,525 |
| CCRO::37TH ST::50 | 37TH STREET | AUSTIN BOULEVARD | 59TH COURT | 8,279 | 45 | 2021 | \$19,365 |
| CCRO::37TH ST::80 | 37TH STREET | 58TH COURT | 58TH AVENUE | 8,672 | 45 | 2021 | \$20,287 |
| CCRO::47TH AVE::50 | 47TH AVENUE | 14TH STREET | 13TH STREET | 17,126 | 42 | 2021 | \$48,684 |
| CCRO::47TH AVE::70 | 47TH AVENUE | 12TH PLACE | ROOSEVELT ROAD | 8,787 | 40 | 2021 | \$27,351 |
| CCRO::48TH CT::120 | 48TH COURT | 15TH STREET | 14TH STREET | 17,260 | 43 | 2021 | \$46,641 |
| CCRO::48TH CT::130 | 48TH COURT | 14TH STREET | 13TH STREET | 17,214 | 40 | 2021 | \$53,578 |
| CCRO::48TH CT::140 | 48TH COURT | 13TH STREET | ROOSEVELT ROAD | 17,500 | 44 | 2021 | \$44,287 |
| CCRO::48TH CT::60 | 48TH COURT | 28TH STREET | END | 8,147 | 43 | 2021 | \$22,014 |
| CCRO::49TH AVE::10 | 49TH AVENUE | 32ND PLACE | 33RD STREET | 8,109 | 41 | 2021 | \$23,940 |
| CCRO::49TH AVE::100 | 49TH AVENUE | 28TH PLACE | 28TH STREET | 8,688 | 42 | 2021 | \$24,133 |
| CCRO::49TH AVE::130 | 49TH AVENUE | 25TH STREET | 24TH PLACE | 8,533 | 42 | 2021 | \$23,703 |
| CCRO::49TH AVE::190 | 49TH AVENUE | 22ND STREET | 21ST PLACE | 8,143 | 52 | 2021 | \$11,269 |
| CCRO::49TH AVE::30 | 49TH AVENUE | 32ND STREET | 31ST PLACE | 8,837 | 41 | 2021 | \$26,091 |
| CCRO::49TH AVE::70 | 49TH AVENUE | 30TH STREET | 29TH PLACE | 8,499 | 44 | 2021 | \$21,509 |
| CCRO::49TH AVE::80 | 49TH AVENUE | 29TH PLACE | 29TH STREET | 8,838 | 39 | 2021 | \$29,329 |
| CCRO::49TH AVE::90 | 49TH AVENUE | 29TH STREET | 28TH PLACE | 8,539 | 42 | 2021 | \$24,273 |
| CCRO::49TH CT::60 | 49TH COURT | 15TH STREET | 14TH STREET | 17,525 | 41 | 2021 | \$51,739 |
| CCRO::50TH AVE::110 | 50TH AVENUE | 28TH STREET | OGDEN AVENUE | 4,636 | 40 | 2021 | \$14,431 |
| CCRO::50TH AVE::140 | 50TH AVENUE | 25TH PLACE | 25TH STREET | 21,273 | 42 | 2021 | \$59,093 |
| CCRO::50TH AVE::160 | 50TH AVENUE | 24TH PLACE | 24TH STREET | 13,441 | 45 | 2021 | \$31,442 |
| CCRO::50TH AVE::230 | 50TH AVENUE | 21ST STREET | 19TH STREET | 26,695 | 43 | 2021 | \$72,135 |
| CCRO::50TH AVE::260 | 50TH AVENUE | 16TH STREET | 15TH STREET | 25,744 | 43 | 2021 | \$69,565 |
| CCRO::50TH AVE::280 | 50TH AVENUE | 14TH STREET | 13TH STREET | 26,588 | 39 | 2021 | \$88,234 |
| CCRO::50TH AVE::40 | 50TH AVENUE | 31ST PLACE | 31ST STREET | 8,502 | 44 | 2021 | \$21,517 |
| CCRO::50TH CT::110 | 50TH COURT | 21ST STREET | 19TH STREET | 17,116 | 41 | 2021 | \$50,534 |
| CCRO::50TH CT::20 | 50TH COURT | 32ND PLACE | 32ND STREET | 8,662 | 44 | 2021 | \$21,921 |
| CCRO::50TH CT::40 | 50TH COURT | 31ST PLACE | 31ST STREET | 8,667 | 41 | 2021 | \$25,039 |
| CCRO::50TH CT::70 | 50TH COURT | 30TH STREET | 29TH PLACE | 8,651 | 45 | 2021 | \$20,238 |
| CCRO::50TH CT::80 | 50TH COURT | 29TH PLACE | 29TH STREET | 8,666 | 42 | 2021 | \$24,072 |
| CCRO::50TH CT::90 | 50TH COURT | 29TH STREET | OGDEN AVENUE | 16,621 | 45 | 2021 | \$38,880 |
| CCRO::51ST AVE::10 | 51ST AVENUE | 32ND PLACE | END | 7,979 | 44 | 2021 | \$20,194 |
| CCRO::51ST AVE::120 | 51ST AVENUE | 25TH STREET | 24TH PLACE | 8,879 | 54 | 2021 | \$11,195 |
| CCRO::51ST AVE::20 | 51ST AVENUE | 32ND PLACE | 32ND STREET | 8,655 | 43 | 2021 | \$23,388 |
| CCRO::51ST AVE::210 | 51ST AVENUE | 19TH STREET | 18TH STREET | 17,183 | 42 | 2021 | \$48,846 |
| CCRO::51ST AVE::30 | 51ST AVENUE | 32ND STREET | 31ST PLACE | 8,800 | 44 | 2021 | \$21,455 |
| CCRO::51ST AVE::70 | 51ST AVENUE | 30TH STREET | 29TH PLACE | 8,623 | 40 | 2021 | \$26,839 |
| CCRO::51ST AVE::80 | 51ST AVENUE | 29TH PLACE | 29TH STREET | 8,726 | 54 | 2021 | \$10,456 |
| CCRO::51ST CT::20 | 51ST COURT | 21ST STREET | 19TH STREET | 17,065 | 54 | 2021 | \$21,517 |
| CCRO::51ST CT::50 | 51ST COURT | 16TH STREET | 15TH STREET | 16,530 | 54 | 2021 | \$19,807 |
| CCRO::52ND CT::40 | 52ND COURT | 33RD STREET | 32ND STREET | 16,950 | 44 | 2021 | \$41,326 |
| CCRO::53RD AVE::150 | 53RD AVENUE | 24TH STREET | 23RD PLACE | 8,878 | 40 | 2021 | \$27,632 |
| CCRO::53RD AVE::20 | 53RD AVENUE | 37TH STREET | 36TH STREET | 17,282 | 42 | 2021 | \$49,127 |
| CCRO::53RD AVE::30 | 53RD AVENUE | 36TH STREET | 35TH STREET | 17,297 | 44 | 2021 | \$42,172 |
| CCRO::53RD AVE::50 | 53RD AVENUE | 33RD STREET | 32ND STREET | 17,123 | 54 | 2021 | \$20,517 |
| CCRO::53RD AVE::70 | 53RD AVENUE | 31ST STREET | 30TH PLACE | 9,069 | 54 | 2021 | \$10,867 |
| CCRO::53RD CT::50 | 53RD COURT | 33RD STREET | 32ND STREET | 17,306 | 52 | 2021 | \$23,949 |
| CCRO::53RD CT::60 | 53RD COURT | 32ND STREET | 31ST STREET | 17,138 | 40 | 2021 | \$53,343 |
| CCRO::54TH AVE::10 | 54TH AVENUE | 38TH STREET | 37TH STREET | 17,271 | 43 | 2021 | \$45,254 |
| CCRO::54TH AVE::180 | 54TH AVENUE | 22ND STREET | 16TH STREET | 70,690 | 39 | 2021 | \$234,585 |
| CCRO::54TH AVE::70 | 54TH AVENUE | 31ST STREET | 30TH PLACE | 8,870 | 41 | 2021 | \$26,186 |
| CCRO::55TH AVE::100 | 55TH AVENUE | 24TH PLACE | 24TH STREET | 8,786 | 41 | 2021 | \$25,382 |
| CCRO::55TH AVE::120 | 55TH AVENUE | 23RD PLACE | 23RD STREET | 8,665 | 42 | 2021 | \$24,632 |
| CCRO::55TH AVE::40 | 55TH AVENUE | 32ND STREET | 31ST STREET | 17,143 | 53 | 2021 | \$22,675 |
| CCRO::55TH CT::80 | 55TH COURT | 15TH PLACE | 14TH STREET | 27,062 | 54 | 2021 | \$34,122 |
| CCRO::56TH CT::10 | 56TH COURT | 39TH STREET | 38TH STREET | 17,371 | 44 | 2021 | \$42,351 |
| CCRO::56TH CT::100 | 56TH COURT | 21ST STREET | 19TH STREET | 17,367 | 54 | 2021 | \$20,810 |
| CCRO::56TH CT::30 | 56TH COURT | 37TH STREET | 36TH STREET | 17,237 | 54 | 2021 | \$21,734 |
| CCRO::56TH CT::80 | 56TH COURT | 25TH STREET | 24TH STREET | 17,378 | 44 | 2021 | \$42,368 |
| CCRO::57TH AVE::150 | 57TH AVENUE | 19TH STREET | 18TH STREET | 17,238 | 43 | 2021 | \$46,580 |

| Pavement ID | Road Name | From | To | Area | PCI | Year | Cost |
|----------------------|----------------|------------------|------------------|--------|-----|------|----------|
| CCRO::57TH AVE::160 | 57TH AVENUE | 18TH STREET | 16TH STREET | 17,377 | 43 | 2021 | \$45,533 |
| CCRO::57TH AVE::190 | 57TH AVENUE | 14TH STREET | 13TH STREET | 17,544 | 40 | 2021 | \$54,607 |
| CCRO::57TH CT::100 | 57TH COURT | 21ST STREET | 19TH STREET | 17,381 | 41 | 2021 | \$50,214 |
| CCRO::57TH CT::20 | 57TH COURT | 38TH STREET | 37TH STREET | 17,316 | 44 | 2021 | \$43,822 |
| CCRO::57TH CT::40 | 57TH COURT | 36TH STREET | 35TH STREET | 17,460 | 44 | 2021 | \$44,187 |
| CCRO::57TH CT::70 | 57TH COURT | 26TH STREET | 25TH STREET | 17,331 | 39 | 2021 | \$57,512 |
| CCRO::58TH AVE::160 | 58TH AVENUE | 21ST STREET | 19TH STREET | 17,438 | 44 | 2021 | \$42,516 |
| CCRO::58TH AVE::180 | 58TH AVENUE | 18TH STREET | 16TH STREET | 17,389 | 43 | 2021 | \$45,565 |
| CCRO::58TH CT::110 | 58TH COURT | 24TH STREET | 23RD STREET | 17,338 | 43 | 2021 | \$46,852 |
| CCRO::58TH CT::140 | 58TH COURT | 21ST STREET | 19TH STREET | 17,326 | 44 | 2021 | \$43,849 |
| CCRO::58TH CT::170 | 58TH COURT | 16TH STREET | 15TH STREET | 16,966 | 44 | 2021 | \$42,937 |
| CCRO::58TH CT::190 | 58TH COURT | 14TH STREET | 13TH STREET | 17,448 | 45 | 2021 | \$40,815 |
| CCRO::58TH CT::200 | 58TH COURT | 13TH STREET | ROOSEVELT ROAD | 17,210 | 39 | 2021 | \$57,110 |
| CCRO::58TH CT::90 | 58TH COURT | 26TH STREET | 25TH STREET | 17,479 | 53 | 2021 | \$23,120 |
| CCRO::59TH AVE::130 | 59TH AVENUE | 23RD STREET | 22ND STREET | 16,863 | 43 | 2021 | \$44,187 |
| CCRO::59TH AVE::170 | 59TH AVENUE | 18TH STREET | 16TH STREET | 17,420 | 43 | 2021 | \$47,072 |
| CCRO::59TH AVE::20 | 59TH AVENUE | 38TH STREET | 37TH STREET | 17,377 | 45 | 2021 | \$40,649 |
| CCRO::59TH AVE::80 | 59TH AVENUE | OGDEN AVENUE | 31ST STREET | 13,759 | 45 | 2021 | \$32,185 |
| CCRO::59TH AVE::90 | 59TH AVENUE | 28TH STREET | 27TH STREET | 17,366 | 44 | 2021 | \$42,340 |
| CCRO::59TH CT::60 | 59TH COURT | 34TH STREET | PARK AVENUE | 21,935 | 42 | 2021 | \$62,355 |
| CCRO::60TH CT::140 | 60TH COURT | 23RD STREET | 22ND STREET | 17,038 | 43 | 2021 | \$46,041 |
| CCRO::60TH CT::180 | 60TH COURT | 19TH STREET | 18TH STREET | 15,790 | 53 | 2021 | \$20,886 |
| CCRO::60TH CT::40 | 60TH COURT | 35TH STREET | 34TH STREET | 17,031 | 45 | 2021 | \$39,840 |
| CCRO::61ST AVE::140 | 61ST AVENUE | 24TH STREET | 23RD STREET | 17,570 | 39 | 2021 | \$58,305 |
| CCRO::61ST AVE::160 | 61ST AVENUE | 22ND STREET | 21ST STREET | 17,805 | 43 | 2021 | \$48,114 |
| CCRO::61ST AVE::170 | 61ST AVENUE | 21ST STREET | 19TH STREET | 15,258 | 43 | 2021 | \$39,981 |
| CCRO::61ST AVE::190 | 61ST AVENUE | 19TH STREET | 18TH STREET | 16,396 | 40 | 2021 | \$51,032 |
| CCRO::61ST AVE::80 | 61ST AVENUE | OGDEN AVENUE | END | 5,792 | 41 | 2021 | \$17,101 |
| CCRO::61ST CT::120 | 61ST COURT | 26TH STREET | 25TH STREET | 17,475 | 42 | 2021 | \$48,544 |
| CCRO::61ST CT::160 | 61ST COURT | 22ND STREET | 21ST STREET | 17,650 | 52 | 2021 | \$24,425 |
| CCRO::61ST CT::190 | 61ST COURT | 19TH STREET | 18TH STREET | 16,436 | 44 | 2021 | \$40,073 |
| CCRO::61ST CT::30 | 61ST COURT | 37TH STREET | 36TH STREET | 17,183 | 44 | 2021 | \$43,485 |
| CCRO::61ST CT::50 | 61ST COURT | 35TH STREET | 34TH STREET | 17,297 | 39 | 2021 | \$57,402 |
| CCRO::CNTRL AVE::250 | CENTRAL AVENUE | 16TH STREET | 15TH PLACE | 10,230 | 53 | 2021 | \$13,302 |
| CCRO::LMBRD AVE::10 | LOMBARD AVENUE | 39TH STREET | 38TH STREET | 17,278 | 44 | 2021 | \$43,727 |
| CCRO::LMBRD AVE::100 | LOMBARD AVENUE | PARK AVENUE | END | 2,340 | 45 | 2021 | \$5,474 |
| CCRO::LMBRD AVE::110 | LOMBARD AVENUE | 31ST STREET | END | 13,322 | 44 | 2021 | \$32,481 |
| CCRO::LMBRD AVE::30 | LOMBARD AVENUE | 37TH STREET | 36TH STREET | 17,472 | 44 | 2021 | \$42,598 |
| CCRO::LMBRD AVE::40 | LOMBARD AVENUE | 36TH STREET | 35TH STREET | 17,337 | 43 | 2021 | \$45,430 |
| CCRO::LMBRD AVE::60 | LOMBARD AVENUE | 34TH STREET | OGDEN AVENUE | 15,260 | 45 | 2021 | \$35,697 |
| CCRO::LMBRD AVE::80 | LOMBARD AVENUE | 33RD STREET | 32ND STREET | 16,357 | 53 | 2021 | \$21,635 |
| CCRO::LMBRD AVE::90 | LOMBARD AVENUE | 32ND STREET | PARK AVENUE | 5,883 | 44 | 2021 | \$14,343 |
| CCRO::13TH ST::10 | 13TH STREET | LOMBARD AVENUE | 61ST COURT | 8,604 | 35 | 2022 | \$36,804 |
| CCRO::13TH ST::130 | 13TH STREET | LARAMIE AVENUE | 51ST COURT | 8,580 | 37 | 2022 | \$33,060 |
| CCRO::13TH ST::150 | 13TH STREET | 51ST AVENUE | 50TH COURT | 8,520 | 27 | 2022 | \$49,479 |
| CCRO::13TH ST::20 | 13TH STREET | 61ST COURT | 61ST AVENUE | 8,746 | 35 | 2022 | \$37,409 |
| CCRO::13TH ST::210 | 13TH STREET | CICERO AVENUE | 47TH AVENUE | 17,400 | 36 | 2022 | \$70,735 |
| CCRO::13TH ST::50 | 13TH STREET | AUSTIN BOULEVARD | 59TH COURT | 8,797 | 36 | 2022 | \$35,764 |
| CCRO::13TH ST::60 | 13TH STREET | 59TH COURT | 59TH AVENUE | 8,504 | 34 | 2022 | \$38,179 |
| CCRO::14TH ST::10 | 14TH STREET | LOMBARD AVENUE | 61ST COURT | 8,643 | 35 | 2022 | \$36,969 |
| CCRO::14TH ST::100 | 14TH STREET | 57TH COURT | 57TH AVENUE | 8,637 | 35 | 2022 | \$36,945 |
| CCRO::14TH ST::140 | 14TH STREET | LARAMIE AVENUE | 51ST COURT | 8,515 | 38 | 2022 | \$31,003 |
| CCRO::14TH ST::20 | 14TH STREET | 61ST COURT | 61ST AVENUE | 8,725 | 33 | 2022 | \$41,025 |
| CCRO::14TH ST::220 | 14TH STREET | CICERO AVENUE | 47TH AVENUE | 17,313 | 36 | 2022 | \$70,383 |
| CCRO::15TH ST::120 | 15TH STREET | 56TH COURT | CENTRAL AVENUE | 8,630 | 37 | 2022 | \$33,251 |
| CCRO::15TH ST::20 | 15TH STREET | 61ST COURT | 61ST AVENUE | 8,713 | 29 | 2022 | \$47,827 |
| CCRO::15TH ST::40 | 15TH STREET | 60TH COURT | AUSTIN BOULEVARD | 8,453 | 30 | 2022 | \$45,054 |
| CCRO::15TH ST::60 | 15TH STREET | 59TH COURT | 59TH AVENUE | 8,549 | 33 | 2022 | \$40,195 |
| CCRO::15TH ST::80 | 15TH STREET | 58TH COURT | 58TH AVENUE | 8,692 | 34 | 2022 | \$39,023 |
| CCRO::18TH ST::100 | 18TH STREET | 57TH COURT | 57TH AVENUE | 8,621 | 53 | 2022 | \$11,896 |
| CCRO::18TH ST::130 | 18TH STREET | CENTRAL AVENUE | 55TH COURT | 8,721 | 27 | 2022 | \$50,645 |

| Pavement ID | Road Name | From | To | Area | PCI | Year | Cost |
|--------------------|-------------|------------------|------------------|--------|-----|------|-----------|
| CCRO::18TH ST::150 | 18TH STREET | LARAMIE AVENUE | 51ST COURT | 8,575 | 53 | 2022 | \$11,833 |
| CCRO::18TH ST::170 | 18TH STREET | 51ST AVENUE | 50TH COURT | 8,631 | 55 | 2022 | \$10,377 |
| CCRO::18TH ST::200 | 18TH STREET | 49TH COURT | 49TH AVENUE | 8,557 | 53 | 2022 | \$11,808 |
| CCRO::19TH ST::130 | 19TH STREET | 58TH AVENUE | 57TH COURT | 8,743 | 53 | 2022 | \$11,556 |
| CCRO::19TH ST::160 | 19TH STREET | 56TH COURT | CENTRAL AVENUE | 8,720 | 52 | 2022 | \$12,533 |
| CCRO::19TH ST::170 | 19TH STREET | CENTRAL AVENUE | 55TH COURT | 8,752 | 53 | 2022 | \$11,568 |
| CCRO::19TH ST::180 | 19TH STREET | 55TH COURT | 55TH AVENUE | 8,634 | 55 | 2022 | \$10,381 |
| CCRO::19TH ST::220 | 19TH STREET | 50TH COURT | 50TH AVENUE | 8,883 | 33 | 2022 | \$41,765 |
| CCRO::19TH ST::230 | 19TH STREET | 50TH AVENUE | 49TH COURT | 8,678 | 32 | 2022 | \$42,645 |
| CCRO::19TH ST::250 | 19TH STREET | 49TH AVENUE | 48TH COURT | 8,905 | 36 | 2022 | \$36,203 |
| CCRO::19TH ST::50 | 19TH STREET | 60TH COURT | AUSTIN BOULEVARD | 4,924 | 54 | 2022 | \$6,219 |
| CCRO::19TH ST::60 | 19TH STREET | 61ST AVENUE | 60TH COURT | 5,048 | 53 | 2022 | \$6,966 |
| CCRO::21ST ST::180 | 21ST STREET | 49TH COURT | 49TH AVENUE | 8,663 | 52 | 2022 | \$12,451 |
| CCRO::23RD PL::20 | 23RD PLACE | 57TH AVENUE | CENTRAL AVENUE | 17,386 | 29 | 2022 | \$95,437 |
| CCRO::23RD PL::60 | 23RD PLACE | 53RD AVENUE | LARAMIE AVENUE | 17,135 | 37 | 2022 | \$66,022 |
| CCRO::23RD PL::70 | 23RD PLACE | LARAMIE AVENUE | 51ST AVENUE | 17,382 | 53 | 2022 | \$23,986 |
| CCRO::23RD PL::90 | 23RD PLACE | 50TH AVENUE | 49TH AVENUE | 17,581 | 37 | 2022 | \$67,743 |
| CCRO::23RD ST::130 | 23RD STREET | 54TH AVENUE | 53RD AVENUE | 17,510 | 52 | 2022 | \$25,166 |
| CCRO::23RD ST::150 | 23RD STREET | LARAMIE AVENUE | 51ST AVENUE | 17,389 | 36 | 2022 | \$70,692 |
| CCRO::23RD ST::160 | 23RD STREET | 51ST AVENUE | 50TH AVENUE | 17,730 | 28 | 2022 | \$100,144 |
| CCRO::23RD ST::170 | 23RD STREET | 50TH AVENUE | 49TH AVENUE | 17,314 | 34 | 2022 | \$77,736 |
| CCRO::24TH PL::60 | 24TH PLACE | 51ST AVENUE | 50TH AVENUE | 17,456 | 54 | 2022 | \$22,045 |
| CCRO::24TH ST::50 | 24TH STREET | AUSTIN BOULEVARD | 59TH COURT | 8,634 | 53 | 2022 | \$11,412 |
| CCRO::25TH PL::30 | 25TH PLACE | 54TH AVENUE | 53RD AVENUE | 17,507 | 38 | 2022 | \$63,740 |
| CCRO::25TH PL::50 | 25TH PLACE | LARAMIE AVENUE | 51ST AVENUE | 16,360 | 38 | 2022 | \$59,564 |
| CCRO::25TH ST::10 | 25TH STREET | LOMBARD AVENUE | 61ST COURT | 8,733 | 37 | 2022 | \$33,647 |
| CCRO::25TH ST::20 | 25TH STREET | 61ST COURT | 61ST AVENUE | 8,747 | 33 | 2022 | \$41,129 |
| CCRO::25TH ST::30 | 25TH STREET | 61ST AVENUE | 60TH COURT | 8,870 | 31 | 2022 | \$45,469 |
| CCRO::25TH ST::40 | 25TH STREET | 60TH COURT | AUSTIN BOULEVARD | 8,412 | 33 | 2022 | \$39,550 |
| CCRO::27TH ST::30 | 27TH STREET | 61ST AVENUE | 60TH COURT | 8,907 | 28 | 2022 | \$50,309 |
| CCRO::27TH ST::40 | 27TH STREET | 60TH COURT | AUSTIN BOULEVARD | 8,355 | 30 | 2022 | \$44,534 |
| CCRO::27TH ST::70 | 27TH STREET | 59TH AVENUE | 58TH COURT | 8,680 | 54 | 2022 | \$10,962 |
| CCRO::28TH ST::50 | 28TH STREET | AUSTIN BOULEVARD | 59TH COURT | 8,767 | 30 | 2022 | \$46,729 |
| CCRO::29TH PL::20 | 29TH PLACE | LARAMIE AVENUE | 51ST AVENUE | 11,231 | 30 | 2022 | \$59,863 |
| CCRO::29TH ST::50 | 29TH STREET | 50TH AVENUE | 49TH AVENUE | 11,929 | 28 | 2022 | \$67,378 |
| CCRO::30TH PL::20 | 30TH PLACE | 54TH AVENUE | 53RD AVENUE | 17,608 | 36 | 2022 | \$71,581 |
| CCRO::30TH PL::30 | 30TH PLACE | 53RD AVENUE | LARAMIE AVENUE | 17,592 | 37 | 2022 | \$67,785 |
| CCRO::30TH ST::80 | 30TH STREET | 49TH AVENUE | 48TH COURT | 11,752 | 53 | 2022 | \$15,534 |
| CCRO::31ST ST::110 | 31ST STREET | 52ND COURT | LARAMIE AVENUE | 13,275 | 54 | 2022 | \$16,596 |
| CCRO::31ST ST::170 | 31ST STREET | 48TH COURT | CICERO AVENUE | 17,792 | 53 | 2022 | \$24,141 |
| CCRO::32ND PL::20 | 32ND PLACE | 51ST AVENUE | 50TH COURT | 11,605 | 34 | 2022 | \$52,102 |
| CCRO::32ND ST::140 | 32ND STREET | LARAMIE AVENUE | 51ST AVENUE | 12,362 | 32 | 2022 | \$60,750 |
| CCRO::32ND ST::160 | 32ND STREET | 50TH COURT | 50TH AVENUE | 12,814 | 32 | 2022 | \$62,970 |
| CCRO::32ND ST::170 | 32ND STREET | 50TH AVENUE | 49TH AVENUE | 12,661 | 32 | 2022 | \$62,217 |
| CCRO::34TH ST::150 | 34TH STREET | 55TH AVENUE | 54TH AVENUE | 17,528 | 52 | 2022 | \$25,192 |
| CCRO::34TH ST::70 | 34TH STREET | 59TH AVENUE | 58TH COURT | 8,719 | 30 | 2022 | \$46,475 |
| CCRO::35TH ST::10 | 35TH STREET | LOMBARD AVENUE | 61ST COURT | 8,548 | 30 | 2022 | \$45,559 |
| CCRO::35TH ST::100 | 35TH STREET | 57TH COURT | 57TH AVENUE | 8,629 | 53 | 2022 | \$11,708 |
| CCRO::35TH ST::20 | 35TH STREET | 61ST COURT | 61ST AVENUE | 8,679 | 29 | 2022 | \$47,643 |
| CCRO::36TH ST::180 | 36TH STREET | 53RD COURT | 53RD AVENUE | 8,521 | 52 | 2022 | \$12,247 |
| CCRO::36TH ST::20 | 36TH STREET | 61ST COURT | 61ST AVENUE | 8,708 | 29 | 2022 | \$47,798 |
| CCRO::36TH ST::30 | 36TH STREET | 61ST AVENUE | 60TH COURT | 8,412 | 32 | 2022 | \$41,337 |
| CCRO::37TH ST::160 | 37TH STREET | 54TH COURT | 54TH AVENUE | 8,530 | 54 | 2022 | \$10,772 |
| CCRO::37TH ST::190 | 37TH STREET | 53RD AVENUE | 52ND COURT | 8,706 | 27 | 2022 | \$50,558 |
| CCRO::37TH ST::20 | 37TH STREET | 61ST COURT | 61ST AVENUE | 8,733 | 31 | 2022 | \$44,770 |
| CCRO::37TH ST::200 | 37TH STREET | 52ND COURT | LARAMIE AVENUE | 7,836 | 27 | 2022 | \$45,505 |
| CCRO::47TH AVE::10 | 47TH AVENUE | 22ND STREET | 21ST PLACE | 9,706 | 28 | 2022 | \$54,820 |
| CCRO::47TH AVE::20 | 47TH AVENUE | 21ST PLACE | 21ST STREET | 8,002 | 29 | 2022 | \$43,925 |
| CCRO::47TH AVE::40 | 47TH AVENUE | 20TH STREET | 19TH STREET | 8,710 | 27 | 2022 | \$50,582 |
| CCRO::47TH CT::10 | 47TH COURT | 22ND STREET | 21ST STREET | 16,574 | 54 | 2022 | \$20,931 |
| CCRO::49TH AVE::20 | 49TH AVENUE | 32ND PLACE | 32ND STREET | 8,684 | 31 | 2022 | \$44,517 |

| Pavement ID | Road Name | From | To | Area | PCI | Year | Cost |
|----------------------|----------------|----------------|----------------|--------|-----|------|-----------|
| CCRO::49TH AVE::40 | 49TH AVENUE | 31ST PLACE | 31ST STREET | 8,429 | 37 | 2022 | \$32,480 |
| CCRO::49TH AVE::50 | 49TH AVENUE | 31ST STREET | 30TH PLACE | 8,747 | 28 | 2022 | \$49,405 |
| CCRO::49TH AVE::60 | 49TH AVENUE | 30TH PLACE | 30TH STREET | 8,684 | 37 | 2022 | \$33,460 |
| CCRO::49TH CT::70 | 49TH COURT | 14TH STREET | 13TH STREET | 17,134 | 34 | 2022 | \$76,925 |
| CCRO::50TH AVE::30 | 50TH AVENUE | 32ND STREET | 31ST PLACE | 8,614 | 37 | 2022 | \$33,191 |
| CCRO::50TH AVE::50 | 50TH AVENUE | 31ST STREET | 30TH PLACE | 8,769 | 31 | 2022 | \$44,951 |
| CCRO::50TH AVE::80 | 50TH AVENUE | 29TH PLACE | 29TH STREET | 8,781 | 53 | 2022 | \$11,607 |
| CCRO::51ST AVE::150 | 51ST AVENUE | 23RD PLACE | 23RD STREET | 8,580 | 52 | 2022 | \$12,332 |
| CCRO::51ST AVE::200 | 51ST AVENUE | 21ST STREET | 19TH STREET | 17,073 | 33 | 2022 | \$80,275 |
| CCRO::51ST AVE::60 | 51ST AVENUE | 30TH PLACE | 30TH STREET | 8,733 | 36 | 2022 | \$35,502 |
| CCRO::51ST CT::30 | 51ST COURT | 19TH STREET | 18TH STREET | 17,218 | 28 | 2022 | \$97,254 |
| CCRO::51ST CT::40 | 51ST COURT | 18TH STREET | 16TH STREET | 18,142 | 32 | 2022 | \$89,154 |
| CCRO::51ST CT::60 | 51ST COURT | 15TH STREET | 14TH STREET | 17,349 | 53 | 2022 | \$23,940 |
| CCRO::51ST CT::70 | 51ST COURT | 14TH STREET | 13TH STREET | 17,246 | 33 | 2022 | \$81,090 |
| CCRO::51ST CT::80 | 51ST COURT | 13TH STREET | ROOSEVELT ROAD | 17,219 | 53 | 2022 | \$23,761 |
| CCRO::52ND CT::10 | 52ND COURT | 38TH STREET | 37TH STREET | 17,261 | 53 | 2022 | \$23,819 |
| CCRO::53RD AVE::130 | 53RD AVENUE | 25TH STREET | 24TH PLACE | 8,593 | 28 | 2022 | \$48,538 |
| CCRO::53RD AVE::40 | 53RD AVENUE | 35TH STREET | END | 8,528 | 37 | 2022 | \$32,859 |
| CCRO::53RD AVE::80 | 53RD AVENUE | 30TH PLACE | 30TH STREET | 8,534 | 55 | 2022 | \$10,260 |
| CCRO::54TH AVE::30 | 54TH AVENUE | 36TH STREET | 35TH STREET | 17,427 | 33 | 2022 | \$81,941 |
| CCRO::54TH AVE::80 | 54TH AVENUE | 30TH PLACE | 30TH STREET | 8,633 | 39 | 2022 | \$29,598 |
| CCRO::54TH AVE::90 | 54TH AVENUE | 30TH STREET | OGDEN AVENUE | 8,074 | 34 | 2022 | \$36,252 |
| CCRO::55TH AVE::130 | 55TH AVENUE | 23RD STREET | 22ND PLACE | 8,629 | 39 | 2022 | \$29,586 |
| CCRO::55TH AVE::160 | 55TH AVENUE | 19TH STREET | 18TH STREET | 21,389 | 54 | 2022 | \$27,012 |
| CCRO::55TH AVE::70 | 55TH AVENUE | 26TH STREET | 25TH PLACE | 8,658 | 54 | 2022 | \$10,934 |
| CCRO::55TH CT::10 | 55TH COURT | 37TH STREET | 36TH STREET | 17,198 | 30 | 2022 | \$91,666 |
| CCRO::55TH CT::50 | 55TH COURT | 21ST PLACE | 19TH STREET | 22,807 | 53 | 2022 | \$31,472 |
| CCRO::55TH CT::70 | 55TH COURT | 18TH STREET | 16TH STREET | 18,192 | 34 | 2022 | \$81,678 |
| CCRO::56TH CT::20 | 56TH COURT | 38TH STREET | 37TH STREET | 17,243 | 52 | 2022 | \$24,782 |
| CCRO::57TH AVE::170 | 57TH AVENUE | 16TH STREET | 15TH STREET | 17,174 | 28 | 2022 | \$97,007 |
| CCRO::57TH AVE::200 | 57TH AVENUE | 13TH STREET | ROOSEVELT ROAD | 17,335 | 26 | 2022 | \$103,435 |
| CCRO::58TH AVE::150 | 58TH AVENUE | 22ND STREET | 21ST STREET | 17,648 | 27 | 2022 | \$102,492 |
| CCRO::58TH CT::100 | 58TH COURT | 25TH STREET | 24TH STREET | 17,204 | 55 | 2022 | \$20,685 |
| CCRO::58TH CT::150 | 58TH COURT | 19TH STREET | 18TH STREET | 17,576 | 54 | 2022 | \$22,196 |
| CCRO::58TH CT::60 | 58TH COURT | 34TH STREET | PARK AVENUE | 16,000 | 32 | 2022 | \$78,623 |
| CCRO::59TH CT::50 | 59TH COURT | 35TH STREET | 34TH STREET | 17,043 | 29 | 2022 | \$93,551 |
| CCRO::60TH CT::130 | 60TH COURT | 24TH STREET | 23RD STREET | 17,080 | 29 | 2022 | \$93,753 |
| CCRO::60TH CT::150 | 60TH COURT | 22ND STREET | 21ST STREET | 17,760 | 55 | 2022 | \$21,354 |
| CCRO::61ST AVE::100 | 61ST AVENUE | 28TH STREET | 27TH STREET | 17,439 | 32 | 2022 | \$85,695 |
| CCRO::61ST AVE::200 | 61ST AVENUE | 18TH STREET | 16TH STREET | 17,424 | 29 | 2022 | \$95,645 |
| CCRO::61ST CT::180 | 61ST COURT | 19TH STREET | 19TH STREET | 3,142 | 52 | 2022 | \$4,516 |
| CCRO::61ST CT::240 | 61ST COURT | 13TH STREET | ROOSEVELT ROAD | 17,091 | 30 | 2022 | \$91,094 |
| CCRO::61ST CT::80 | 61ST COURT | 31ST STREET | END | 9,618 | 55 | 2022 | \$11,564 |
| CCRO::FRNTG RD::10 | FRONTAGE ROAD | CICERO AVENUE | 31ST STREET | 31,940 | 33 | 2022 | \$150,179 |
| CCRO::FRNTG RD::20 | FRONTAGE ROAD | 31ST STREET | END | 28,340 | 26 | 2022 | \$169,097 |
| CCRO::LMBRD AVE::130 | LOMBARD AVENUE | 28TH STREET | 29TH STREET | 21,892 | 54 | 2022 | \$27,646 |
| CCRO::LMBRD AVE::220 | LOMBARD AVENUE | 19TH STREET | 19TH STREET | 3,435 | 33 | 2022 | \$16,152 |
| CCRO::PRK AV::40 | PARK AVENUE | 58TH COURT | 58TH AVENUE | 9,025 | 30 | 2022 | \$48,104 |
| CCRO::12TH PL::10 | 12TH PLACE | CICERO AVENUE | 47TH AVENUE | 17,435 | 24 | 2023 | \$113,042 |
| CCRO::13TH ST::160 | 13TH STREET | 50TH COURT | 50TH AVENUE | 8,669 | 25 | 2023 | \$54,783 |
| CCRO::13TH ST::180 | 13TH STREET | 49TH COURT | 49TH AVENUE | 8,736 | 24 | 2023 | \$56,641 |
| CCRO::15TH ST::130 | 15TH STREET | LARAMIE AVENUE | 51ST COURT | 8,547 | 52 | 2023 | \$12,443 |
| CCRO::18TH ST::140 | 18TH STREET | 55TH COURT | 55TH AVENUE | 8,547 | 25 | 2023 | \$54,014 |
| CCRO::18TH ST::190 | 18TH STREET | 50TH AVENUE | 49TH COURT | 8,719 | 52 | 2023 | \$12,693 |
| CCRO::18TH ST::20 | 18TH STREET | 61ST COURT | 61ST AVENUE | 8,797 | 54 | 2023 | \$11,811 |
| CCRO::18TH ST::240 | 18TH STREET | 47TH COURT | END | 3,819 | 53 | 2023 | \$5,343 |
| CCRO::19TH ST::20 | 19TH STREET | 61ST COURT | 61ST AVENUE | 5,140 | 26 | 2023 | \$31,639 |
| CCRO::19TH ST::270 | 19TH STREET | CICERO AVENUE | 47TH COURT | 8,880 | 26 | 2023 | \$54,659 |
| CCRO::19TH ST::290 | 19TH STREET | 47TH AVENUE | 46TH COURT | 8,970 | 25 | 2023 | \$56,684 |
| CCRO::19TH ST::80 | 19TH STREET | START | 61ST COURT | 4,091 | 26 | 2023 | \$25,185 |
| CCRO::21ST ST::130 | 21ST STREET | LARAMIE AVENUE | 51ST COURT | 8,549 | 26 | 2023 | \$52,625 |

| Pavement ID | Road Name | From | To | Area | PCI | Year | Cost |
|---------------------|-------------|----------------|------------------|--------|-----|------|-----------|
| CCRO::21ST ST::160 | 21ST STREET | 50TH COURT | 50TH AVENUE | 8,953 | 25 | 2023 | \$56,579 |
| CCRO::23RD PL::10 | 23RD PLACE | 58TH AVENUE | 57TH AVENUE | 17,343 | 23 | 2023 | \$115,287 |
| CCRO::23RD ST::180 | 23RD STREET | 49TH AVENUE | CICERO AVENUE | 17,423 | 24 | 2023 | \$112,964 |
| CCRO::23RD ST::40 | 23RD STREET | 60TH COURT | AUSTIN BOULEVARD | 8,536 | 24 | 2023 | \$55,341 |
| CCRO::24TH ST::20 | 24TH STREET | 61ST COURT | 61ST AVENUE | 8,862 | 52 | 2023 | \$12,901 |
| CCRO::25TH PL::40 | 25TH PLACE | 53RD AVENUE | LARAMIE AVENUE | 15,899 | 25 | 2023 | \$100,473 |
| CCRO::27TH ST::60 | 27TH STREET | 59TH COURT | 59TH AVENUE | 8,969 | 54 | 2023 | \$12,042 |
| CCRO::28TH ST::100 | 28TH STREET | 48TH COURT | CICERO AVENUE | 11,602 | 53 | 2023 | \$16,233 |
| CCRO::28TH ST::60 | 28TH STREET | 59TH COURT | 59TH AVENUE | 8,908 | 24 | 2023 | \$57,752 |
| CCRO::30TH PL::10 | 30TH PLACE | 55TH AVENUE | 54TH AVENUE | 17,486 | 24 | 2023 | \$113,373 |
| CCRO::33RD ST::10 | 33RD STREET | 61ST AVENUE | 60TH COURT | 9,856 | 25 | 2023 | \$62,288 |
| CCRO::33RD ST::20 | 33RD STREET | 60TH COURT | AUSTIN BOULEVARD | 10,247 | 25 | 2023 | \$64,755 |
| CCRO::33RD ST::30 | 33RD STREET | 54TH COURT | END | 5,290 | 24 | 2023 | \$34,300 |
| CCRO::33RD ST::80 | 33RD STREET | 52ND COURT | LARAMIE AVENUE | 9,291 | 25 | 2023 | \$58,717 |
| CCRO::34TH ST::10 | 34TH STREET | LOMBARD AVENUE | 61ST COURT | 8,565 | 26 | 2023 | \$52,721 |
| CCRO::34TH ST::140 | 34TH STREET | 55TH COURT | 55TH AVENUE | 8,549 | 53 | 2023 | \$11,961 |
| CCRO::34TH ST::170 | 34TH STREET | 53RD COURT | LARAMIE AVENUE | 25,994 | 23 | 2023 | \$172,791 |
| CCRO::34TH ST::30 | 34TH STREET | 61ST AVENUE | 60TH COURT | 8,506 | 26 | 2023 | \$52,359 |
| CCRO::34TH ST::40 | 34TH STREET | 60TH COURT | AUSTIN BOULEVARD | 8,820 | 24 | 2023 | \$57,184 |
| CCRO::35TH ST::30 | 35TH STREET | 61ST AVENUE | 60TH COURT | 8,459 | 26 | 2023 | \$52,072 |
| CCRO::36TH ST::10 | 36TH STREET | LOMBARD AVENUE | 61ST COURT | 8,531 | 25 | 2023 | \$53,910 |
| CCRO::36TH ST::160 | 36TH STREET | 54TH COURT | 54TH AVENUE | 8,632 | 25 | 2023 | \$54,550 |
| CCRO::37TH ST::180 | 37TH STREET | 53RD COURT | 53RD AVENUE | 8,690 | 24 | 2023 | \$56,341 |
| CCRO::37TH ST::30 | 37TH STREET | 61ST AVENUE | 60TH COURT | 8,672 | 24 | 2023 | \$56,227 |
| CCRO::37TH ST::90 | 37TH STREET | 58TH AVENUE | 57TH COURT | 8,487 | 52 | 2023 | \$12,355 |
| CCRO::38TH ST::160 | 38TH STREET | 52ND COURT | LARAMIE AVENUE | 7,850 | 25 | 2023 | \$49,605 |
| CCRO::46TH CT::10 | 46TH COURT | 19TH STREET | 16TH STREET | 34,496 | 23 | 2023 | \$229,307 |
| CCRO::47TH AVE::60 | 47TH AVENUE | 13TH STREET | 12TH PLACE | 8,797 | 24 | 2023 | \$57,037 |
| CCRO::48TH CT::110 | 48TH COURT | 16TH STREET | 15TH STREET | 17,015 | 25 | 2023 | \$107,529 |
| CCRO::48TH CT::20 | 48TH COURT | 32ND STREET | 31ST STREET | 17,256 | 53 | 2023 | \$24,144 |
| CCRO::49TH AVE::120 | 49TH AVENUE | 25TH PLACE | 25TH STREET | 8,640 | 54 | 2023 | \$11,096 |
| CCRO::49TH AVE::160 | 49TH AVENUE | 23RD PLACE | 23RD STREET | 8,619 | 25 | 2023 | \$54,466 |
| CCRO::49TH CT::80 | 49TH COURT | 13TH STREET | ROOSEVELT ROAD | 17,403 | 23 | 2023 | \$115,684 |
| CCRO::50TH AVE::100 | 50TH AVENUE | 28TH PLACE | 28TH STREET | 8,610 | 25 | 2023 | \$54,408 |
| CCRO::50TH AVE::200 | 50TH AVENUE | 22ND PLACE | 22ND STREET | 12,773 | 23 | 2023 | \$84,906 |
| CCRO::50TH CT::100 | 50TH COURT | 21ST PLACE | 21ST STREET | 10,019 | 24 | 2023 | \$64,956 |
| CCRO::50TH CT::120 | 50TH COURT | 19TH STREET | 18TH STREET | 17,472 | 25 | 2023 | \$110,414 |
| CCRO::50TH CT::130 | 50TH COURT | 18TH STREET | 16TH STREET | 17,850 | 24 | 2023 | \$115,730 |
| CCRO::50TH CT::50 | 50TH COURT | 31ST STREET | 30TH PLACE | 8,547 | 52 | 2023 | \$12,443 |
| CCRO::52ND CT::30 | 52ND COURT | 36TH STREET | 35TH STREET | 17,451 | 54 | 2023 | \$22,414 |
| CCRO::52ND CT::50 | 52ND COURT | 32ND STREET | 31ST STREET | 17,104 | 53 | 2023 | \$23,932 |
| CCRO::53RD AVE::100 | 53RD AVENUE | 29TH PLACE | OGDEN AVENUE | 6,733 | 52 | 2023 | \$9,802 |
| CCRO::53RD AVE::160 | 53RD AVENUE | 23RD PLACE | 23RD STREET | 8,643 | 25 | 2023 | \$54,620 |
| CCRO::55TH AVE::110 | 55TH AVENUE | 24TH STREET | 23RD PLACE | 8,548 | 26 | 2023 | \$52,620 |
| CCRO::55TH AVE::30 | 55TH AVENUE | 35TH STREET | 34TH STREET | 17,221 | 24 | 2023 | \$111,654 |
| CCRO::55TH AVE::80 | 55TH AVENUE | 25TH PLACE | 25TH STREET | 8,609 | 25 | 2023 | \$54,404 |
| CCRO::55TH CT::30 | 55TH COURT | 35TH STREET | 34TH STREET | 17,150 | 54 | 2023 | \$22,027 |
| CCRO::56TH CT::130 | 56TH COURT | 16TH STREET | 15TH STREET | 17,109 | 54 | 2023 | \$21,974 |
| CCRO::56TH CT::90 | 56TH COURT | 22ND STREET | 21ST STREET | 17,748 | 52 | 2023 | \$25,837 |
| CCRO::59TH AVE::120 | 59TH AVENUE | 24TH STREET | 23RD STREET | 17,361 | 23 | 2023 | \$115,407 |
| CCRO::59TH AVE::150 | 59TH AVENUE | 21ST STREET | 19TH STREET | 17,270 | 25 | 2023 | \$109,138 |
| CCRO::59TH AVE::200 | 59TH AVENUE | 14TH STREET | 13TH STREET | 17,136 | 25 | 2023 | \$108,288 |
| CCRO::59TH AVE::50 | 59TH AVENUE | 35TH STREET | 34TH STREET | 17,289 | 25 | 2023 | \$109,258 |
| CCRO::59TH AVE::60 | 59TH AVENUE | 34TH STREET | PARK AVENUE | 18,827 | 24 | 2023 | \$122,065 |
| CCRO::59TH CT::110 | 59TH COURT | 25TH STREET | 24TH STREET | 17,315 | 25 | 2023 | \$109,425 |
| CCRO::61ST AVE::180 | 61ST AVENUE | 19TH STREET | 19TH STREET | 2,984 | 54 | 2023 | \$3,832 |
| CCRO::61ST AVE::90 | 61ST AVENUE | 31ST STREET | END | 14,467 | 52 | 2023 | \$21,060 |
| CCRO::61ST CT::150 | 61ST COURT | 23RD STREET | 22ND STREET | 16,914 | 24 | 2023 | \$109,663 |
| CCRO::61ST CT::220 | 61ST COURT | 15TH STREET | 14TH STREET | 17,193 | 24 | 2023 | \$111,473 |
| CCRO::61ST CT::230 | 61ST COURT | 14TH STREET | 13TH STREET | 17,603 | 24 | 2023 | \$114,129 |
| CCRO::61ST CT::60 | 61ST COURT | 34TH STREET | OGDEN AVENUE | 18,899 | 53 | 2023 | \$26,443 |

| Pavement ID | Road Name | From | To | Area | PCI | Year | Cost |
|----------------------|----------------|------------------|------------------|--------|-----|------|-----------|
| CCRO::LMBRD AVE::160 | LOMBARD AVENUE | 26TH STREET | 25TH STREET | 17,408 | 23 | 2023 | \$115,715 |
| CCRO::LMBRD AVE::170 | LOMBARD AVENUE | 25TH STREET | 24TH STREET | 19,966 | 24 | 2023 | \$129,449 |
| CCRO::LMBRD AVE::180 | LOMBARD AVENUE | 24TH STREET | 23RD STREET | 19,966 | 23 | 2023 | \$132,722 |
| CCRO::LMBRD AVE::190 | LOMBARD AVENUE | 23RD STREET | 22ND STREET | 19,421 | 23 | 2023 | \$129,099 |
| CCRO::LMBRD AVE::210 | LOMBARD AVENUE | 21ST STREET | 19TH STREET | 17,571 | 23 | 2023 | \$116,804 |
| CCRO::LMBRD AVE::230 | LOMBARD AVENUE | 19TH STREET | 18TH STREET | 18,980 | 23 | 2023 | \$126,168 |
| CCRO::LMBRD AVE::240 | LOMBARD AVENUE | 18TH STREET | 16TH STREET | 20,209 | 23 | 2023 | \$134,340 |
| CCRO::LMBRD AVE::260 | LOMBARD AVENUE | 15TH STREET | 14TH STREET | 19,867 | 25 | 2023 | \$125,552 |
| CCRO::PRK AV::10 | PARK AVENUE | AUSTIN BOULEVARD | 59TH COURT | 8,947 | 24 | 2023 | \$58,007 |
| CCRO::PRK AV::20 | PARK AVENUE | 59TH COURT | 59TH AVENUE | 9,102 | 26 | 2023 | \$56,031 |
| CCRO::PRK AV::30 | PARK AVENUE | 59TH AVENUE | 58TH COURT | 9,222 | 25 | 2023 | \$58,277 |
| CCRO::PRK AV::50 | PARK AVENUE | 58TH AVENUE | 57TH COURT | 8,977 | 25 | 2023 | \$56,730 |
| CCRO::12TH PL::20 | 12TH PLACE | 47TH AVENUE | END | 13,419 | 10 | 2024 | \$98,176 |
| CCRO::13TH ST::140 | 13TH STREET | 51ST COURT | 51ST AVENUE | 8,848 | 20 | 2024 | \$64,736 |
| CCRO::14TH ST::150 | 14TH STREET | 51ST COURT | 51ST AVENUE | 8,836 | 52 | 2024 | \$13,644 |
| CCRO::19TH ST::10 | 19TH STREET | LOMBARD AVENUE | 61ST COURT | 4,898 | 22 | 2024 | \$34,416 |
| CCRO::19TH ST::210 | 19TH STREET | 51ST AVENUE | 50TH COURT | 8,605 | 23 | 2024 | \$59,006 |
| CCRO::19TH ST::70 | 19TH STREET | 61ST COURT | 61ST AVENUE | 5,128 | 22 | 2024 | \$36,031 |
| CCRO::21ST PL::10 | 21ST PLACE | CENTRAL AVENUE | 55TH COURT | 8,797 | 23 | 2024 | \$60,318 |
| CCRO::21ST ST::140 | 21ST STREET | 51ST COURT | 51ST AVENUE | 8,788 | 23 | 2024 | \$60,256 |
| CCRO::21ST ST::150 | 21ST STREET | 51ST AVENUE | 50TH COURT | 8,582 | 21 | 2024 | \$61,744 |
| CCRO::23RD ST::10 | 23RD STREET | LOMBARD AVENUE | 61ST COURT | 8,434 | 19 | 2024 | \$61,703 |
| CCRO::23RD ST::20 | 23RD STREET | 61ST COURT | 61ST AVENUE | 8,979 | 23 | 2024 | \$61,569 |
| CCRO::23RD ST::30 | 23RD STREET | 61ST AVENUE | 60TH COURT | 8,839 | 21 | 2024 | \$63,595 |
| CCRO::24TH PL::50 | 24TH PLACE | LARAMIE AVENUE | 51ST AVENUE | 17,429 | 52 | 2024 | \$25,972 |
| CCRO::25TH PL::10 | 25TH PLACE | CENTRAL AVENUE | 55TH AVENUE | 17,645 | 19 | 2024 | \$129,098 |
| CCRO::25TH ST::130 | 25TH STREET | CENTRAL AVENUE | 55TH AVENUE | 17,747 | 53 | 2024 | \$25,471 |
| CCRO::27TH ST::20 | 27TH STREET | 61ST COURT | 61ST AVENUE | 8,646 | 23 | 2024 | \$59,287 |
| CCRO::27TH ST::50 | 27TH STREET | AUSTIN BOULEVARD | 59TH COURT | 8,698 | 54 | 2024 | \$11,984 |
| CCRO::27TH ST::80 | 27TH STREET | 58TH COURT | 58TH AVENUE | 8,302 | 52 | 2024 | \$12,820 |
| CCRO::28TH ST::40 | 28TH STREET | 60TH COURT | AUSTIN BOULEVARD | 8,390 | 52 | 2024 | \$12,503 |
| CCRO::28TH ST::70 | 28TH STREET | 59TH AVENUE | 58TH COURT | 8,446 | 22 | 2024 | \$59,341 |
| CCRO::29TH PL::40 | 29TH PLACE | 50TH COURT | 50TH AVENUE | 11,818 | 22 | 2024 | \$83,034 |
| CCRO::32ND PL::30 | 32ND PLACE | 50TH COURT | 50TH AVENUE | 11,913 | 23 | 2024 | \$81,684 |
| CCRO::32ND ST::180 | 32ND STREET | 49TH AVENUE | 48TH COURT | 12,737 | 19 | 2024 | \$93,191 |
| CCRO::33RD ST::100 | 33RD STREET | 49TH AVENUE | 48TH COURT | 11,679 | 23 | 2024 | \$80,081 |
| CCRO::33RD ST::40 | 33RD STREET | 54TH COURT | 54TH AVENUE | 9,284 | 22 | 2024 | \$65,230 |
| CCRO::33RD ST::50 | 33RD STREET | 54TH AVENUE | 53RD COURT | 9,370 | 22 | 2024 | \$65,833 |
| CCRO::33RD ST::60 | 33RD STREET | 53RD COURT | 53RD AVENUE | 9,387 | 23 | 2024 | \$64,364 |
| CCRO::33RD ST::70 | 33RD STREET | 53RD AVENUE | 52ND COURT | 9,471 | 22 | 2024 | \$66,539 |
| CCRO::33RD ST::90 | 33RD STREET | 50TH AVENUE | 49TH AVENUE | 11,701 | 22 | 2024 | \$82,209 |
| CCRO::34TH ST::100 | 34TH STREET | 57TH COURT | 57TH AVENUE | 8,633 | 23 | 2024 | \$59,197 |
| CCRO::34TH ST::120 | 34TH STREET | 56TH COURT | CENTRAL AVENUE | 8,530 | 23 | 2024 | \$58,492 |
| CCRO::34TH ST::160 | 34TH STREET | 54TH AVENUE | 53RD COURT | 8,848 | 23 | 2024 | \$60,672 |
| CCRO::34TH ST::20 | 34TH STREET | 61ST COURT | 61ST AVENUE | 8,653 | 23 | 2024 | \$59,334 |
| CCRO::34TH ST::80 | 34TH STREET | 58TH COURT | 58TH AVENUE | 8,642 | 20 | 2024 | \$63,224 |
| CCRO::34TH ST::90 | 34TH STREET | 58TH AVENUE | 57TH COURT | 8,632 | 20 | 2024 | \$63,151 |
| CCRO::36TH ST::40 | 36TH STREET | 60TH COURT | AUSTIN BOULEVARD | 9,161 | 23 | 2024 | \$62,816 |
| CCRO::37TH ST::10 | 37TH STREET | LOMBARD AVENUE | 61ST COURT | 8,516 | 23 | 2024 | \$58,395 |
| CCRO::37TH ST::40 | 37TH STREET | 60TH COURT | AUSTIN BOULEVARD | 8,840 | 23 | 2024 | \$60,617 |
| CCRO::38TH ST::130 | 38TH STREET | 54TH AVENUE | 53RD COURT | 8,163 | 20 | 2024 | \$59,725 |
| CCRO::38TH ST::140 | 38TH STREET | 53RD COURT | 53RD AVENUE | 8,859 | 22 | 2024 | \$62,242 |
| CCRO::38TH ST::150 | 38TH STREET | 53RD AVENUE | 52ND COURT | 8,697 | 23 | 2024 | \$59,632 |
| CCRO::38TH ST::60 | 38TH STREET | 59TH COURT | 59TH AVENUE | 9,018 | 54 | 2024 | \$12,425 |
| CCRO::48TH CT::70 | 48TH COURT | 21ST PLACE | 21ST STREET | 9,727 | 22 | 2024 | \$68,338 |
| CCRO::48TH CT::80 | 48TH COURT | 21ST STREET | 19TH STREET | 17,295 | 16 | 2024 | \$126,534 |
| CCRO::48TH CT::90 | 48TH COURT | 19TH STREET | 18TH STREET | 17,253 | 20 | 2024 | \$126,225 |
| CCRO::49TH AVE::170 | 49TH AVENUE | 23RD STREET | 22ND PLACE | 8,876 | 23 | 2024 | \$60,865 |
| CCRO::49TH AVE::240 | 49TH AVENUE | 16TH STREET | 15TH STREET | 16,887 | 52 | 2024 | \$25,164 |
| CCRO::49TH AVE::260 | 49TH AVENUE | 14TH STREET | 13TH STREET | 17,305 | 23 | 2024 | \$118,662 |
| CCRO::49TH AVE::270 | 49TH AVENUE | 13TH STREET | ROOSEVELT ROAD | 17,404 | 22 | 2024 | \$122,275 |

| Pavement ID | Road Name | From | To | Area | PCI | Year | Cost |
|----------------------|----------------|-------------|--------------|--------|-----|------|-----------|
| CCRO::50TH AVE::190 | 50TH AVENUE | 23RD STREET | 22ND PLACE | 12,750 | 23 | 2024 | \$87,429 |
| CCRO::50TH AVE::220 | 50TH AVENUE | 21ST PLACE | 21ST STREET | 15,007 | 23 | 2024 | \$102,902 |
| CCRO::50TH AVE::60 | 50TH AVENUE | 30TH PLACE | 30TH STREET | 8,661 | 23 | 2024 | \$59,391 |
| CCRO::50TH AVE::90 | 50TH AVENUE | 29TH STREET | 28TH PLACE | 8,614 | 23 | 2024 | \$59,066 |
| CCRO::50TH CT::60 | 50TH COURT | 30TH PLACE | 30TH STREET | 8,694 | 53 | 2024 | \$12,478 |
| CCRO::51ST AVE::140 | 51ST AVENUE | 24TH STREET | 23RD PLACE | 8,635 | 53 | 2024 | \$12,393 |
| CCRO::51ST AVE::190 | 51ST AVENUE | 21ST PLACE | 21ST STREET | 10,003 | 52 | 2024 | \$15,447 |
| CCRO::52ND CT::20 | 52ND COURT | 37TH STREET | 36TH STREET | 17,250 | 52 | 2024 | \$26,639 |
| CCRO::53RD AVE::120 | 53RD AVENUE | 25TH PLACE | 25TH STREET | 8,838 | 19 | 2024 | \$64,658 |
| CCRO::53RD AVE::90 | 53RD AVENUE | 30TH STREET | 29TH PLACE | 8,907 | 52 | 2024 | \$13,273 |
| CCRO::53RD CT::10 | 53RD COURT | 38TH STREET | 37TH STREET | 17,349 | 54 | 2024 | \$23,903 |
| CCRO::53RD CT::40 | 53RD COURT | 35TH STREET | 34TH STREET | 17,247 | 52 | 2024 | \$25,700 |
| CCRO::54TH AVE::40 | 54TH AVENUE | 35TH STREET | 34TH STREET | 17,156 | 54 | 2024 | \$22,651 |
| CCRO::55TH AVE::60 | 55TH AVENUE | 30TH PLACE | OGDEN AVENUE | 8,712 | 21 | 2024 | \$62,682 |
| CCRO::55TH CT::90 | 55TH COURT | 14TH STREET | 13TH STREET | 25,473 | 52 | 2024 | \$37,959 |
| CCRO::57TH AVE::180 | 57TH AVENUE | 15TH STREET | 14TH STREET | 17,032 | 21 | 2024 | \$122,540 |
| CCRO::58TH AVE::80 | 58TH AVENUE | 27TH STREET | 26TH STREET | 17,313 | 54 | 2024 | \$23,854 |
| CCRO::58TH CT::50 | 58TH COURT | 35TH STREET | 34TH STREET | 17,232 | 54 | 2024 | \$22,752 |
| CCRO::58TH CT::80 | 58TH COURT | 27TH STREET | 26TH STREET | 17,130 | 52 | 2024 | \$25,527 |
| CCRO::59TH AVE::100 | 59TH AVENUE | 27TH STREET | 26TH STREET | 17,098 | 23 | 2024 | \$117,239 |
| CCRO::59TH AVE::110 | 59TH AVENUE | 26TH STREET | 25TH STREET | 17,415 | 22 | 2024 | \$122,353 |
| CCRO::59TH AVE::180 | 59TH AVENUE | 16TH STREET | 15TH STREET | 17,199 | 20 | 2024 | \$125,836 |
| CCRO::59TH AVE::190 | 59TH AVENUE | 15TH STREET | 14TH STREET | 17,489 | 17 | 2024 | \$127,958 |
| CCRO::59TH CT::120 | 59TH COURT | 24TH STREET | 23RD STREET | 17,300 | 52 | 2024 | \$26,715 |
| CCRO::59TH CT::150 | 59TH COURT | 21ST STREET | 19TH STREET | 17,310 | 54 | 2024 | \$22,855 |
| CCRO::60TH CT::10 | 60TH COURT | 39TH STREET | 38TH STREET | 17,443 | 54 | 2024 | \$23,030 |
| CCRO::60TH CT::170 | 60TH COURT | 19TH STREET | 19TH STREET | 3,668 | 52 | 2024 | \$5,663 |
| CCRO::60TH CT::200 | 60TH COURT | 16TH STREET | 15TH STREET | 17,199 | 53 | 2024 | \$24,684 |
| CCRO::60TH CT::30 | 60TH COURT | 36TH STREET | 35TH STREET | 17,324 | 23 | 2024 | \$118,788 |
| CCRO::61ST AVE::110 | 61ST AVENUE | 27TH STREET | 26TH STREET | 17,220 | 23 | 2024 | \$118,075 |
| CCRO::61ST CT::140 | 61ST COURT | 24TH STREET | 23RD STREET | 17,292 | 23 | 2024 | \$118,573 |
| CCRO::CNTRL AVE::140 | CENTRAL AVENUE | 24TH PLACE | 24TH STREET | 15,147 | 53 | 2024 | \$22,315 |
| CCRO::CNTRL AVE::50 | CENTRAL AVENUE | 36TH STREET | 35TH STREET | 31,206 | 53 | 2024 | \$45,974 |
| CCRO::LMBRD AVE::140 | LOMBARD AVENUE | 28TH STREET | 27TH STREET | 22,022 | 22 | 2024 | \$154,725 |
| CCRO::LMBRD AVE::150 | LOMBARD AVENUE | 27TH STREET | 26TH STREET | 17,333 | 23 | 2024 | \$118,851 |
| CCRO::LMBRD AVE::200 | LOMBARD AVENUE | 22ND STREET | 21ST STREET | 20,446 | 22 | 2024 | \$143,653 |
| CCRO::LMBRD AVE::250 | LOMBARD AVENUE | 16TH STREET | 15TH STREET | 19,821 | 22 | 2024 | \$139,261 |
| CCRO::LMBRD AVE::270 | LOMBARD AVENUE | 14TH STREET | 13TH STREET | 20,174 | 22 | 2024 | \$141,736 |
| CCRO::PRK AV::60 | PARK AVENUE | 57TH COURT | 57TH AVENUE | 8,746 | 23 | 2024 | \$59,973 |

APPENDIX D – PAVEMENT MAINTENANCE POLICIES AND UNIT COSTS

Table D-1. Recommended Asphalt Pavement Maintenance Policy.

| Pavement Distress | Severity | Recommended Maintenance Type | Units |
|---------------------------|----------|------------------------------|-------|
| Alligator Cracking | Low | Crack Sealing | FT |
| Alligator Cracking | Medium | Patching - AC Deep | SF |
| Alligator Cracking | High | Patching - AC Deep | SF |
| Block Cracking | Low | Crack Sealing - AC | FT |
| Block Cracking | Medium | Crack Sealing - AC | FT |
| Block Cracking | High | Patching - AC Shallow | SF |
| Bumps and Sags | Medium | Patching - AC Shallow | SF |
| Bumps and Sags | High | Patching - AC Deep | SF |
| Corrugation | Medium | Patching - AC Shallow | SF |
| Corrugation | High | Patching - AC Deep | SF |
| Depressions | Medium | Patching - AC Deep | SF |
| Depressions | High | Patching - AC Deep | SF |
| Edge Cracking | Low | Crack Sealing - AC | FT |
| Edge Cracking | Medium | Crack Sealing - AC | FT |
| Edge Cracking | High | Patching - AC Shallow | SF |
| Joint Reflection Cracking | Low | Crack Sealing - AC | FT |
| Joint Reflection Cracking | Medium | Crack Sealing - AC | FT |
| Joint Reflection Cracking | High | Patching - AC Shallow | SF |
| Lane/Shoulder Dropoff | Medium | Shoulder leveling | FT |
| Lane/Shoulder Dropoff | High | Shoulder leveling | FT |
| Long. and Trans. Cracking | Low | Crack Sealing - AC | FT |
| Long. and Trans. Cracking | Medium | Crack Sealing - AC | FT |
| Long. and Trans. Cracking | High | Patching - AC Shallow | SF |
| Patching and Utility Cuts | High | Patching - AC Deep | SF |
| Potholes | Low | Patching - AC Deep | SF |
| Potholes | Medium | Patching - AC Deep | SF |
| Potholes | High | Patching - AC Deep | SF |
| Rutting | Medium | Patching - AC Shallow | SF |
| Rutting | High | Patching - AC Deep | SF |
| Shoving | Medium | Grinding (Localized) | FT |
| Shoving | High | Grinding (Localized) | FT |
| Slippage Cracking | Low | Crack Sealing - AC | FT |
| Slippage Cracking | Medium | Patching - AC Shallow | SF |
| Slippage Cracking | High | Patching - AC Shallow | SF |

Table D-2. Recommended Concrete Pavement Maintenance Policy.

| Pavement Distress | Severity | Recommended Maintenance Type | Units |
|---------------------------|----------|------------------------------|-------|
| Blow ups | Medium | Patching - PCC Full Depth | SF |
| Blow ups | High | Patching - PCC Full Depth | SF |
| Corner Breaks | Low | Crack Sealing - PCC | FT |
| Corner Breaks | Medium | Patching - PCC Full Depth | FT |
| Corner Breaks | High | Patching - PCC Full Depth | SF |
| Divided (Shattered) Slabs | Low | Crack Sealing - PCC | FT |
| Divided (Shattered) Slabs | Medium | Slab Replacement - PCC | SF |
| Divided (Shattered) Slabs | High | Slab Replacement - PCC | SF |
| Durability (D) Cracking | Medium | Patching - PCC Full Depth | SF |
| Durability (D) Cracking | High | Slab Replacement - PCC | SF |
| Faulting | Medium | Grinding (Localized) | FT |
| Faulting | High | Grinding (Localized) | FT |
| Joint Seal Damage | Medium | Joint Seal (Localized) | FT |
| Joint Seal Damage | High | Joint Seal (Localized) | FT |
| Lane/Shoulder Dropoff | Medium | Shoulder leveling | FT |
| Lane/Shoulder Dropoff | High | Shoulder leveling | FT |
| Linear Cracking | Low | Crack Sealing - PCC | FT |
| Linear Cracking | Medium | Crack Sealing - PCC | FT |
| Linear Cracking | High | Patching - PCC Partial Depth | SF |
| Patches, Large | High | Patching - PCC Full Depth | SF |
| Patches, Small | High | Patching - PCC Partial Depth | SF |
| Punchouts | Medium | Patching - PCC Full Depth | SF |
| Punchouts | High | Slab Replacement - PCC | SF |
| Sealing | High | Slab Replacement - PCC | SF |
| Corner Spalls | Medium | Patching - PCC Partial Depth | SF |
| Corner Spalls | High | Patching - PCC Partial Depth | SF |
| Joint Spalls | Medium | Patching - PCC Partial Depth | SF |
| Joint Spalls | High | Patching - PCC Partial Depth | SF |

Table D-3. Estimate Unit Cost for Maintenance Activities.

| Maintenance Type | Est. Unit Cost | Units |
|------------------------------|----------------|-------|
| Crack Sealing - AC | \$1.00 | FT |
| Joint Seal - Silicon | \$2.75 | FT |
| Crack Sealing - PCC | \$1.50 | FT |
| Grinding (Localized) | \$4.00 | FT |
| Joint Seal (Localized) | \$1.50 | FT |
| Patching - AC Deep | \$11.00 | SF |
| Patching - AC Leveling | \$1.20 | SF |
| Patching - AC Shallow | \$5.50 | SF |
| Patching - PCC Full Depth | \$30.00 | SF |
| Patching - PCC Partial Depth | \$7.00 | SF |
| Shoulder leveling | \$1.20 | FT |
| Slab Replacement - PCC | \$20.00 | SF |

APPENDIX E – TABULATED PREVENTIVE MAINTENANCE RECOMMENDATIONS

| Pavement ID | Road Name | From | To | Area | Distress Type | Density | Maint. Activity | Cost |
|--------------------|-------------|------------------|----------------|--------|---------------|---------|------------------------------|----------|
| CCRO::13TH ST::120 | 13TH STREET | 56TH COURT | CENTRAL AVENUE | 8,529 | L & T CR | 0.7% | Crack Sealing - AC | \$59 |
| CCRO::13TH ST::120 | 13TH STREET | 56TH COURT | CENTRAL AVENUE | 8,529 | ALLIGATOR CR | 0.2% | Crack Sealing - AC | \$9 |
| CCRO::13TH ST::120 | 13TH STREET | 56TH COURT | CENTRAL AVENUE | 8,529 | L & T CR | 2.1% | Crack Sealing - AC | \$176 |
| CCRO::13TH ST::120 | 13TH STREET | 56TH COURT | CENTRAL AVENUE | 8,529 | RUTTING | 0.1% | Patching - AC Shallow | \$25 |
| CCRO::13TH ST::30 | 13TH STREET | 61ST AVENUE | 60TH COURT | 8,420 | ALLIGATOR CR | 0.6% | Crack Sealing - AC | \$26 |
| CCRO::13TH ST::30 | 13TH STREET | 61ST AVENUE | 60TH COURT | 8,420 | L & T CR | 0.9% | Crack Sealing - AC | \$72 |
| CCRO::13TH ST::30 | 13TH STREET | 61ST AVENUE | 60TH COURT | 8,420 | L & T CR | 0.4% | Crack Sealing - AC | \$36 |
| CCRO::14TH ST::150 | 14TH STREET | 51ST COURT | 51ST AVENUE | 8,836 | L & T CR | 3.8% | Crack Sealing - AC | \$335 |
| CCRO::14TH ST::150 | 14TH STREET | 51ST COURT | 51ST AVENUE | 8,836 | L & T CR | 3.0% | Crack Sealing - AC | \$269 |
| CCRO::14TH ST::150 | 14TH STREET | 51ST COURT | 51ST AVENUE | 8,836 | ALLIGATOR CR | 1.6% | Crack Sealing - AC | \$58 |
| CCRO::14TH ST::50 | 14TH STREET | AUSTIN BOULEVARD | 59TH COURT | 8,736 | ALLIGATOR CR | 1.5% | Crack Sealing - AC | \$55 |
| CCRO::14TH ST::50 | 14TH STREET | AUSTIN BOULEVARD | 59TH COURT | 8,736 | L & T CR | 0.4% | Crack Sealing - AC | \$36 |
| CCRO::14TH ST::50 | 14TH STREET | AUSTIN BOULEVARD | 59TH COURT | 8,736 | L & T CR | 1.8% | Crack Sealing - AC | \$160 |
| CCRO::14TH ST::50 | 14TH STREET | AUSTIN BOULEVARD | 59TH COURT | 8,736 | RUTTING | 0.0% | Patching - AC Shallow | \$20 |
| CCRO::15TH PL::10 | 15TH PLACE | CENTRAL AVENUE | 55TH COURT | 8,859 | LINEAR CR | 33.3% | Crack Sealing - PCC | \$356 |
| CCRO::15TH PL::10 | 15TH PLACE | CENTRAL AVENUE | 55TH COURT | 8,859 | JT SEAL DMG | 100.0% | Joint Seal (Localized) | \$1,576 |
| CCRO::15TH PL::10 | 15TH PLACE | CENTRAL AVENUE | 55TH COURT | 8,859 | LARGE PATCH | 13.3% | Patching - PCC Full Depth | \$14,026 |
| CCRO::15TH PL::10 | 15TH PLACE | CENTRAL AVENUE | 55TH COURT | 8,859 | JOINT SPALL | 6.7% | Patching - PCC Partial Depth | \$573 |
| CCRO::15TH ST::130 | 15TH STREET | LARAMIE AVENUE | 51ST COURT | 8,547 | L & T CR | 0.6% | Crack Sealing - AC | \$54 |
| CCRO::15TH ST::130 | 15TH STREET | LARAMIE AVENUE | 51ST COURT | 8,547 | L & T CR | 2.5% | Crack Sealing - AC | \$215 |
| CCRO::15TH ST::130 | 15TH STREET | LARAMIE AVENUE | 51ST COURT | 8,547 | ALLIGATOR CR | 2.4% | Crack Sealing - AC | \$80 |
| CCRO::15TH ST::130 | 15TH STREET | LARAMIE AVENUE | 51ST COURT | 8,547 | ALLIGATOR CR | 0.6% | Patching - AC Deep | \$957 |
| CCRO::16TH ST::100 | 16TH STREET | 57TH COURT | 57TH AVENUE | 13,173 | L & T CR | 0.4% | Crack Sealing - AC | \$55 |
| CCRO::16TH ST::100 | 16TH STREET | 57TH COURT | 57TH AVENUE | 13,173 | L & T CR | 2.7% | Crack Sealing - AC | \$356 |
| CCRO::16TH ST::100 | 16TH STREET | 57TH COURT | 57TH AVENUE | 13,173 | ALLIGATOR CR | 0.2% | Crack Sealing - AC | \$16 |
| CCRO::16TH ST::110 | 16TH STREET | 57TH AVENUE | 56TH COURT | 13,605 | ALLIGATOR CR | 1.8% | Crack Sealing - AC | \$95 |
| CCRO::16TH ST::110 | 16TH STREET | 57TH AVENUE | 56TH COURT | 13,605 | L & T CR | 0.8% | Crack Sealing - AC | \$111 |
| CCRO::16TH ST::110 | 16TH STREET | 57TH AVENUE | 56TH COURT | 13,605 | L & T CR | 1.2% | Crack Sealing - AC | \$163 |
| CCRO::16TH ST::120 | 16TH STREET | 56TH COURT | CENTRAL AVENUE | 13,074 | L & T CR | 1.7% | Crack Sealing - AC | \$218 |
| CCRO::16TH ST::120 | 16TH STREET | 56TH COURT | CENTRAL AVENUE | 13,074 | ALLIGATOR CR | 0.4% | Crack Sealing - AC | \$26 |
| CCRO::16TH ST::120 | 16TH STREET | 56TH COURT | CENTRAL AVENUE | 13,074 | L & T CR | 1.7% | Crack Sealing - AC | \$221 |
| CCRO::16TH ST::120 | 16TH STREET | 56TH COURT | CENTRAL AVENUE | 13,074 | ALLIGATOR CR | 0.1% | Patching - AC Deep | \$366 |
| CCRO::16TH ST::120 | 16TH STREET | 56TH COURT | CENTRAL AVENUE | 13,074 | RUTTING | 0.7% | Patching - AC Shallow | \$467 |
| CCRO::16TH ST::20 | 16TH STREET | 61ST COURT | 61ST AVENUE | 13,384 | ALLIGATOR CR | 1.7% | Crack Sealing - AC | \$90 |
| CCRO::16TH ST::20 | 16TH STREET | 61ST COURT | 61ST AVENUE | 13,384 | L & T CR | 1.7% | Crack Sealing - AC | \$222 |
| CCRO::16TH ST::20 | 16TH STREET | 61ST COURT | 61ST AVENUE | 13,384 | L & T CR | 0.2% | Crack Sealing - AC | \$28 |
| CCRO::16TH ST::20 | 16TH STREET | 61ST COURT | 61ST AVENUE | 13,384 | ALLIGATOR CR | 1.2% | Patching - AC Deep | \$2,281 |
| CCRO::16TH ST::30 | 16TH STREET | 61ST AVENUE | 60TH COURT | 13,175 | ALLIGATOR CR | 1.7% | Crack Sealing - AC | \$86 |
| CCRO::16TH ST::30 | 16TH STREET | 61ST AVENUE | 60TH COURT | 13,175 | L & T CR | 4.4% | Crack Sealing - AC | \$574 |
| CCRO::16TH ST::30 | 16TH STREET | 61ST AVENUE | 60TH COURT | 13,175 | L & T CR | 0.4% | Crack Sealing - AC | \$55 |

| Pavement ID | Road Name | From | To | Area | Distress Type | Density | Maint. Activity | Cost |
|--------------------|-------------|------------------|------------------|--------|---------------|---------|-----------------------|---------|
| CCRO::16TH ST::40 | 16TH STREET | 60TH COURT | AUSTIN BOULEVARD | 13,483 | L & T CR | 0.2% | Crack Sealing - AC | \$31 |
| CCRO::16TH ST::40 | 16TH STREET | 60TH COURT | AUSTIN BOULEVARD | 13,483 | ALLIGATOR CR | 1.1% | Crack Sealing - AC | \$60 |
| CCRO::16TH ST::40 | 16TH STREET | 60TH COURT | AUSTIN BOULEVARD | 13,483 | L & T CR | 2.5% | Crack Sealing - AC | \$333 |
| CCRO::16TH ST::50 | 16TH STREET | AUSTIN BOULEVARD | 59TH COURT | 13,302 | L & T CR | 0.4% | Crack Sealing - AC | \$46 |
| CCRO::16TH ST::50 | 16TH STREET | AUSTIN BOULEVARD | 59TH COURT | 13,302 | ALLIGATOR CR | 0.4% | Crack Sealing - AC | \$26 |
| CCRO::16TH ST::50 | 16TH STREET | AUSTIN BOULEVARD | 59TH COURT | 13,302 | L & T CR | 2.4% | Crack Sealing - AC | \$317 |
| CCRO::16TH ST::60 | 16TH STREET | 59TH COURT | 59TH AVENUE | 13,180 | L & T CR | 0.6% | Crack Sealing - AC | \$83 |
| CCRO::16TH ST::60 | 16TH STREET | 59TH COURT | 59TH AVENUE | 13,180 | ALLIGATOR CR | 1.0% | Crack Sealing - AC | \$54 |
| CCRO::16TH ST::60 | 16TH STREET | 59TH COURT | 59TH AVENUE | 13,180 | L & T CR | 1.7% | Crack Sealing - AC | \$219 |
| CCRO::16TH ST::70 | 16TH STREET | 59TH AVENUE | 58TH COURT | 13,463 | L & T CR | 3.3% | Crack Sealing - AC | \$438 |
| CCRO::16TH ST::70 | 16TH STREET | 59TH AVENUE | 58TH COURT | 13,463 | L & T CR | 0.2% | Crack Sealing - AC | \$28 |
| CCRO::16TH ST::80 | 16TH STREET | 58TH COURT | 58TH AVENUE | 13,328 | ALLIGATOR CR | 0.3% | Crack Sealing - AC | \$22 |
| CCRO::16TH ST::80 | 16TH STREET | 58TH COURT | 58TH AVENUE | 13,328 | L & T CR | 3.7% | Crack Sealing - AC | \$490 |
| CCRO::16TH ST::80 | 16TH STREET | 58TH COURT | 58TH AVENUE | 13,328 | L & T CR | 0.4% | Crack Sealing - AC | \$55 |
| CCRO::16TH ST::90 | 16TH STREET | 58TH AVENUE | 57TH COURT | 13,625 | L & T CR | 0.8% | Crack Sealing - AC | \$111 |
| CCRO::16TH ST::90 | 16TH STREET | 58TH AVENUE | 57TH COURT | 13,625 | L & T CR | 1.0% | Crack Sealing - AC | \$137 |
| CCRO::16TH ST::90 | 16TH STREET | 58TH AVENUE | 57TH COURT | 13,625 | ALLIGATOR CR | 0.2% | Crack Sealing - AC | \$17 |
| CCRO::18TH ST::170 | 18TH STREET | 51ST AVENUE | 50TH COURT | 8,631 | L & T CR | 3.7% | Crack Sealing - AC | \$321 |
| CCRO::18TH ST::170 | 18TH STREET | 51ST AVENUE | 50TH COURT | 8,631 | ALLIGATOR CR | 1.3% | Crack Sealing - AC | \$48 |
| CCRO::18TH ST::170 | 18TH STREET | 51ST AVENUE | 50TH COURT | 8,631 | L & T CR | 2.1% | Crack Sealing - AC | \$178 |
| CCRO::18TH ST::170 | 18TH STREET | 51ST AVENUE | 50TH COURT | 8,631 | ALLIGATOR CR | 0.8% | Patching - AC Deep | \$1,124 |
| CCRO::18TH ST::190 | 18TH STREET | 50TH AVENUE | 49TH COURT | 8,719 | L & T CR | 1.7% | Crack Sealing - AC | \$144 |
| CCRO::18TH ST::190 | 18TH STREET | 50TH AVENUE | 49TH COURT | 8,719 | L & T CR | 1.0% | Crack Sealing - AC | \$89 |
| CCRO::18TH ST::190 | 18TH STREET | 50TH AVENUE | 49TH COURT | 8,719 | ALLIGATOR CR | 0.5% | Crack Sealing - AC | \$22 |
| CCRO::18TH ST::190 | 18TH STREET | 50TH AVENUE | 49TH COURT | 8,719 | BLOCK CR | 30.7% | Crack Sealing - AC | \$816 |
| CCRO::18TH ST::190 | 18TH STREET | 50TH AVENUE | 49TH COURT | 8,719 | ALLIGATOR CR | 0.5% | Patching - AC Deep | \$871 |
| CCRO::18TH ST::20 | 18TH STREET | 61ST COURT | 61ST AVENUE | 8,797 | L & T CR | 0.8% | Crack Sealing - AC | \$71 |
| CCRO::18TH ST::20 | 18TH STREET | 61ST COURT | 61ST AVENUE | 8,797 | L & T CR | 3.9% | Crack Sealing - AC | \$338 |
| CCRO::18TH ST::20 | 18TH STREET | 61ST COURT | 61ST AVENUE | 8,797 | ALLIGATOR CR | 0.6% | Crack Sealing - AC | \$27 |
| CCRO::18TH ST::20 | 18TH STREET | 61ST COURT | 61ST AVENUE | 8,797 | ALLIGATOR CR | 0.9% | Patching - AC Deep | \$1,294 |
| CCRO::18TH ST::20 | 18TH STREET | 61ST COURT | 61ST AVENUE | 8,797 | RUTTING | 0.1% | Patching - AC Shallow | \$44 |
| CCRO::18TH ST::240 | 18TH STREET | 47TH COURT | END | 3,819 | BLOCK CR | 12.2% | Crack Sealing - AC | \$142 |
| CCRO::18TH ST::240 | 18TH STREET | 47TH COURT | END | 3,819 | L & T CR | 2.1% | Crack Sealing - AC | \$79 |
| CCRO::18TH ST::240 | 18TH STREET | 47TH COURT | END | 3,819 | ALLIGATOR CR | 0.9% | Patching - AC Deep | \$679 |
| CCRO::18TH ST::50 | 18TH STREET | AUSTIN BOULEVARD | 59TH COURT | 8,805 | ALLIGATOR CR | 0.6% | Crack Sealing - AC | \$25 |
| CCRO::18TH ST::50 | 18TH STREET | AUSTIN BOULEVARD | 59TH COURT | 8,805 | L & T CR | 0.4% | Crack Sealing - AC | \$39 |
| CCRO::19TH ST::130 | 19TH STREET | 58TH AVENUE | 57TH COURT | 8,743 | L & T CR | 2.0% | Crack Sealing - AC | \$176 |
| CCRO::19TH ST::130 | 19TH STREET | 58TH AVENUE | 57TH COURT | 8,743 | L & T CR | 3.5% | Crack Sealing - AC | \$303 |
| CCRO::19TH ST::130 | 19TH STREET | 58TH AVENUE | 57TH COURT | 8,743 | ALLIGATOR CR | 1.7% | Crack Sealing - AC | \$61 |
| CCRO::19TH ST::130 | 19TH STREET | 58TH AVENUE | 57TH COURT | 8,743 | ALLIGATOR CR | 1.5% | Patching - AC Deep | \$1,970 |

| Pavement ID | Road Name | From | To | Area | Distress Type | Density | Maint. Activity | Cost |
|--------------------|-------------|----------------|------------------|--------|---------------|---------|-----------------------|---------|
| CCRO::19TH ST::170 | 19TH STREET | CENTRAL AVENUE | 55TH COURT | 8,752 | L & T CR | 0.9% | Crack Sealing - AC | \$80 |
| CCRO::19TH ST::170 | 19TH STREET | CENTRAL AVENUE | 55TH COURT | 8,752 | L & T CR | 0.2% | Crack Sealing - AC | \$20 |
| CCRO::19TH ST::170 | 19TH STREET | CENTRAL AVENUE | 55TH COURT | 8,752 | ALLIGATOR CR | 3.8% | Patching - AC Deep | \$4,515 |
| CCRO::19TH ST::170 | 19TH STREET | CENTRAL AVENUE | 55TH COURT | 8,752 | RUTTING | 0.0% | Patching - AC Shallow | \$18 |
| CCRO::19TH ST::180 | 19TH STREET | 55TH COURT | 55TH AVENUE | 8,634 | ALLIGATOR CR | 0.2% | Crack Sealing - AC | \$10 |
| CCRO::19TH ST::180 | 19TH STREET | 55TH COURT | 55TH AVENUE | 8,634 | L & T CR | 3.5% | Crack Sealing - AC | \$301 |
| CCRO::19TH ST::180 | 19TH STREET | 55TH COURT | 55TH AVENUE | 8,634 | L & T CR | 1.7% | Crack Sealing - AC | \$143 |
| CCRO::19TH ST::180 | 19TH STREET | 55TH COURT | 55TH AVENUE | 8,634 | ALLIGATOR CR | 0.4% | Patching - AC Deep | \$664 |
| CCRO::19TH ST::50 | 19TH STREET | 60TH COURT | AUSTIN BOULEVARD | 4,924 | ALLIGATOR CR | 0.7% | Crack Sealing - AC | \$19 |
| CCRO::19TH ST::50 | 19TH STREET | 60TH COURT | AUSTIN BOULEVARD | 4,924 | L & T CR | 0.8% | Crack Sealing - AC | \$42 |
| CCRO::19TH ST::50 | 19TH STREET | 60TH COURT | AUSTIN BOULEVARD | 4,924 | EDGE CR | 1.1% | Crack Sealing - AC | \$53 |
| CCRO::19TH ST::50 | 19TH STREET | 60TH COURT | AUSTIN BOULEVARD | 4,924 | L & T CR | 0.2% | Crack Sealing - AC | \$10 |
| CCRO::19TH ST::50 | 19TH STREET | 60TH COURT | AUSTIN BOULEVARD | 4,924 | ALLIGATOR CR | 0.9% | Patching - AC Deep | \$830 |
| CCRO::19TH ST::50 | 19TH STREET | 60TH COURT | AUSTIN BOULEVARD | 4,924 | RUTTING | 0.3% | Patching - AC Shallow | \$71 |
| CCRO::21ST PL::70 | 21ST PLACE | 49TH COURT | 49TH AVENUE | 8,768 | RUTTING | 0.0% | Patching - AC Shallow | \$15 |
| CCRO::21ST PL::80 | 21ST PLACE | 49TH AVENUE | 48TH COURT | 8,779 | ALLIGATOR CR | 1.0% | Crack Sealing - AC | \$40 |
| CCRO::21ST PL::90 | 21ST PLACE | 48TH COURT | CICERO AVENUE | 8,570 | L & T CR | 0.2% | Crack Sealing - AC | \$18 |
| CCRO::21ST PL::90 | 21ST PLACE | 48TH COURT | CICERO AVENUE | 8,570 | ALLIGATOR CR | 0.8% | Crack Sealing - AC | \$31 |
| CCRO::22ND PL::90 | 22ND PLACE | 49TH AVENUE | CICERO AVENUE | 17,238 | L & T CR | 0.7% | Crack Sealing - AC | \$116 |
| CCRO::24TH PL::10 | 24TH PLACE | CENTRAL AVENUE | 55TH AVENUE | 17,555 | L & T CR | 0.5% | Crack Sealing - AC | \$80 |
| CCRO::24TH PL::10 | 24TH PLACE | CENTRAL AVENUE | 55TH AVENUE | 17,555 | ALLIGATOR CR | 1.3% | Crack Sealing - AC | \$92 |
| CCRO::24TH PL::10 | 24TH PLACE | CENTRAL AVENUE | 55TH AVENUE | 17,555 | ALLIGATOR CR | 0.1% | Patching - AC Deep | \$336 |
| CCRO::24TH PL::20 | 24TH PLACE | 55TH AVENUE | 54TH AVENUE | 17,453 | L & T CR | 0.2% | Crack Sealing - AC | \$39 |
| CCRO::24TH PL::20 | 24TH PLACE | 55TH AVENUE | 54TH AVENUE | 17,453 | ALLIGATOR CR | 0.2% | Crack Sealing - AC | \$19 |
| CCRO::24TH PL::20 | 24TH PLACE | 55TH AVENUE | 54TH AVENUE | 17,453 | L & T CR | 0.2% | Crack Sealing - AC | \$40 |
| CCRO::24TH PL::20 | 24TH PLACE | 55TH AVENUE | 54TH AVENUE | 17,453 | ALLIGATOR CR | 0.5% | Patching - AC Deep | \$1,403 |
| CCRO::24TH PL::20 | 24TH PLACE | 55TH AVENUE | 54TH AVENUE | 17,453 | BLOCK CR | 1.8% | Patching - AC Shallow | \$1,696 |
| CCRO::24TH PL::30 | 24TH PLACE | 54TH AVENUE | 53RD AVENUE | 17,507 | ALLIGATOR CR | 0.3% | Crack Sealing - AC | \$23 |
| CCRO::24TH PL::30 | 24TH PLACE | 54TH AVENUE | 53RD AVENUE | 17,507 | L & T CR | 0.9% | Crack Sealing - AC | \$157 |
| CCRO::24TH PL::30 | 24TH PLACE | 54TH AVENUE | 53RD AVENUE | 17,507 | L & T CR | 1.4% | Crack Sealing - AC | \$236 |
| CCRO::24TH PL::40 | 24TH PLACE | 53RD AVENUE | LARAMIE AVENUE | 17,209 | L & T CR | 0.2% | Crack Sealing - AC | \$40 |
| CCRO::24TH PL::40 | 24TH PLACE | 53RD AVENUE | LARAMIE AVENUE | 17,209 | ALLIGATOR CR | 0.1% | Crack Sealing - AC | \$8 |
| CCRO::24TH PL::40 | 24TH PLACE | 53RD AVENUE | LARAMIE AVENUE | 17,209 | L & T CR | 1.0% | Crack Sealing - AC | \$179 |
| CCRO::24TH PL::40 | 24TH PLACE | 53RD AVENUE | LARAMIE AVENUE | 17,209 | ALLIGATOR CR | 0.3% | Patching - AC Deep | \$1,037 |
| CCRO::24TH PL::50 | 24TH PLACE | LARAMIE AVENUE | 51ST AVENUE | 17,429 | L & T CR | 2.0% | Crack Sealing - AC | \$340 |
| CCRO::24TH PL::50 | 24TH PLACE | LARAMIE AVENUE | 51ST AVENUE | 17,429 | L & T CR | 2.6% | Crack Sealing - AC | \$451 |
| CCRO::24TH PL::50 | 24TH PLACE | LARAMIE AVENUE | 51ST AVENUE | 17,429 | ALLIGATOR CR | 1.4% | Crack Sealing - AC | \$92 |
| CCRO::24TH PL::50 | 24TH PLACE | LARAMIE AVENUE | 51ST AVENUE | 17,429 | ALLIGATOR CR | 0.0% | Patching - AC Deep | \$224 |
| CCRO::24TH PL::60 | 24TH PLACE | 51ST AVENUE | 50TH AVENUE | 17,456 | ALLIGATOR CR | 1.4% | Crack Sealing - AC | \$95 |
| CCRO::24TH PL::60 | 24TH PLACE | 51ST AVENUE | 50TH AVENUE | 17,456 | BLOCK CR | 41.4% | Crack Sealing - AC | \$2,201 |

| Pavement ID | Road Name | From | To | Area | Distress Type | Density | Maint. Activity | Cost |
|--------------------|-------------|------------------|------------------|--------|---------------|---------|------------------------|---------|
| CCRO::24TH PL::60 | 24TH PLACE | 51ST AVENUE | 50TH AVENUE | 17,456 | L & T CR | 1.8% | Crack Sealing - AC | \$316 |
| CCRO::24TH PL::60 | 24TH PLACE | 51ST AVENUE | 50TH AVENUE | 17,456 | L & T CR | 1.8% | Crack Sealing - AC | \$317 |
| CCRO::24TH PL::60 | 24TH PLACE | 51ST AVENUE | 50TH AVENUE | 17,456 | ALLIGATOR CR | 0.2% | Patching - AC Deep | \$719 |
| CCRO::24TH PL::60 | 24TH PLACE | 51ST AVENUE | 50TH AVENUE | 17,456 | RUTTING | 0.0% | Patching - AC Shallow | \$17 |
| CCRO::24TH ST::20 | 24TH STREET | 61ST COURT | 61ST AVENUE | 8,862 | ALLIGATOR CR | 0.6% | Crack Sealing - AC | \$25 |
| CCRO::24TH ST::20 | 24TH STREET | 61ST COURT | 61ST AVENUE | 8,862 | L & T CR | 0.8% | Crack Sealing - AC | \$70 |
| CCRO::24TH ST::20 | 24TH STREET | 61ST COURT | 61ST AVENUE | 8,862 | ALLIGATOR CR | 1.0% | Patching - AC Deep | \$1,483 |
| CCRO::24TH ST::30 | 24TH STREET | 61ST AVENUE | 60TH COURT | 8,853 | ALLIGATOR CR | 0.3% | Crack Sealing - AC | \$16 |
| CCRO::24TH ST::40 | 24TH STREET | 60TH COURT | AUSTIN BOULEVARD | 8,505 | L & T CR | 1.1% | Crack Sealing - AC | \$90 |
| CCRO::24TH ST::40 | 24TH STREET | 60TH COURT | AUSTIN BOULEVARD | 8,505 | ALLIGATOR CR | 0.4% | Crack Sealing - AC | \$17 |
| CCRO::24TH ST::40 | 24TH STREET | 60TH COURT | AUSTIN BOULEVARD | 8,505 | L & T CR | 1.7% | Crack Sealing - AC | \$140 |
| CCRO::24TH ST::40 | 24TH STREET | 60TH COURT | AUSTIN BOULEVARD | 8,505 | ALLIGATOR CR | 0.4% | Patching - AC Deep | \$667 |
| CCRO::24TH ST::50 | 24TH STREET | AUSTIN BOULEVARD | 59TH COURT | 8,634 | L & T CR | 1.7% | Crack Sealing - AC | \$148 |
| CCRO::24TH ST::50 | 24TH STREET | AUSTIN BOULEVARD | 59TH COURT | 8,634 | BLOCK CR | 2.7% | Crack Sealing - AC | \$70 |
| CCRO::24TH ST::50 | 24TH STREET | AUSTIN BOULEVARD | 59TH COURT | 8,634 | ALLIGATOR CR | 1.6% | Crack Sealing - AC | \$59 |
| CCRO::24TH ST::50 | 24TH STREET | AUSTIN BOULEVARD | 59TH COURT | 8,634 | L & T CR | 1.7% | Crack Sealing - AC | \$146 |
| CCRO::24TH ST::50 | 24TH STREET | AUSTIN BOULEVARD | 59TH COURT | 8,634 | ALLIGATOR CR | 1.1% | Patching - AC Deep | \$1,485 |
| CCRO::25TH PL::20 | 25TH PLACE | 55TH AVENUE | 54TH AVENUE | 17,433 | ALLIGATOR CR | 0.2% | Crack Sealing - AC | \$18 |
| CCRO::25TH PL::20 | 25TH PLACE | 55TH AVENUE | 54TH AVENUE | 17,433 | L & T CR | 1.0% | Crack Sealing - AC | \$177 |
| CCRO::25TH PL::20 | 25TH PLACE | 55TH AVENUE | 54TH AVENUE | 17,433 | ALLIGATOR CR | 1.0% | Patching - AC Deep | \$2,455 |
| CCRO::25TH ST::130 | 25TH STREET | CENTRAL AVENUE | 55TH AVENUE | 17,747 | ALLIGATOR CR | 0.9% | Crack Sealing - AC | \$63 |
| CCRO::25TH ST::130 | 25TH STREET | CENTRAL AVENUE | 55TH AVENUE | 17,747 | L & T CR | 2.4% | Crack Sealing - AC | \$427 |
| CCRO::25TH ST::130 | 25TH STREET | CENTRAL AVENUE | 55TH AVENUE | 17,747 | L & T CR | 1.6% | Crack Sealing - AC | \$284 |
| CCRO::25TH ST::130 | 25TH STREET | CENTRAL AVENUE | 55TH AVENUE | 17,747 | ALLIGATOR CR | 0.9% | Patching - AC Deep | \$2,345 |
| CCRO::27TH ST::50 | 27TH STREET | AUSTIN BOULEVARD | 59TH COURT | 8,698 | L & T CR | 0.2% | Crack Sealing - AC | \$20 |
| CCRO::27TH ST::50 | 27TH STREET | AUSTIN BOULEVARD | 59TH COURT | 8,698 | L & T CR | 4.7% | Crack Sealing - AC | \$410 |
| CCRO::27TH ST::60 | 27TH STREET | 59TH COURT | 59TH AVENUE | 8,969 | L & T CR | 4.7% | Crack Sealing - AC | \$425 |
| CCRO::27TH ST::60 | 27TH STREET | 59TH COURT | 59TH AVENUE | 8,969 | L & T CR | 1.2% | Crack Sealing - AC | \$108 |
| CCRO::27TH ST::60 | 27TH STREET | 59TH COURT | 59TH AVENUE | 8,969 | ALLIGATOR CR | 0.4% | Crack Sealing - AC | \$20 |
| CCRO::27TH ST::60 | 27TH STREET | 59TH COURT | 59TH AVENUE | 8,969 | ALLIGATOR CR | 0.1% | Patching - AC Deep | \$322 |
| CCRO::27TH ST::60 | 27TH STREET | 59TH COURT | 59TH AVENUE | 8,969 | RUTTING | 0.1% | Patching - AC Shallow | \$29 |
| CCRO::27TH ST::70 | 27TH STREET | 59TH AVENUE | 58TH COURT | 8,680 | L & T CR | 0.8% | Crack Sealing - AC | \$71 |
| CCRO::27TH ST::70 | 27TH STREET | 59TH AVENUE | 58TH COURT | 8,680 | L & T CR | 2.9% | Crack Sealing - AC | \$248 |
| CCRO::27TH ST::70 | 27TH STREET | 59TH AVENUE | 58TH COURT | 8,680 | ALLIGATOR CR | 2.9% | Patching - AC Deep | \$3,560 |
| CCRO::27TH ST::80 | 27TH STREET | 58TH COURT | 58TH AVENUE | 8,302 | L & T CR | 0.7% | Crack Sealing - AC | \$57 |
| CCRO::27TH ST::80 | 27TH STREET | 58TH COURT | 58TH AVENUE | 8,302 | L & T CR | 4.1% | Crack Sealing - AC | \$340 |
| CCRO::28TH PL::10 | 28TH PLACE | 50TH AVENUE | 49TH AVENUE | 11,869 | L & T CR | 0.2% | Crack Sealing - AC | \$21 |
| CCRO::28TH PL::10 | 28TH PLACE | 50TH AVENUE | 49TH AVENUE | 11,869 | L & T CR | 1.1% | Crack Sealing - AC | \$124 |
| CCRO::28TH PL::10 | 28TH PLACE | 50TH AVENUE | 49TH AVENUE | 11,869 | ALLIGATOR CR | 0.2% | Patching - AC Deep | \$434 |
| CCRO::28TH ST::10 | 28TH STREET | LOMBARD AVENUE | 61ST COURT | 8,874 | JT SEAL DMG | 100.0% | Joint Seal (Localized) | \$1,579 |

| Pavement ID | Road Name | From | To | Area | Distress Type | Density | Maint. Activity | Cost |
|--------------------|-------------|----------------|------------------|--------|---------------|---------|------------------------------|----------|
| CCRO::28TH ST::10 | 28TH STREET | LOMBARD AVENUE | 61ST COURT | 8,874 | LARGE PATCH | 20.0% | Patching - PCC Full Depth | \$21,038 |
| CCRO::28TH ST::10 | 28TH STREET | LOMBARD AVENUE | 61ST COURT | 8,874 | JOINT SPALL | 5.0% | Patching - PCC Partial Depth | \$54 |
| CCRO::28TH ST::100 | 28TH STREET | 48TH COURT | CICERO AVENUE | 11,602 | L & T CR | 1.4% | Crack Sealing - AC | \$163 |
| CCRO::28TH ST::100 | 28TH STREET | 48TH COURT | CICERO AVENUE | 11,602 | L & T CR | 2.4% | Crack Sealing - AC | \$283 |
| CCRO::28TH ST::100 | 28TH STREET | 48TH COURT | CICERO AVENUE | 11,602 | ALLIGATOR CR | 2.1% | Crack Sealing - AC | \$94 |
| CCRO::28TH ST::100 | 28TH STREET | 48TH COURT | CICERO AVENUE | 11,602 | ALLIGATOR CR | 0.7% | Patching - AC Deep | \$1,356 |
| CCRO::28TH ST::20 | 28TH STREET | 61ST COURT | 61ST AVENUE | 8,678 | JT SEAL DMG | 100.0% | Joint Seal (Localized) | \$1,543 |
| CCRO::28TH ST::20 | 28TH STREET | 61ST COURT | 61ST AVENUE | 8,678 | LARGE PATCH | 15.0% | Patching - PCC Full Depth | \$15,502 |
| CCRO::28TH ST::40 | 28TH STREET | 60TH COURT | AUSTIN BOULEVARD | 8,390 | ALLIGATOR CR | 6.7% | Patching - AC Deep | \$7,284 |
| CCRO::28TH ST::40 | 28TH STREET | 60TH COURT | AUSTIN BOULEVARD | 8,390 | RUTTING | 0.5% | Patching - AC Shallow | \$226 |
| CCRO::28TH ST::90 | 28TH STREET | 49TH AVENUE | 48TH COURT | 11,642 | L & T CR | 0.3% | Crack Sealing - AC | \$39 |
| CCRO::28TH ST::90 | 28TH STREET | 49TH AVENUE | 48TH COURT | 11,642 | L & T CR | 3.4% | Crack Sealing - AC | \$395 |
| CCRO::30TH ST::80 | 30TH STREET | 49TH AVENUE | 48TH COURT | 11,752 | L & T CR | 3.2% | Crack Sealing - AC | \$380 |
| CCRO::30TH ST::80 | 30TH STREET | 49TH AVENUE | 48TH COURT | 11,752 | L & T CR | 0.9% | Crack Sealing - AC | \$99 |
| CCRO::30TH ST::80 | 30TH STREET | 49TH AVENUE | 48TH COURT | 11,752 | ALLIGATOR CR | 1.9% | Crack Sealing - AC | \$88 |
| CCRO::30TH ST::80 | 30TH STREET | 49TH AVENUE | 48TH COURT | 11,752 | ALLIGATOR CR | 1.6% | Patching - AC Deep | \$2,728 |
| CCRO::31ST ST::30 | 31ST STREET | OGDEN AVENUE | CENTRAL AVENUE | 32,814 | ALLIGATOR CR | 0.4% | Crack Sealing - AC | \$51 |
| CCRO::31ST ST::30 | 31ST STREET | OGDEN AVENUE | CENTRAL AVENUE | 32,814 | L & T CR | 2.1% | Crack Sealing - AC | \$700 |
| CCRO::31ST ST::30 | 31ST STREET | OGDEN AVENUE | CENTRAL AVENUE | 32,814 | L & T CR | 3.1% | Crack Sealing - AC | \$1,028 |
| CCRO::31ST ST::30 | 31ST STREET | OGDEN AVENUE | CENTRAL AVENUE | 32,814 | ALLIGATOR CR | 0.5% | Patching - AC Deep | \$2,379 |
| CCRO::31ST ST::30 | 31ST STREET | OGDEN AVENUE | CENTRAL AVENUE | 32,814 | RUTTING | 0.0% | Patching - AC Shallow | \$28 |
| CCRO::32ND ST::30 | 32ND STREET | 61ST COURT | 61ST AVENUE | 13,546 | L & T CR | 0.4% | Crack Sealing - AC | \$55 |
| CCRO::32ND ST::30 | 32ND STREET | 61ST COURT | 61ST AVENUE | 13,546 | ALLIGATOR CR | 0.5% | Crack Sealing - AC | \$32 |
| CCRO::32ND ST::40 | 32ND STREET | 61ST AVENUE | 60TH COURT | 13,785 | L & T CR | 2.4% | Crack Sealing - AC | \$325 |
| CCRO::32ND ST::40 | 32ND STREET | 61ST AVENUE | 60TH COURT | 13,785 | RUTTING | 0.0% | Patching - AC Shallow | \$24 |
| CCRO::32ND ST::50 | 32ND STREET | 60TH COURT | AUSTIN BOULEVARD | 12,948 | L & T CR | 0.7% | Crack Sealing - AC | \$87 |
| CCRO::32ND ST::50 | 32ND STREET | 60TH COURT | AUSTIN BOULEVARD | 12,948 | ALLIGATOR CR | 0.6% | Crack Sealing - AC | \$35 |
| CCRO::32ND ST::50 | 32ND STREET | 60TH COURT | AUSTIN BOULEVARD | 12,948 | L & T CR | 2.0% | Crack Sealing - AC | \$264 |
| CCRO::34TH ST::140 | 34TH STREET | 55TH COURT | 55TH AVENUE | 8,549 | L & T CR | 0.4% | Crack Sealing - AC | \$35 |
| CCRO::34TH ST::140 | 34TH STREET | 55TH COURT | 55TH AVENUE | 8,549 | L & T CR | 0.2% | Crack Sealing - AC | \$18 |
| CCRO::34TH ST::140 | 34TH STREET | 55TH COURT | 55TH AVENUE | 8,549 | BLOCK CR | 76.3% | Crack Sealing - AC | \$1,987 |
| CCRO::34TH ST::140 | 34TH STREET | 55TH COURT | 55TH AVENUE | 8,549 | ALLIGATOR CR | 0.5% | Crack Sealing - AC | \$23 |
| CCRO::35TH ST::110 | 35TH STREET | 57TH AVENUE | 56TH COURT | 8,910 | L & T CR | 0.6% | Crack Sealing - AC | \$53 |
| CCRO::35TH ST::110 | 35TH STREET | 57TH AVENUE | 56TH COURT | 8,910 | ALLIGATOR CR | 3.2% | Crack Sealing - AC | \$108 |
| CCRO::35TH ST::110 | 35TH STREET | 57TH AVENUE | 56TH COURT | 8,910 | L & T CR | 3.0% | Crack Sealing - AC | \$265 |
| CCRO::35TH ST::110 | 35TH STREET | 57TH AVENUE | 56TH COURT | 8,910 | ALLIGATOR CR | 0.8% | Patching - AC Deep | \$1,184 |
| CCRO::35TH ST::200 | 35TH STREET | 52ND COURT | LARAMIE AVENUE | 8,243 | RUTTING | 0.1% | Patching - AC Shallow | \$50 |
| CCRO::35TH ST::60 | 35TH STREET | 59TH COURT | 59TH AVENUE | 8,779 | ALLIGATOR CR | 3.9% | Crack Sealing - AC | \$129 |
| CCRO::35TH ST::60 | 35TH STREET | 59TH COURT | 59TH AVENUE | 8,779 | L & T CR | 2.0% | Crack Sealing - AC | \$177 |
| CCRO::35TH ST::60 | 35TH STREET | 59TH COURT | 59TH AVENUE | 8,779 | ALLIGATOR CR | 1.0% | Patching - AC Deep | \$1,426 |

| Pavement ID | Road Name | From | To | Area | Distress Type | Density | Maint. Activity | Cost |
|--------------------|-------------|------------------|------------------|-------|---------------|---------|-----------------------|---------|
| CCRO::35TH ST::70 | 35TH STREET | 59TH AVENUE | 58TH COURT | 8,697 | ALLIGATOR CR | 2.1% | Crack Sealing - AC | \$73 |
| CCRO::35TH ST::70 | 35TH STREET | 59TH AVENUE | 58TH COURT | 8,697 | L & T CR | 2.0% | Crack Sealing - AC | \$176 |
| CCRO::35TH ST::70 | 35TH STREET | 59TH AVENUE | 58TH COURT | 8,697 | BLOCK CR | 1.6% | Crack Sealing - AC | \$42 |
| CCRO::35TH ST::70 | 35TH STREET | 59TH AVENUE | 58TH COURT | 8,697 | L & T CR | 2.1% | Crack Sealing - AC | \$178 |
| CCRO::35TH ST::70 | 35TH STREET | 59TH AVENUE | 58TH COURT | 8,697 | ALLIGATOR CR | 0.2% | Patching - AC Deep | \$381 |
| CCRO::35TH ST::70 | 35TH STREET | 59TH AVENUE | 58TH COURT | 8,697 | RUTTING | 0.1% | Patching - AC Shallow | \$43 |
| CCRO::35TH ST::80 | 35TH STREET | 58TH COURT | 58TH AVENUE | 8,576 | BLOCK CR | 80.8% | Crack Sealing - AC | \$2,112 |
| CCRO::35TH ST::80 | 35TH STREET | 58TH COURT | 58TH AVENUE | 8,576 | ALLIGATOR CR | 0.4% | Crack Sealing - AC | \$18 |
| CCRO::35TH ST::80 | 35TH STREET | 58TH COURT | 58TH AVENUE | 8,576 | L & T CR | 0.6% | Crack Sealing - AC | \$53 |
| CCRO::35TH ST::80 | 35TH STREET | 58TH COURT | 58TH AVENUE | 8,576 | ALLIGATOR CR | 0.1% | Patching - AC Deep | \$305 |
| CCRO::35TH ST::90 | 35TH STREET | 58TH AVENUE | 57TH COURT | 8,646 | L & T CR | 2.5% | Crack Sealing - AC | \$213 |
| CCRO::35TH ST::90 | 35TH STREET | 58TH AVENUE | 57TH COURT | 8,646 | ALLIGATOR CR | 0.8% | Crack Sealing - AC | \$31 |
| CCRO::35TH ST::90 | 35TH STREET | 58TH AVENUE | 57TH COURT | 8,646 | L & T CR | 1.6% | Crack Sealing - AC | \$141 |
| CCRO::35TH ST::90 | 35TH STREET | 58TH AVENUE | 57TH COURT | 8,646 | RUTTING | 0.6% | Patching - AC Shallow | \$278 |
| CCRO::36TH ST::50 | 36TH STREET | AUSTIN BOULEVARD | 59TH COURT | 8,404 | ALLIGATOR CR | 0.2% | Crack Sealing - AC | \$10 |
| CCRO::36TH ST::60 | 36TH STREET | 59TH COURT | 59TH AVENUE | 8,874 | L & T CR | 1.0% | Crack Sealing - AC | \$88 |
| CCRO::36TH ST::60 | 36TH STREET | 59TH COURT | 59TH AVENUE | 8,874 | ALLIGATOR CR | 0.3% | Crack Sealing - AC | \$15 |
| CCRO::36TH ST::60 | 36TH STREET | 59TH COURT | 59TH AVENUE | 8,874 | L & T CR | 0.2% | Crack Sealing - AC | \$18 |
| CCRO::36TH ST::60 | 36TH STREET | 59TH COURT | 59TH AVENUE | 8,874 | ALLIGATOR CR | 0.2% | Patching - AC Deep | \$357 |
| CCRO::36TH ST::80 | 36TH STREET | 58TH COURT | 58TH AVENUE | 8,857 | ALLIGATOR CR | 0.2% | Patching - AC Deep | \$394 |
| CCRO::37TH ST::160 | 37TH STREET | 54TH COURT | 54TH AVENUE | 8,530 | ALLIGATOR CR | 0.5% | Crack Sealing - AC | \$22 |
| CCRO::37TH ST::160 | 37TH STREET | 54TH COURT | 54TH AVENUE | 8,530 | L & T CR | 2.3% | Crack Sealing - AC | \$197 |
| CCRO::37TH ST::160 | 37TH STREET | 54TH COURT | 54TH AVENUE | 8,530 | L & T CR | 2.3% | Crack Sealing - AC | \$194 |
| CCRO::37TH ST::160 | 37TH STREET | 54TH COURT | 54TH AVENUE | 8,530 | BLOCK CR | 4.0% | Crack Sealing - AC | \$105 |
| CCRO::37TH ST::160 | 37TH STREET | 54TH COURT | 54TH AVENUE | 8,530 | ALLIGATOR CR | 1.5% | Patching - AC Deep | \$1,920 |
| CCRO::37TH ST::90 | 37TH STREET | 58TH AVENUE | 57TH COURT | 8,487 | L & T CR | 2.2% | Crack Sealing - AC | \$189 |
| CCRO::37TH ST::90 | 37TH STREET | 58TH AVENUE | 57TH COURT | 8,487 | ALLIGATOR CR | 0.7% | Crack Sealing - AC | \$28 |
| CCRO::37TH ST::90 | 37TH STREET | 58TH AVENUE | 57TH COURT | 8,487 | L & T CR | 0.7% | Crack Sealing - AC | \$57 |
| CCRO::37TH ST::90 | 37TH STREET | 58TH AVENUE | 57TH COURT | 8,487 | ALLIGATOR CR | 1.4% | Patching - AC Deep | \$1,817 |
| CCRO::38TH ST::10 | 38TH STREET | LOMBARD AVENUE | 61ST COURT | 8,502 | BLOCK CR | 8.8% | Crack Sealing - AC | \$227 |
| CCRO::38TH ST::40 | 38TH STREET | 60TH COURT | AUSTIN BOULEVARD | 8,779 | L & T CR | 0.2% | Crack Sealing - AC | \$18 |
| CCRO::38TH ST::40 | 38TH STREET | 60TH COURT | AUSTIN BOULEVARD | 8,779 | ALLIGATOR CR | 0.3% | Crack Sealing - AC | \$15 |
| CCRO::38TH ST::40 | 38TH STREET | 60TH COURT | AUSTIN BOULEVARD | 8,779 | L & T CR | 0.4% | Crack Sealing - AC | \$35 |
| CCRO::38TH ST::40 | 38TH STREET | 60TH COURT | AUSTIN BOULEVARD | 8,779 | ALLIGATOR CR | 0.1% | Patching - AC Deep | \$177 |
| CCRO::38TH ST::40 | 38TH STREET | 60TH COURT | AUSTIN BOULEVARD | 8,779 | RUTTING | 0.0% | Patching - AC Shallow | \$18 |
| CCRO::38TH ST::50 | 38TH STREET | AUSTIN BOULEVARD | 59TH COURT | 8,260 | L & T CR | 0.2% | Crack Sealing - AC | \$18 |
| CCRO::38TH ST::50 | 38TH STREET | AUSTIN BOULEVARD | 59TH COURT | 8,260 | L & T CR | 0.6% | Crack Sealing - AC | \$53 |
| CCRO::38TH ST::50 | 38TH STREET | AUSTIN BOULEVARD | 59TH COURT | 8,260 | ALLIGATOR CR | 0.1% | Crack Sealing - AC | \$5 |
| CCRO::38TH ST::50 | 38TH STREET | AUSTIN BOULEVARD | 59TH COURT | 8,260 | ALLIGATOR CR | 0.5% | Patching - AC Deep | \$798 |
| CCRO::38TH ST::60 | 38TH STREET | 59TH COURT | 59TH AVENUE | 9,018 | ALLIGATOR CR | 0.1% | Crack Sealing - AC | \$7 |

| Pavement ID | Road Name | From | To | Area | Distress Type | Density | Maint. Activity | Cost |
|---------------------|-------------|-------------|-------------|--------|---------------|---------|------------------------------|---------|
| CCRO::38TH ST::60 | 38TH STREET | 59TH COURT | 59TH AVENUE | 9,018 | L & T CR | 0.4% | Crack Sealing - AC | \$36 |
| CCRO::38TH ST::60 | 38TH STREET | 59TH COURT | 59TH AVENUE | 9,018 | ALLIGATOR CR | 2.1% | Patching - AC Deep | \$2,678 |
| CCRO::38TH ST::80 | 38TH STREET | 58TH COURT | 58TH AVENUE | 8,533 | RUTTING | 0.0% | Patching - AC Shallow | \$15 |
| CCRO::47TH CT::10 | 47TH COURT | 22ND STREET | 21ST STREET | 16,574 | L & T CR | 0.4% | Crack Sealing - AC | \$67 |
| CCRO::47TH CT::10 | 47TH COURT | 22ND STREET | 21ST STREET | 16,574 | L & T CR | 0.8% | Crack Sealing - AC | \$135 |
| CCRO::47TH CT::10 | 47TH COURT | 22ND STREET | 21ST STREET | 16,574 | PATCH/UT CUT | 0.3% | Patching - AC Deep | \$903 |
| CCRO::47TH CT::10 | 47TH COURT | 22ND STREET | 21ST STREET | 16,574 | ALLIGATOR CR | 1.2% | Patching - AC Deep | \$2,913 |
| CCRO::47TH CT::10 | 47TH COURT | 22ND STREET | 21ST STREET | 16,574 | RUTTING | 0.0% | Patching - AC Shallow | \$38 |
| CCRO::48TH CT::20 | 48TH COURT | 32ND STREET | 31ST STREET | 17,256 | BLOCK CR | 97.7% | Crack Sealing - AC | \$5,139 |
| CCRO::48TH CT::20 | 48TH COURT | 32ND STREET | 31ST STREET | 17,256 | RUTTING | 0.0% | Patching - AC Shallow | \$19 |
| CCRO::49TH AVE::120 | 49TH AVENUE | 25TH PLACE | 25TH STREET | 8,640 | L & T CR | 1.9% | Crack Sealing - AC | \$160 |
| CCRO::49TH AVE::120 | 49TH AVENUE | 25TH PLACE | 25TH STREET | 8,640 | L & T CR | 1.7% | Crack Sealing - AC | \$143 |
| CCRO::49TH AVE::120 | 49TH AVENUE | 25TH PLACE | 25TH STREET | 8,640 | ALLIGATOR CR | 0.3% | Crack Sealing - AC | \$16 |
| CCRO::49TH AVE::120 | 49TH AVENUE | 25TH PLACE | 25TH STREET | 8,640 | ALLIGATOR CR | 0.8% | Patching - AC Deep | \$1,218 |
| CCRO::49TH AVE::240 | 49TH AVENUE | 16TH STREET | 15TH STREET | 16,887 | BLOCK CR | 1.0% | Crack Sealing - AC | \$51 |
| CCRO::49TH AVE::240 | 49TH AVENUE | 16TH STREET | 15TH STREET | 16,887 | ALLIGATOR CR | 0.1% | Crack Sealing - AC | \$14 |
| CCRO::49TH AVE::240 | 49TH AVENUE | 16TH STREET | 15TH STREET | 16,887 | ALLIGATOR CR | 3.6% | Patching - AC Deep | \$7,902 |
| CCRO::49TH AVE::240 | 49TH AVENUE | 16TH STREET | 15TH STREET | 16,887 | RUTTING | 0.0% | Patching - AC Shallow | \$30 |
| CCRO::49TH AVE::250 | 49TH AVENUE | 15TH STREET | 14TH STREET | 17,284 | L & T CR | 0.9% | Crack Sealing - AC | \$155 |
| CCRO::49TH AVE::250 | 49TH AVENUE | 15TH STREET | 14TH STREET | 17,284 | L & T CR | 1.4% | Crack Sealing - AC | \$234 |
| CCRO::49TH AVE::250 | 49TH AVENUE | 15TH STREET | 14TH STREET | 17,284 | ALLIGATOR CR | 0.5% | Crack Sealing - AC | \$36 |
| CCRO::49TH AVE::250 | 49TH AVENUE | 15TH STREET | 14TH STREET | 17,284 | ALLIGATOR CR | 0.7% | Patching - AC Deep | \$1,764 |
| CCRO::50TH AVE::120 | 50TH AVENUE | 26TH STREET | END | 6,374 | LINEAR CR | 8.3% | Crack Sealing - PCC | \$64 |
| CCRO::50TH AVE::120 | 50TH AVENUE | 26TH STREET | END | 6,374 | CORNER BREAK | 8.3% | Crack Sealing - PCC | \$42 |
| CCRO::50TH AVE::120 | 50TH AVENUE | 26TH STREET | END | 6,374 | JT SEAL DMG | 100.0% | Joint Seal (Localized) | \$1,123 |
| CCRO::50TH AVE::120 | 50TH AVENUE | 26TH STREET | END | 6,374 | JOINT SPALL | 8.3% | Patching - PCC Partial Depth | \$515 |
| CCRO::50TH AVE::210 | 50TH AVENUE | 22ND STREET | 21ST PLACE | 12,486 | L & T CR | 1.7% | Crack Sealing - AC | \$214 |
| CCRO::50TH AVE::210 | 50TH AVENUE | 22ND STREET | 21ST PLACE | 12,486 | L & T CR | 1.4% | Crack Sealing - AC | \$168 |
| CCRO::50TH AVE::70 | 50TH AVENUE | 30TH STREET | 29TH PLACE | 8,618 | L & T CR | 1.9% | Crack Sealing - AC | \$159 |
| CCRO::50TH AVE::70 | 50TH AVENUE | 30TH STREET | 29TH PLACE | 8,618 | ALLIGATOR CR | 1.2% | Crack Sealing - AC | \$45 |
| CCRO::50TH AVE::80 | 50TH AVENUE | 29TH PLACE | 29TH STREET | 8,781 | L & T CR | 2.2% | Crack Sealing - AC | \$193 |
| CCRO::50TH AVE::80 | 50TH AVENUE | 29TH PLACE | 29TH STREET | 8,781 | L & T CR | 0.4% | Crack Sealing - AC | \$36 |
| CCRO::50TH AVE::80 | 50TH AVENUE | 29TH PLACE | 29TH STREET | 8,781 | ALLIGATOR CR | 0.1% | Crack Sealing - AC | \$9 |
| CCRO::50TH AVE::80 | 50TH AVENUE | 29TH PLACE | 29TH STREET | 8,781 | ALLIGATOR CR | 4.9% | Patching - AC Deep | \$5,660 |
| CCRO::50TH AVE::80 | 50TH AVENUE | 29TH PLACE | 29TH STREET | 8,781 | BLOCK CR | 3.1% | Patching - AC Shallow | \$1,505 |
| CCRO::50TH CT::50 | 50TH COURT | 31ST STREET | 30TH PLACE | 8,547 | L & T CR | 1.9% | Crack Sealing - AC | \$159 |
| CCRO::50TH CT::50 | 50TH COURT | 31ST STREET | 30TH PLACE | 8,547 | L & T CR | 1.2% | Crack Sealing - AC | \$106 |
| CCRO::50TH CT::50 | 50TH COURT | 31ST STREET | 30TH PLACE | 8,547 | ALLIGATOR CR | 3.3% | Patching - AC Deep | \$3,890 |
| CCRO::50TH CT::50 | 50TH COURT | 31ST STREET | 30TH PLACE | 8,547 | BLOCK CR | 0.8% | Patching - AC Shallow | \$387 |
| CCRO::50TH CT::60 | 50TH COURT | 30TH PLACE | 30TH STREET | 8,694 | L & T CR | 0.2% | Crack Sealing - AC | \$18 |

| Pavement ID | Road Name | From | To | Area | Distress Type | Density | Maint. Activity | Cost |
|---------------------|-------------|-------------|--------------|--------|---------------|---------|-----------------------|---------|
| CCRO::50TH CT::60 | 50TH COURT | 30TH PLACE | 30TH STREET | 8,694 | L & T CR | 1.0% | Crack Sealing - AC | \$89 |
| CCRO::50TH CT::60 | 50TH COURT | 30TH PLACE | 30TH STREET | 8,694 | ALLIGATOR CR | 5.0% | Patching - AC Deep | \$5,693 |
| CCRO::51ST AVE::110 | 51ST AVENUE | 25TH PLACE | 25TH STREET | 8,273 | L & T CR | 2.1% | Crack Sealing - AC | \$177 |
| CCRO::51ST AVE::110 | 51ST AVENUE | 25TH PLACE | 25TH STREET | 8,273 | L & T CR | 1.7% | Crack Sealing - AC | \$140 |
| CCRO::51ST AVE::110 | 51ST AVENUE | 25TH PLACE | 25TH STREET | 8,273 | ALLIGATOR CR | 0.8% | Crack Sealing - AC | \$31 |
| CCRO::51ST AVE::110 | 51ST AVENUE | 25TH PLACE | 25TH STREET | 8,273 | EDGE CR | 0.1% | Crack Sealing - AC | \$5 |
| CCRO::51ST AVE::140 | 51ST AVENUE | 24TH STREET | 23RD PLACE | 8,635 | ALLIGATOR CR | 1.0% | Crack Sealing - AC | \$40 |
| CCRO::51ST AVE::140 | 51ST AVENUE | 24TH STREET | 23RD PLACE | 8,635 | EDGE CR | 0.2% | Crack Sealing - AC | \$19 |
| CCRO::51ST AVE::140 | 51ST AVENUE | 24TH STREET | 23RD PLACE | 8,635 | L & T CR | 3.1% | Crack Sealing - AC | \$263 |
| CCRO::51ST AVE::140 | 51ST AVENUE | 24TH STREET | 23RD PLACE | 8,635 | ALLIGATOR CR | 0.8% | Patching - AC Deep | \$1,201 |
| CCRO::51ST AVE::190 | 51ST AVENUE | 21ST PLACE | 21ST STREET | 10,003 | L & T CR | 2.4% | Crack Sealing - AC | \$239 |
| CCRO::51ST AVE::190 | 51ST AVENUE | 21ST PLACE | 21ST STREET | 10,003 | L & T CR | 2.0% | Crack Sealing - AC | \$204 |
| CCRO::51ST AVE::190 | 51ST AVENUE | 21ST PLACE | 21ST STREET | 10,003 | ALLIGATOR CR | 0.3% | Crack Sealing - AC | \$17 |
| CCRO::51ST AVE::190 | 51ST AVENUE | 21ST PLACE | 21ST STREET | 10,003 | ALLIGATOR CR | 0.6% | Patching - AC Deep | \$1,047 |
| CCRO::51ST AVE::220 | 51ST AVENUE | 18TH STREET | 16TH STREET | 18,156 | ALLIGATOR CR | 0.7% | Crack Sealing - AC | \$51 |
| CCRO::51ST AVE::220 | 51ST AVENUE | 18TH STREET | 16TH STREET | 18,156 | L & T CR | 1.0% | Crack Sealing - AC | \$186 |
| CCRO::51ST AVE::220 | 51ST AVENUE | 18TH STREET | 16TH STREET | 18,156 | ALLIGATOR CR | 1.3% | Patching - AC Deep | \$3,234 |
| CCRO::52ND CT::20 | 52ND COURT | 37TH STREET | 36TH STREET | 17,250 | L & T CR | 1.4% | Crack Sealing - AC | \$233 |
| CCRO::52ND CT::20 | 52ND COURT | 37TH STREET | 36TH STREET | 17,250 | L & T CR | 1.8% | Crack Sealing - AC | \$313 |
| CCRO::52ND CT::20 | 52ND COURT | 37TH STREET | 36TH STREET | 17,250 | ALLIGATOR CR | 1.2% | Crack Sealing - AC | \$82 |
| CCRO::52ND CT::20 | 52ND COURT | 37TH STREET | 36TH STREET | 17,250 | ALLIGATOR CR | 0.4% | Patching - AC Deep | \$1,258 |
| CCRO::52ND CT::30 | 52ND COURT | 36TH STREET | 35TH STREET | 17,451 | L & T CR | 2.1% | Crack Sealing - AC | \$361 |
| CCRO::52ND CT::30 | 52ND COURT | 36TH STREET | 35TH STREET | 17,451 | L & T CR | 0.3% | Crack Sealing - AC | \$59 |
| CCRO::52ND CT::30 | 52ND COURT | 36TH STREET | 35TH STREET | 17,451 | ALLIGATOR CR | 2.7% | Crack Sealing - AC | \$172 |
| CCRO::52ND CT::30 | 52ND COURT | 36TH STREET | 35TH STREET | 17,451 | ALLIGATOR CR | 0.3% | Patching - AC Deep | \$981 |
| CCRO::52ND CT::30 | 52ND COURT | 36TH STREET | 35TH STREET | 17,451 | RUTTING | 0.1% | Patching - AC Shallow | \$59 |
| CCRO::52ND CT::50 | 52ND COURT | 32ND STREET | 31ST STREET | 17,104 | L & T CR | 3.1% | Crack Sealing - AC | \$521 |
| CCRO::52ND CT::50 | 52ND COURT | 32ND STREET | 31ST STREET | 17,104 | ALLIGATOR CR | 0.9% | Crack Sealing - AC | \$65 |
| CCRO::52ND CT::50 | 52ND COURT | 32ND STREET | 31ST STREET | 17,104 | L & T CR | 1.7% | Crack Sealing - AC | \$294 |
| CCRO::52ND CT::50 | 52ND COURT | 32ND STREET | 31ST STREET | 17,104 | ALLIGATOR CR | 0.9% | Patching - AC Deep | \$2,376 |
| CCRO::53RD AVE::100 | 53RD AVENUE | 29TH PLACE | OGDEN AVENUE | 6,733 | L & T CR | 2.7% | Crack Sealing - AC | \$184 |
| CCRO::53RD AVE::100 | 53RD AVENUE | 29TH PLACE | OGDEN AVENUE | 6,733 | L & T CR | 3.8% | Crack Sealing - AC | \$254 |
| CCRO::53RD AVE::100 | 53RD AVENUE | 29TH PLACE | OGDEN AVENUE | 6,733 | ALLIGATOR CR | 2.5% | Crack Sealing - AC | \$67 |
| CCRO::53RD AVE::100 | 53RD AVENUE | 29TH PLACE | OGDEN AVENUE | 6,733 | RUTTING | 0.2% | Patching - AC Shallow | \$60 |
| CCRO::53RD AVE::80 | 53RD AVENUE | 30TH PLACE | 30TH STREET | 8,534 | ALLIGATOR CR | 1.3% | Crack Sealing - AC | \$48 |
| CCRO::53RD AVE::80 | 53RD AVENUE | 30TH PLACE | 30TH STREET | 8,534 | L & T CR | 1.5% | Crack Sealing - AC | \$125 |
| CCRO::53RD AVE::80 | 53RD AVENUE | 30TH PLACE | 30TH STREET | 8,534 | L & T CR | 0.8% | Crack Sealing - AC | \$71 |
| CCRO::53RD AVE::80 | 53RD AVENUE | 30TH PLACE | 30TH STREET | 8,534 | ALLIGATOR CR | 0.3% | Patching - AC Deep | \$488 |
| CCRO::53RD AVE::90 | 53RD AVENUE | 30TH STREET | 29TH PLACE | 8,907 | L & T CR | 3.4% | Crack Sealing - AC | \$302 |
| CCRO::53RD AVE::90 | 53RD AVENUE | 30TH STREET | 29TH PLACE | 8,907 | L & T CR | 0.2% | Crack Sealing - AC | \$18 |

| Pavement ID | Road Name | From | To | Area | Distress Type | Density | Maint. Activity | Cost |
|---------------------|-------------|-------------|-------------|--------|---------------|---------|-----------------------|---------|
| CCRO::53RD AVE::90 | 53RD AVENUE | 30TH STREET | 29TH PLACE | 8,907 | ALLIGATOR CR | 2.5% | Crack Sealing - AC | \$86 |
| CCRO::53RD AVE::90 | 53RD AVENUE | 30TH STREET | 29TH PLACE | 8,907 | ALLIGATOR CR | 0.4% | Patching - AC Deep | \$698 |
| CCRO::53RD CT::10 | 53RD COURT | 38TH STREET | 37TH STREET | 17,349 | ALLIGATOR CR | 1.1% | Crack Sealing - AC | \$75 |
| CCRO::53RD CT::10 | 53RD COURT | 38TH STREET | 37TH STREET | 17,349 | L & T CR | 1.5% | Crack Sealing - AC | \$254 |
| CCRO::53RD CT::10 | 53RD COURT | 38TH STREET | 37TH STREET | 17,349 | L & T CR | 0.1% | Crack Sealing - AC | \$22 |
| CCRO::53RD CT::10 | 53RD COURT | 38TH STREET | 37TH STREET | 17,349 | ALLIGATOR CR | 0.6% | Patching - AC Deep | \$1,628 |
| CCRO::53RD CT::10 | 53RD COURT | 38TH STREET | 37TH STREET | 17,349 | RUTTING | 0.0% | Patching - AC Shallow | \$34 |
| CCRO::53RD CT::40 | 53RD COURT | 35TH STREET | 34TH STREET | 17,247 | L & T CR | 1.7% | Crack Sealing - AC | \$294 |
| CCRO::53RD CT::40 | 53RD COURT | 35TH STREET | 34TH STREET | 17,247 | ALLIGATOR CR | 2.7% | Crack Sealing - AC | \$168 |
| CCRO::53RD CT::40 | 53RD COURT | 35TH STREET | 34TH STREET | 17,247 | ALLIGATOR CR | 0.5% | Patching - AC Deep | \$1,353 |
| CCRO::54TH AVE::40 | 54TH AVENUE | 35TH STREET | 34TH STREET | 17,156 | ALLIGATOR CR | 1.2% | Crack Sealing - AC | \$80 |
| CCRO::54TH AVE::40 | 54TH AVENUE | 35TH STREET | 34TH STREET | 17,156 | L & T CR | 0.2% | Crack Sealing - AC | \$39 |
| CCRO::54TH AVE::40 | 54TH AVENUE | 35TH STREET | 34TH STREET | 17,156 | L & T CR | 1.2% | Crack Sealing - AC | \$201 |
| CCRO::54TH AVE::40 | 54TH AVENUE | 35TH STREET | 34TH STREET | 17,156 | ALLIGATOR CR | 0.4% | Patching - AC Deep | \$1,200 |
| CCRO::54TH CT::20 | 54TH COURT | 36TH STREET | 35TH STREET | 17,271 | L & T CR | 0.5% | Crack Sealing - AC | \$78 |
| CCRO::54TH CT::20 | 54TH COURT | 36TH STREET | 35TH STREET | 17,271 | L & T CR | 3.4% | Crack Sealing - AC | \$589 |
| CCRO::54TH CT::20 | 54TH COURT | 36TH STREET | 35TH STREET | 17,271 | ALLIGATOR CR | 2.0% | Crack Sealing - AC | \$131 |
| CCRO::54TH CT::20 | 54TH COURT | 36TH STREET | 35TH STREET | 17,271 | RUTTING | 0.0% | Patching - AC Shallow | \$32 |
| CCRO::55TH AVE::10 | 55TH AVENUE | 37TH STREET | 36TH STREET | 17,250 | RUTTING | 0.8% | Patching - AC Shallow | \$715 |
| CCRO::55TH AVE::150 | 55TH AVENUE | 22ND STREET | 22ND STREET | 908 | L & T CR | 1.0% | Crack Sealing - AC | \$9 |
| CCRO::55TH AVE::160 | 55TH AVENUE | 19TH STREET | 18TH STREET | 21,389 | L & T CR | 1.4% | Crack Sealing - AC | \$299 |
| CCRO::55TH AVE::160 | 55TH AVENUE | 19TH STREET | 18TH STREET | 21,389 | L & T CR | 5.6% | Crack Sealing - AC | \$1,201 |
| CCRO::55TH AVE::160 | 55TH AVENUE | 19TH STREET | 18TH STREET | 21,389 | ALLIGATOR CR | 0.3% | Crack Sealing - AC | \$30 |
| CCRO::55TH AVE::160 | 55TH AVENUE | 19TH STREET | 18TH STREET | 21,389 | ALLIGATOR CR | 0.5% | Patching - AC Deep | \$1,745 |
| CCRO::55TH AVE::160 | 55TH AVENUE | 19TH STREET | 18TH STREET | 21,389 | RUTTING | 0.0% | Patching - AC Shallow | \$17 |
| CCRO::55TH AVE::70 | 55TH AVENUE | 26TH STREET | 25TH PLACE | 8,658 | L & T CR | 0.2% | Crack Sealing - AC | \$20 |
| CCRO::55TH AVE::70 | 55TH AVENUE | 26TH STREET | 25TH PLACE | 8,658 | L & T CR | 0.9% | Crack Sealing - AC | \$76 |
| CCRO::55TH AVE::70 | 55TH AVENUE | 26TH STREET | 25TH PLACE | 8,658 | ALLIGATOR CR | 1.0% | Crack Sealing - AC | \$38 |
| CCRO::55TH AVE::70 | 55TH AVENUE | 26TH STREET | 25TH PLACE | 8,658 | ALLIGATOR CR | 1.6% | Patching - AC Deep | \$2,088 |
| CCRO::55TH AVE::70 | 55TH AVENUE | 26TH STREET | 25TH PLACE | 8,658 | BLOCK CR | 2.6% | Patching - AC Shallow | \$1,233 |
| CCRO::55TH CT::20 | 55TH COURT | 36TH STREET | 35TH STREET | 17,325 | ALLIGATOR CR | 0.4% | Crack Sealing - AC | \$30 |
| CCRO::55TH CT::20 | 55TH COURT | 36TH STREET | 35TH STREET | 17,325 | L & T CR | 2.8% | Crack Sealing - AC | \$478 |
| CCRO::55TH CT::30 | 55TH COURT | 35TH STREET | 34TH STREET | 17,150 | ALLIGATOR CR | 2.5% | Crack Sealing - AC | \$158 |
| CCRO::55TH CT::30 | 55TH COURT | 35TH STREET | 34TH STREET | 17,150 | L & T CR | 4.1% | Crack Sealing - AC | \$701 |
| CCRO::55TH CT::30 | 55TH COURT | 35TH STREET | 34TH STREET | 17,150 | ALLIGATOR CR | 0.7% | Patching - AC Deep | \$1,941 |
| CCRO::55TH CT::90 | 55TH COURT | 14TH STREET | 13TH STREET | 25,473 | L & T CR | 1.7% | Crack Sealing - AC | \$427 |
| CCRO::55TH CT::90 | 55TH COURT | 14TH STREET | 13TH STREET | 25,473 | L & T CR | 1.7% | Crack Sealing - AC | \$443 |
| CCRO::55TH CT::90 | 55TH COURT | 14TH STREET | 13TH STREET | 25,473 | ALLIGATOR CR | 0.6% | Crack Sealing - AC | \$62 |
| CCRO::55TH CT::90 | 55TH COURT | 14TH STREET | 13TH STREET | 25,473 | RUTTING | 0.1% | Patching - AC Shallow | \$87 |
| CCRO::56TH CT::130 | 56TH COURT | 16TH STREET | 15TH STREET | 17,109 | ALLIGATOR CR | 1.4% | Crack Sealing - AC | \$94 |

| Pavement ID | Road Name | From | To | Area | Distress Type | Density | Maint. Activity | Cost |
|---------------------|-------------|-------------|----------------|--------|---------------|---------|-----------------------|---------|
| CCRO::56TH CT::130 | 56TH COURT | 16TH STREET | 15TH STREET | 17,109 | L & T CR | 0.5% | Crack Sealing - AC | \$78 |
| CCRO::56TH CT::130 | 56TH COURT | 16TH STREET | 15TH STREET | 17,109 | L & T CR | 0.6% | Crack Sealing - AC | \$97 |
| CCRO::56TH CT::130 | 56TH COURT | 16TH STREET | 15TH STREET | 17,109 | ALLIGATOR CR | 1.2% | Patching - AC Deep | \$2,937 |
| CCRO::56TH CT::130 | 56TH COURT | 16TH STREET | 15TH STREET | 17,109 | RUTTING | 0.1% | Patching - AC Shallow | \$84 |
| CCRO::56TH CT::150 | 56TH COURT | 14TH STREET | 13TH STREET | 17,246 | L & T CR | 1.8% | Crack Sealing - AC | \$311 |
| CCRO::56TH CT::150 | 56TH COURT | 14TH STREET | 13TH STREET | 17,246 | ALLIGATOR CR | 0.5% | Crack Sealing - AC | \$40 |
| CCRO::56TH CT::150 | 56TH COURT | 14TH STREET | 13TH STREET | 17,246 | L & T CR | 2.4% | Crack Sealing - AC | \$412 |
| CCRO::56TH CT::150 | 56TH COURT | 14TH STREET | 13TH STREET | 17,246 | ALLIGATOR CR | 0.2% | Patching - AC Deep | \$561 |
| CCRO::56TH CT::160 | 56TH COURT | 13TH STREET | ROOSEVELT ROAD | 17,260 | ALLIGATOR CR | 0.6% | Crack Sealing - AC | \$44 |
| CCRO::56TH CT::160 | 56TH COURT | 13TH STREET | ROOSEVELT ROAD | 17,260 | L & T CR | 1.8% | Crack Sealing - AC | \$312 |
| CCRO::56TH CT::160 | 56TH COURT | 13TH STREET | ROOSEVELT ROAD | 17,260 | L & T CR | 2.5% | Crack Sealing - AC | \$434 |
| CCRO::56TH CT::160 | 56TH COURT | 13TH STREET | ROOSEVELT ROAD | 17,260 | ALLIGATOR CR | 0.1% | Patching - AC Deep | \$321 |
| CCRO::56TH CT::160 | 56TH COURT | 13TH STREET | ROOSEVELT ROAD | 17,260 | RUTTING | 0.0% | Patching - AC Shallow | \$41 |
| CCRO::56TH CT::90 | 56TH COURT | 22ND STREET | 21ST STREET | 17,748 | ALLIGATOR CR | 0.7% | Crack Sealing - AC | \$51 |
| CCRO::56TH CT::90 | 56TH COURT | 22ND STREET | 21ST STREET | 17,748 | L & T CR | 0.8% | Crack Sealing - AC | \$144 |
| CCRO::56TH CT::90 | 56TH COURT | 22ND STREET | 21ST STREET | 17,748 | L & T CR | 1.0% | Crack Sealing - AC | \$175 |
| CCRO::56TH CT::90 | 56TH COURT | 22ND STREET | 21ST STREET | 17,748 | ALLIGATOR CR | 1.1% | Patching - AC Deep | \$2,840 |
| CCRO::57TH AVE::130 | 57TH AVENUE | 22ND STREET | 21ST STREET | 17,576 | RUTTING | 0.1% | Patching - AC Shallow | \$74 |
| CCRO::57TH AVE::70 | 57TH AVENUE | 26TH STREET | 25TH STREET | 17,207 | L & T CR | 0.2% | Crack Sealing - AC | \$39 |
| CCRO::57TH AVE::70 | 57TH AVENUE | 26TH STREET | 25TH STREET | 17,207 | ALLIGATOR CR | 0.3% | Crack Sealing - AC | \$24 |
| CCRO::57TH AVE::70 | 57TH AVENUE | 26TH STREET | 25TH STREET | 17,207 | L & T CR | 0.6% | Crack Sealing - AC | \$98 |
| CCRO::57TH AVE::70 | 57TH AVENUE | 26TH STREET | 25TH STREET | 17,207 | ALLIGATOR CR | 0.4% | Patching - AC Deep | \$1,264 |
| CCRO::57TH AVE::70 | 57TH AVENUE | 26TH STREET | 25TH STREET | 17,207 | RUTTING | 0.0% | Patching - AC Shallow | \$17 |
| CCRO::57TH AVE::80 | 57TH AVENUE | 25TH STREET | 24TH STREET | 17,526 | L & T CR | 0.9% | Crack Sealing - AC | \$157 |
| CCRO::57TH AVE::80 | 57TH AVENUE | 25TH STREET | 24TH STREET | 17,526 | ALLIGATOR CR | 0.2% | Crack Sealing - AC | \$20 |
| CCRO::57TH AVE::80 | 57TH AVENUE | 25TH STREET | 24TH STREET | 17,526 | L & T CR | 0.8% | Crack Sealing - AC | \$139 |
| CCRO::57TH AVE::80 | 57TH AVENUE | 25TH STREET | 24TH STREET | 17,526 | ALLIGATOR CR | 0.8% | Patching - AC Deep | \$2,024 |
| CCRO::57TH AVE::90 | 57TH AVENUE | 24TH STREET | 23RD PLACE | 8,479 | ALLIGATOR CR | 0.1% | Crack Sealing - AC | \$6 |
| CCRO::57TH AVE::90 | 57TH AVENUE | 24TH STREET | 23RD PLACE | 8,479 | ALLIGATOR CR | 0.2% | Patching - AC Deep | \$357 |
| CCRO::57TH CT::130 | 57TH COURT | 16TH STREET | 15TH STREET | 17,071 | ALLIGATOR CR | 0.3% | Crack Sealing - AC | \$24 |
| CCRO::57TH CT::130 | 57TH COURT | 16TH STREET | 15TH STREET | 17,071 | L & T CR | 0.1% | Crack Sealing - AC | \$20 |
| CCRO::57TH CT::130 | 57TH COURT | 16TH STREET | 15TH STREET | 17,071 | L & T CR | 0.2% | Crack Sealing - AC | \$39 |
| CCRO::57TH CT::130 | 57TH COURT | 16TH STREET | 15TH STREET | 17,071 | BLOCK CR | 0.4% | Patching - AC Shallow | \$407 |
| CCRO::57TH CT::140 | 57TH COURT | 15TH STREET | 14TH STREET | 17,357 | L & T CR | 0.2% | Crack Sealing - AC | \$40 |
| CCRO::57TH CT::140 | 57TH COURT | 15TH STREET | 14TH STREET | 17,357 | ALLIGATOR CR | 0.1% | Crack Sealing - AC | \$11 |
| CCRO::57TH CT::150 | 57TH COURT | 14TH STREET | 13TH STREET | 17,275 | L & T CR | 0.3% | Crack Sealing - AC | \$59 |
| CCRO::57TH CT::150 | 57TH COURT | 14TH STREET | 13TH STREET | 17,275 | L & T CR | 0.1% | Crack Sealing - AC | \$20 |
| CCRO::57TH CT::150 | 57TH COURT | 14TH STREET | 13TH STREET | 17,275 | ALLIGATOR CR | 0.3% | Crack Sealing - AC | \$26 |
| CCRO::57TH CT::150 | 57TH COURT | 14TH STREET | 13TH STREET | 17,275 | ALLIGATOR CR | 0.8% | Patching - AC Deep | \$1,982 |
| CCRO::57TH CT::160 | 57TH COURT | 13TH STREET | ROOSEVELT ROAD | 17,286 | RUTTING | 0.0% | Patching - AC Shallow | \$32 |

| Pavement ID | Road Name | From | To | Area | Distress Type | Density | Maint. Activity | Cost |
|--------------------|-------------|-------------|-------------|--------|---------------|---------|-----------------------|---------|
| CCRO::58TH AVE::50 | 58TH AVENUE | 35TH STREET | 34TH STREET | 17,072 | L & T CR | 0.5% | Crack Sealing - AC | \$78 |
| CCRO::58TH AVE::60 | 58TH AVENUE | 34TH STREET | PARK AVENUE | 13,767 | L & T CR | 0.8% | Crack Sealing - AC | \$107 |
| CCRO::58TH AVE::60 | 58TH AVENUE | 34TH STREET | PARK AVENUE | 13,767 | ALLIGATOR CR | 0.2% | Crack Sealing - AC | \$13 |
| CCRO::58TH AVE::60 | 58TH AVENUE | 34TH STREET | PARK AVENUE | 13,767 | ALLIGATOR CR | 0.4% | Patching - AC Deep | \$956 |
| CCRO::58TH AVE::80 | 58TH AVENUE | 27TH STREET | 26TH STREET | 17,313 | L & T CR | 3.5% | Crack Sealing - AC | \$603 |
| CCRO::58TH AVE::80 | 58TH AVENUE | 27TH STREET | 26TH STREET | 17,313 | L & T CR | 0.6% | Crack Sealing - AC | \$97 |
| CCRO::58TH AVE::80 | 58TH AVENUE | 27TH STREET | 26TH STREET | 17,313 | ALLIGATOR CR | 0.3% | Crack Sealing - AC | \$28 |
| CCRO::58TH CT::100 | 58TH COURT | 25TH STREET | 24TH STREET | 17,204 | L & T CR | 0.6% | Crack Sealing - AC | \$97 |
| CCRO::58TH CT::100 | 58TH COURT | 25TH STREET | 24TH STREET | 17,204 | L & T CR | 2.4% | Crack Sealing - AC | \$407 |
| CCRO::58TH CT::100 | 58TH COURT | 25TH STREET | 24TH STREET | 17,204 | ALLIGATOR CR | 0.9% | Crack Sealing - AC | \$62 |
| CCRO::58TH CT::100 | 58TH COURT | 25TH STREET | 24TH STREET | 17,204 | ALLIGATOR CR | 3.9% | Patching - AC Deep | \$8,616 |
| CCRO::58TH CT::120 | 58TH COURT | 23RD STREET | 22ND STREET | 16,889 | L & T CR | 0.3% | Crack Sealing - AC | \$57 |
| CCRO::58TH CT::120 | 58TH COURT | 23RD STREET | 22ND STREET | 16,889 | ALLIGATOR CR | 0.3% | Crack Sealing - AC | \$23 |
| CCRO::58TH CT::120 | 58TH COURT | 23RD STREET | 22ND STREET | 16,889 | L & T CR | 0.4% | Crack Sealing - AC | \$58 |
| CCRO::58TH CT::150 | 58TH COURT | 19TH STREET | 18TH STREET | 17,576 | L & T CR | 2.4% | Crack Sealing - AC | \$419 |
| CCRO::58TH CT::150 | 58TH COURT | 19TH STREET | 18TH STREET | 17,576 | L & T CR | 0.2% | Crack Sealing - AC | \$39 |
| CCRO::58TH CT::150 | 58TH COURT | 19TH STREET | 18TH STREET | 17,576 | ALLIGATOR CR | 1.4% | Crack Sealing - AC | \$95 |
| CCRO::58TH CT::150 | 58TH COURT | 19TH STREET | 18TH STREET | 17,576 | ALLIGATOR CR | 1.8% | Patching - AC Deep | \$4,245 |
| CCRO::58TH CT::50 | 58TH COURT | 35TH STREET | 34TH STREET | 17,232 | L & T CR | 0.6% | Crack Sealing - AC | \$97 |
| CCRO::58TH CT::50 | 58TH COURT | 35TH STREET | 34TH STREET | 17,232 | ALLIGATOR CR | 0.8% | Crack Sealing - AC | \$57 |
| CCRO::58TH CT::50 | 58TH COURT | 35TH STREET | 34TH STREET | 17,232 | RUTTING | 0.0% | Patching - AC Shallow | \$17 |
| CCRO::58TH CT::70 | 58TH COURT | 28TH STREET | 27TH STREET | 17,322 | L & T CR | 0.4% | Crack Sealing - AC | \$60 |
| CCRO::58TH CT::70 | 58TH COURT | 28TH STREET | 27TH STREET | 17,322 | ALLIGATOR CR | 0.1% | Crack Sealing - AC | \$8 |
| CCRO::58TH CT::70 | 58TH COURT | 28TH STREET | 27TH STREET | 17,322 | L & T CR | 2.0% | Crack Sealing - AC | \$350 |
| CCRO::58TH CT::80 | 58TH COURT | 27TH STREET | 26TH STREET | 17,130 | L & T CR | 1.0% | Crack Sealing - AC | \$174 |
| CCRO::58TH CT::80 | 58TH COURT | 27TH STREET | 26TH STREET | 17,130 | ALLIGATOR CR | 0.3% | Crack Sealing - AC | \$25 |
| CCRO::58TH CT::80 | 58TH COURT | 27TH STREET | 26TH STREET | 17,130 | L & T CR | 1.4% | Crack Sealing - AC | \$231 |
| CCRO::58TH CT::80 | 58TH COURT | 27TH STREET | 26TH STREET | 17,130 | ALLIGATOR CR | 1.8% | Patching - AC Deep | \$4,162 |
| CCRO::59TH AVE::30 | 59TH AVENUE | 37TH STREET | 36TH STREET | 18,221 | ALLIGATOR CR | 0.1% | Crack Sealing - AC | \$11 |
| CCRO::59TH AVE::30 | 59TH AVENUE | 37TH STREET | 36TH STREET | 18,221 | L & T CR | 1.7% | Crack Sealing - AC | \$316 |
| CCRO::59TH AVE::30 | 59TH AVENUE | 37TH STREET | 36TH STREET | 18,221 | RUTTING | 0.0% | Patching - AC Shallow | \$31 |
| CCRO::59TH AVE::40 | 59TH AVENUE | 36TH STREET | 35TH STREET | 16,274 | L & T CR | 0.7% | Crack Sealing - AC | \$112 |
| CCRO::59TH CT::100 | 59TH COURT | 26TH STREET | 25TH STREET | 17,390 | L & T CR | 0.6% | Crack Sealing - AC | \$99 |
| CCRO::59TH CT::100 | 59TH COURT | 26TH STREET | 25TH STREET | 17,390 | ALLIGATOR CR | 0.5% | Crack Sealing - AC | \$38 |
| CCRO::59TH CT::100 | 59TH COURT | 26TH STREET | 25TH STREET | 17,390 | L & T CR | 0.1% | Crack Sealing - AC | \$20 |
| CCRO::59TH CT::120 | 59TH COURT | 24TH STREET | 23RD STREET | 17,300 | ALLIGATOR CR | 1.6% | Crack Sealing - AC | \$105 |
| CCRO::59TH CT::120 | 59TH COURT | 24TH STREET | 23RD STREET | 17,300 | L & T CR | 0.5% | Crack Sealing - AC | \$80 |
| CCRO::59TH CT::120 | 59TH COURT | 24TH STREET | 23RD STREET | 17,300 | L & T CR | 3.2% | Crack Sealing - AC | \$548 |
| CCRO::59TH CT::120 | 59TH COURT | 24TH STREET | 23RD STREET | 17,300 | ALLIGATOR CR | 0.8% | Patching - AC Deep | \$2,096 |
| CCRO::59TH CT::150 | 59TH COURT | 21ST STREET | 19TH STREET | 17,310 | L & T CR | 2.6% | Crack Sealing - AC | \$457 |

| Pavement ID | Road Name | From | To | Area | Distress Type | Density | Maint. Activity | Cost |
|---------------------|-------------|-------------|----------------|--------|---------------|---------|-----------------------|---------|
| CCRO::59TH CT::150 | 59TH COURT | 21ST STREET | 19TH STREET | 17,310 | L & T CR | 0.4% | Crack Sealing - AC | \$60 |
| CCRO::59TH CT::150 | 59TH COURT | 21ST STREET | 19TH STREET | 17,310 | ALLIGATOR CR | 1.4% | Crack Sealing - AC | \$91 |
| CCRO::59TH CT::150 | 59TH COURT | 21ST STREET | 19TH STREET | 17,310 | ALLIGATOR CR | 0.4% | Patching - AC Deep | \$1,159 |
| CCRO::59TH CT::40 | 59TH COURT | 36TH STREET | 35TH STREET | 16,390 | ALLIGATOR CR | 0.0% | Crack Sealing - AC | \$4 |
| CCRO::60TH CT::10 | 60TH COURT | 39TH STREET | 38TH STREET | 17,443 | ALLIGATOR CR | 0.9% | Crack Sealing - AC | \$62 |
| CCRO::60TH CT::10 | 60TH COURT | 39TH STREET | 38TH STREET | 17,443 | L & T CR | 2.8% | Crack Sealing - AC | \$495 |
| CCRO::60TH CT::10 | 60TH COURT | 39TH STREET | 38TH STREET | 17,443 | ALLIGATOR CR | 0.9% | Patching - AC Deep | \$2,300 |
| CCRO::60TH CT::150 | 60TH COURT | 22ND STREET | 21ST STREET | 17,760 | L & T CR | 1.5% | Crack Sealing - AC | \$267 |
| CCRO::60TH CT::150 | 60TH COURT | 22ND STREET | 21ST STREET | 17,760 | ALLIGATOR CR | 1.7% | Crack Sealing - AC | \$116 |
| CCRO::60TH CT::150 | 60TH COURT | 22ND STREET | 21ST STREET | 17,760 | L & T CR | 1.2% | Crack Sealing - AC | \$213 |
| CCRO::60TH CT::150 | 60TH COURT | 22ND STREET | 21ST STREET | 17,760 | PATCH/UT CUT | 1.0% | Patching - AC Deep | \$2,519 |
| CCRO::60TH CT::150 | 60TH COURT | 22ND STREET | 21ST STREET | 17,760 | ALLIGATOR CR | 0.1% | Patching - AC Deep | \$329 |
| CCRO::60TH CT::150 | 60TH COURT | 22ND STREET | 21ST STREET | 17,760 | RUTTING | 0.2% | Patching - AC Shallow | \$168 |
| CCRO::60TH CT::160 | 60TH COURT | 21ST STREET | 19TH STREET | 14,939 | L & T CR | 0.5% | Crack Sealing - AC | \$72 |
| CCRO::60TH CT::160 | 60TH COURT | 21ST STREET | 19TH STREET | 14,939 | L & T CR | 2.8% | Crack Sealing - AC | \$410 |
| CCRO::60TH CT::160 | 60TH COURT | 21ST STREET | 19TH STREET | 14,939 | ALLIGATOR CR | 0.6% | Crack Sealing - AC | \$42 |
| CCRO::60TH CT::160 | 60TH COURT | 21ST STREET | 19TH STREET | 14,939 | ALLIGATOR CR | 0.2% | Patching - AC Deep | \$562 |
| CCRO::60TH CT::170 | 60TH COURT | 19TH STREET | 19TH STREET | 3,668 | L & T CR | 1.7% | Crack Sealing - AC | \$63 |
| CCRO::60TH CT::170 | 60TH COURT | 19TH STREET | 19TH STREET | 3,668 | ALLIGATOR CR | 1.9% | Crack Sealing - AC | \$32 |
| CCRO::60TH CT::170 | 60TH COURT | 19TH STREET | 19TH STREET | 3,668 | ALLIGATOR CR | 0.7% | Patching - AC Deep | \$540 |
| CCRO::60TH CT::20 | 60TH COURT | 38TH STREET | 37TH STREET | 17,137 | ALLIGATOR CR | 0.7% | Crack Sealing - AC | \$50 |
| CCRO::60TH CT::20 | 60TH COURT | 38TH STREET | 37TH STREET | 17,137 | L & T CR | 3.3% | Crack Sealing - AC | \$564 |
| CCRO::60TH CT::200 | 60TH COURT | 16TH STREET | 15TH STREET | 17,199 | ALLIGATOR CR | 0.5% | Crack Sealing - AC | \$40 |
| CCRO::60TH CT::200 | 60TH COURT | 16TH STREET | 15TH STREET | 17,199 | L & T CR | 0.2% | Crack Sealing - AC | \$40 |
| CCRO::60TH CT::200 | 60TH COURT | 16TH STREET | 15TH STREET | 17,199 | ALLIGATOR CR | 0.1% | Patching - AC Deep | \$339 |
| CCRO::60TH CT::200 | 60TH COURT | 16TH STREET | 15TH STREET | 17,199 | RUTTING | 0.0% | Patching - AC Shallow | \$17 |
| CCRO::60TH CT::210 | 60TH COURT | 15TH STREET | 14TH STREET | 16,955 | ALLIGATOR CR | 0.5% | Crack Sealing - AC | \$41 |
| CCRO::60TH CT::220 | 60TH COURT | 14TH STREET | 13TH STREET | 17,323 | BLOCK CR | 3.7% | Crack Sealing - AC | \$196 |
| CCRO::60TH CT::220 | 60TH COURT | 14TH STREET | 13TH STREET | 17,323 | L & T CR | 0.2% | Crack Sealing - AC | \$40 |
| CCRO::60TH CT::220 | 60TH COURT | 14TH STREET | 13TH STREET | 17,323 | ALLIGATOR CR | 0.2% | Crack Sealing - AC | \$19 |
| CCRO::60TH CT::220 | 60TH COURT | 14TH STREET | 13TH STREET | 17,323 | ALLIGATOR CR | 0.3% | Patching - AC Deep | \$891 |
| CCRO::60TH CT::230 | 60TH COURT | 13TH STREET | ROOSEVELT ROAD | 17,492 | L & T CR | 0.9% | Crack Sealing - AC | \$157 |
| CCRO::60TH CT::70 | 60TH COURT | 31ST STREET | END | 16,712 | L & T CR | 0.6% | Crack Sealing - AC | \$100 |
| CCRO::60TH CT::70 | 60TH COURT | 31ST STREET | END | 16,712 | ALLIGATOR CR | 0.2% | Crack Sealing - AC | \$18 |
| CCRO::61ST AVE::150 | 61ST AVENUE | 23RD STREET | 22ND STREET | 16,717 | ALLIGATOR CR | 0.6% | Crack Sealing - AC | \$43 |
| CCRO::61ST AVE::150 | 61ST AVENUE | 23RD STREET | 22ND STREET | 16,717 | ALLIGATOR CR | 0.2% | Patching - AC Deep | \$636 |
| CCRO::61ST AVE::180 | 61ST AVENUE | 19TH STREET | 19TH STREET | 2,984 | ALLIGATOR CR | 0.5% | Crack Sealing - AC | \$10 |
| CCRO::61ST AVE::180 | 61ST AVENUE | 19TH STREET | 19TH STREET | 2,984 | L & T CR | 1.4% | Crack Sealing - AC | \$41 |
| CCRO::61ST AVE::180 | 61ST AVENUE | 19TH STREET | 19TH STREET | 2,984 | ALLIGATOR CR | 1.2% | Patching - AC Deep | \$721 |
| CCRO::61ST AVE::90 | 61ST AVENUE | 31ST STREET | END | 14,467 | ALLIGATOR CR | 0.8% | Crack Sealing - AC | \$50 |

| Pavement ID | Road Name | From | To | Area | Distress Type | Density | Maint. Activity | Cost |
|----------------------|------------------|--------------|----------------|--------|---------------|---------|------------------------|---------|
| CCRO::61ST AVE::90 | 61ST AVENUE | 31ST STREET | END | 14,467 | BLOCK CR | 16.9% | Crack Sealing - AC | \$744 |
| CCRO::61ST AVE::90 | 61ST AVENUE | 31ST STREET | END | 14,467 | L & T CR | 4.0% | Crack Sealing - AC | \$585 |
| CCRO::61ST AVE::90 | 61ST AVENUE | 31ST STREET | END | 14,467 | ALLIGATOR CR | 0.3% | Patching - AC Deep | \$741 |
| CCRO::61ST CT::100 | 61ST COURT | 28TH STREET | 27TH STREET | 17,495 | ALLIGATOR CR | 0.1% | Crack Sealing - AC | \$12 |
| CCRO::61ST CT::100 | 61ST COURT | 28TH STREET | 27TH STREET | 17,495 | L & T CR | 0.3% | Crack Sealing - AC | \$59 |
| CCRO::61ST CT::100 | 61ST COURT | 28TH STREET | 27TH STREET | 17,495 | L & T CR | 1.5% | Crack Sealing - AC | \$256 |
| CCRO::61ST CT::100 | 61ST COURT | 28TH STREET | 27TH STREET | 17,495 | ALLIGATOR CR | 0.6% | Patching - AC Deep | \$1,685 |
| CCRO::61ST CT::60 | 61ST COURT | 34TH STREET | OGDEN AVENUE | 18,899 | L & T CR | 8.9% | Crack Sealing - AC | \$1,683 |
| CCRO::61ST CT::80 | 61ST COURT | 31ST STREET | END | 9,618 | RUTTING | 1.0% | Patching - AC Shallow | \$501 |
| CCRO::ASTN BLVD::100 | AUSTIN BOULEVARD | OGDEN AVENUE | 32ND STREET | 28,964 | ALLIGATOR CR | 0.5% | Crack Sealing - AC | \$64 |
| CCRO::ASTN BLVD::100 | AUSTIN BOULEVARD | OGDEN AVENUE | 32ND STREET | 28,964 | L & T CR | 1.4% | Crack Sealing - AC | \$397 |
| CCRO::ASTN BLVD::100 | AUSTIN BOULEVARD | OGDEN AVENUE | 32ND STREET | 28,964 | L & T CR | 0.3% | Crack Sealing - AC | \$98 |
| CCRO::ASTN BLVD::100 | AUSTIN BOULEVARD | OGDEN AVENUE | 32ND STREET | 28,964 | ALLIGATOR CR | 1.1% | Patching - AC Deep | \$4,171 |
| CCRO::ASTN BLVD::100 | AUSTIN BOULEVARD | OGDEN AVENUE | 32ND STREET | 28,964 | RUTTING | 0.0% | Patching - AC Deep | \$82 |
| CCRO::ASTN BLVD::100 | AUSTIN BOULEVARD | OGDEN AVENUE | 32ND STREET | 28,964 | RUTTING | 0.1% | Patching - AC Shallow | \$188 |
| CCRO::ASTN BLVD::190 | AUSTIN BOULEVARD | 22ND STREET | 21ST STREET | 27,327 | L & T CR | 0.3% | Crack Sealing - AC | \$82 |
| CCRO::ASTN BLVD::190 | AUSTIN BOULEVARD | 22ND STREET | 21ST STREET | 27,327 | L & T CR | 0.2% | Crack Sealing - AC | \$55 |
| CCRO::ASTN BLVD::200 | AUSTIN BOULEVARD | 21ST STREET | 19TH STREET | 22,831 | L & T CR | 0.1% | Crack Sealing - AC | \$28 |
| CCRO::ASTN BLVD::230 | AUSTIN BOULEVARD | 19TH STREET | 18TH STREET | 23,396 | L & T CR | 0.6% | Crack Sealing - AC | \$138 |
| CCRO::ASTN BLVD::240 | AUSTIN BOULEVARD | 18TH STREET | 16TH STREET | 26,606 | L & T CR | 0.4% | Crack Sealing - AC | \$94 |
| CCRO::ASTN BLVD::250 | AUSTIN BOULEVARD | 16TH STREET | 15TH STREET | 26,419 | L & T CR | 0.8% | Crack Sealing - AC | \$213 |
| CCRO::ASTN BLVD::260 | AUSTIN BOULEVARD | 15TH STREET | 14TH STREET | 26,632 | L & T CR | 0.6% | Crack Sealing - AC | \$154 |
| CCRO::ASTN BLVD::270 | AUSTIN BOULEVARD | 14TH STREET | 13TH STREET | 26,930 | ALLIGATOR CR | 0.1% | Crack Sealing - AC | \$16 |
| CCRO::ASTN BLVD::270 | AUSTIN BOULEVARD | 14TH STREET | 13TH STREET | 26,930 | L & T CR | 0.6% | Crack Sealing - AC | \$155 |
| CCRO::ASTN BLVD::280 | AUSTIN BOULEVARD | 13TH STREET | ROOSEVELT ROAD | 26,311 | L & T CR | 0.6% | Crack Sealing - AC | \$153 |
| CCRO::CNTRL AVE::10 | CENTRAL AVENUE | 39TH STREET | 38TH STREET | 14,697 | JT SEAL DMG | 100.0% | Joint Seal (Localized) | \$2,988 |
| CCRO::CNTRL AVE::100 | CENTRAL AVENUE | 31ST STREET | OGDEN AVENUE | 14,722 | RUTTING | 0.0% | Patching - AC Deep | \$24 |
| CCRO::CNTRL AVE::100 | CENTRAL AVENUE | 31ST STREET | OGDEN AVENUE | 14,722 | RUTTING | 0.2% | Patching - AC Shallow | \$161 |
| CCRO::CNTRL AVE::110 | CENTRAL AVENUE | 26TH STREET | 25TH PLACE | 15,812 | ALLIGATOR CR | 2.7% | Crack Sealing - AC | \$154 |
| CCRO::CNTRL AVE::110 | CENTRAL AVENUE | 26TH STREET | 25TH PLACE | 15,812 | L & T CR | 3.3% | Crack Sealing - AC | \$514 |
| CCRO::CNTRL AVE::110 | CENTRAL AVENUE | 26TH STREET | 25TH PLACE | 15,812 | L & T CR | 2.5% | Crack Sealing - AC | \$389 |
| CCRO::CNTRL AVE::110 | CENTRAL AVENUE | 26TH STREET | 25TH PLACE | 15,812 | RUTTING | 0.1% | Patching - AC Shallow | \$80 |
| CCRO::CNTRL AVE::120 | CENTRAL AVENUE | 25TH PLACE | 25TH STREET | 15,509 | L & T CR | 1.7% | Crack Sealing - AC | \$268 |
| CCRO::CNTRL AVE::120 | CENTRAL AVENUE | 25TH PLACE | 25TH STREET | 15,509 | ALLIGATOR CR | 0.7% | Crack Sealing - AC | \$48 |
| CCRO::CNTRL AVE::120 | CENTRAL AVENUE | 25TH PLACE | 25TH STREET | 15,509 | L & T CR | 2.4% | Crack Sealing - AC | \$367 |
| CCRO::CNTRL AVE::120 | CENTRAL AVENUE | 25TH PLACE | 25TH STREET | 15,509 | ALLIGATOR CR | 0.4% | Patching - AC Deep | \$959 |
| CCRO::CNTRL AVE::130 | CENTRAL AVENUE | 25TH STREET | 24TH PLACE | 16,067 | BLOCK CR | 94.9% | Crack Sealing - AC | \$4,649 |
| CCRO::CNTRL AVE::130 | CENTRAL AVENUE | 25TH STREET | 24TH PLACE | 16,067 | RUTTING | 0.1% | Patching - AC Shallow | \$80 |
| CCRO::CNTRL AVE::150 | CENTRAL AVENUE | 24TH STREET | 23RD PLACE | 15,797 | L & T CR | 2.4% | Crack Sealing - AC | \$385 |
| CCRO::CNTRL AVE::150 | CENTRAL AVENUE | 24TH STREET | 23RD PLACE | 15,797 | ALLIGATOR CR | 0.2% | Crack Sealing - AC | \$15 |

| Pavement ID | Road Name | From | To | Area | Distress Type | Density | Maint. Activity | Cost |
|----------------------|----------------|-------------|-------------|--------|---------------|---------|------------------------|---------|
| CCRO::CNTRL AVE::150 | CENTRAL AVENUE | 24TH STREET | 23RD PLACE | 15,797 | L & T CR | 1.2% | Crack Sealing - AC | \$190 |
| CCRO::CNTRL AVE::150 | CENTRAL AVENUE | 24TH STREET | 23RD PLACE | 15,797 | ALLIGATOR CR | 1.0% | Patching - AC Deep | \$2,376 |
| CCRO::CNTRL AVE::150 | CENTRAL AVENUE | 24TH STREET | 23RD PLACE | 15,797 | RUTTING | 0.0% | Patching - AC Shallow | \$28 |
| CCRO::CNTRL AVE::160 | CENTRAL AVENUE | 23RD PLACE | 23RD STREET | 15,501 | ALLIGATOR CR | 1.5% | Crack Sealing - AC | \$92 |
| CCRO::CNTRL AVE::160 | CENTRAL AVENUE | 23RD PLACE | 23RD STREET | 15,501 | L & T CR | 2.7% | Crack Sealing - AC | \$415 |
| CCRO::CNTRL AVE::160 | CENTRAL AVENUE | 23RD PLACE | 23RD STREET | 15,501 | L & T CR | 1.4% | Crack Sealing - AC | \$223 |
| CCRO::CNTRL AVE::170 | CENTRAL AVENUE | 23RD STREET | 22ND PLACE | 15,824 | ALLIGATOR CR | 0.2% | Crack Sealing - AC | \$20 |
| CCRO::CNTRL AVE::170 | CENTRAL AVENUE | 23RD STREET | 22ND PLACE | 15,824 | L & T CR | 0.2% | Crack Sealing - AC | \$33 |
| CCRO::CNTRL AVE::170 | CENTRAL AVENUE | 23RD STREET | 22ND PLACE | 15,824 | L & T CR | 2.2% | Crack Sealing - AC | \$350 |
| CCRO::CNTRL AVE::180 | CENTRAL AVENUE | 22ND PLACE | 22ND STREET | 14,714 | ALLIGATOR CR | 1.5% | Crack Sealing - AC | \$84 |
| CCRO::CNTRL AVE::180 | CENTRAL AVENUE | 22ND PLACE | 22ND STREET | 14,714 | L & T CR | 1.7% | Crack Sealing - AC | \$255 |
| CCRO::CNTRL AVE::190 | CENTRAL AVENUE | 22ND STREET | 21ST PLACE | 12,447 | EDGE CR | 2.9% | Crack Sealing - AC | \$358 |
| CCRO::CNTRL AVE::190 | CENTRAL AVENUE | 22ND STREET | 21ST PLACE | 12,447 | ALLIGATOR CR | 0.4% | Crack Sealing - AC | \$24 |
| CCRO::CNTRL AVE::20 | CENTRAL AVENUE | 39TH STREET | 38TH STREET | 16,768 | JT SEAL DMG | 100.0% | Joint Seal (Localized) | \$3,419 |
| CCRO::CNTRL AVE::200 | CENTRAL AVENUE | 21ST PLACE | 21ST STREET | 14,891 | L & T CR | 0.4% | Crack Sealing - AC | \$64 |
| CCRO::CNTRL AVE::200 | CENTRAL AVENUE | 21ST PLACE | 21ST STREET | 14,891 | RUTTING | 0.0% | Patching - AC Shallow | \$27 |
| CCRO::CNTRL AVE::210 | CENTRAL AVENUE | 21ST STREET | 19TH STREET | 20,188 | L & T CR | 3.9% | Crack Sealing - AC | \$792 |
| CCRO::CNTRL AVE::210 | CENTRAL AVENUE | 21ST STREET | 19TH STREET | 20,188 | L & T CR | 0.4% | Crack Sealing - AC | \$83 |
| CCRO::CNTRL AVE::210 | CENTRAL AVENUE | 21ST STREET | 19TH STREET | 20,188 | ALLIGATOR CR | 1.5% | Patching - AC Deep | \$4,067 |
| CCRO::CNTRL AVE::220 | CENTRAL AVENUE | 19TH STREET | 19TH STREET | 6,390 | L & T CR | 2.8% | Crack Sealing - AC | \$176 |
| CCRO::CNTRL AVE::220 | CENTRAL AVENUE | 19TH STREET | 19TH STREET | 6,390 | L & T CR | 0.4% | Crack Sealing - AC | \$22 |
| CCRO::CNTRL AVE::230 | CENTRAL AVENUE | 19TH STREET | 18TH STREET | 26,630 | L & T CR | 0.5% | Crack Sealing - AC | \$123 |
| CCRO::CNTRL AVE::230 | CENTRAL AVENUE | 19TH STREET | 18TH STREET | 26,630 | L & T CR | 4.2% | Crack Sealing - AC | \$1,120 |
| CCRO::CNTRL AVE::230 | CENTRAL AVENUE | 19TH STREET | 18TH STREET | 26,630 | ALLIGATOR CR | 0.4% | Crack Sealing - AC | \$45 |
| CCRO::CNTRL AVE::230 | CENTRAL AVENUE | 19TH STREET | 18TH STREET | 26,630 | ALLIGATOR CR | 0.2% | Patching - AC Deep | \$1,119 |
| CCRO::CNTRL AVE::230 | CENTRAL AVENUE | 19TH STREET | 18TH STREET | 26,630 | RUTTING | 0.0% | Patching - AC Shallow | \$26 |
| CCRO::CNTRL AVE::240 | CENTRAL AVENUE | 18TH STREET | 16TH STREET | 27,637 | L & T CR | 2.6% | Crack Sealing - AC | \$709 |
| CCRO::CNTRL AVE::240 | CENTRAL AVENUE | 18TH STREET | 16TH STREET | 27,637 | L & T CR | 0.2% | Crack Sealing - AC | \$55 |
| CCRO::CNTRL AVE::240 | CENTRAL AVENUE | 18TH STREET | 16TH STREET | 27,637 | ALLIGATOR CR | 0.7% | Crack Sealing - AC | \$76 |
| CCRO::CNTRL AVE::240 | CENTRAL AVENUE | 18TH STREET | 16TH STREET | 27,637 | ALLIGATOR CR | 1.1% | Patching - AC Deep | \$4,216 |
| CCRO::CNTRL AVE::240 | CENTRAL AVENUE | 18TH STREET | 16TH STREET | 27,637 | RUTTING | 0.0% | Patching - AC Shallow | \$24 |
| CCRO::CNTRL AVE::270 | CENTRAL AVENUE | 15TH STREET | 14TH STREET | 26,288 | L & T CR | 2.2% | Crack Sealing - AC | \$566 |
| CCRO::CNTRL AVE::270 | CENTRAL AVENUE | 15TH STREET | 14TH STREET | 26,288 | ALLIGATOR CR | 0.4% | Crack Sealing - AC | \$48 |
| CCRO::CNTRL AVE::270 | CENTRAL AVENUE | 15TH STREET | 14TH STREET | 26,288 | L & T CR | 1.8% | Crack Sealing - AC | \$474 |
| CCRO::CNTRL AVE::270 | CENTRAL AVENUE | 15TH STREET | 14TH STREET | 26,288 | ALLIGATOR CR | 0.7% | Patching - AC Deep | \$2,660 |
| CCRO::CNTRL AVE::280 | CENTRAL AVENUE | 14TH STREET | 13TH STREET | 26,664 | ALLIGATOR CR | 0.3% | Crack Sealing - AC | \$33 |
| CCRO::CNTRL AVE::280 | CENTRAL AVENUE | 14TH STREET | 13TH STREET | 26,664 | L & T CR | 2.2% | Crack Sealing - AC | \$578 |
| CCRO::CNTRL AVE::280 | CENTRAL AVENUE | 14TH STREET | 13TH STREET | 26,664 | L & T CR | 1.2% | Crack Sealing - AC | \$330 |
| CCRO::CNTRL AVE::280 | CENTRAL AVENUE | 14TH STREET | 13TH STREET | 26,664 | ALLIGATOR CR | 0.9% | Patching - AC Deep | \$3,442 |
| CCRO::CNTRL AVE::280 | CENTRAL AVENUE | 14TH STREET | 13TH STREET | 26,664 | PATCH/UT CUT | 1.0% | Patching - AC Deep | \$3,721 |

| Pavement ID | Road Name | From | To | Area | Distress Type | Density | Maint. Activity | Cost |
|----------------------|----------------|--------------|----------------|--------|---------------|---------|------------------------|----------|
| CCRO::CNTRL AVE::290 | CENTRAL AVENUE | 13TH STREET | ROOSEVELT ROAD | 26,932 | L & T CR | 0.5% | Crack Sealing - AC | \$121 |
| CCRO::CNTRL AVE::290 | CENTRAL AVENUE | 13TH STREET | ROOSEVELT ROAD | 26,932 | ALLIGATOR CR | 0.9% | Crack Sealing - AC | \$96 |
| CCRO::CNTRL AVE::290 | CENTRAL AVENUE | 13TH STREET | ROOSEVELT ROAD | 26,932 | L & T CR | 1.5% | Crack Sealing - AC | \$395 |
| CCRO::CNTRL AVE::290 | CENTRAL AVENUE | 13TH STREET | ROOSEVELT ROAD | 26,932 | ALLIGATOR CR | 0.3% | Patching - AC Deep | \$1,153 |
| CCRO::CNTRL AVE::290 | CENTRAL AVENUE | 13TH STREET | ROOSEVELT ROAD | 26,932 | PATCH/UT CUT | 1.3% | Patching - AC Deep | \$4,627 |
| CCRO::CNTRL AVE::290 | CENTRAL AVENUE | 13TH STREET | ROOSEVELT ROAD | 26,932 | RUTTING | 0.0% | Patching - AC Shallow | \$49 |
| CCRO::CNTRL AVE::30 | CENTRAL AVENUE | 38TH STREET | 37TH STREET | 31,394 | JT SEAL DMG | 100.0% | Joint Seal (Localized) | \$6,462 |
| CCRO::CNTRL AVE::60 | CENTRAL AVENUE | 35TH STREET | 34TH STREET | 31,326 | L & T CR | 0.2% | Crack Sealing - AC | \$72 |
| CCRO::CNTRL AVE::60 | CENTRAL AVENUE | 35TH STREET | 34TH STREET | 31,326 | ALLIGATOR CR | 0.5% | Crack Sealing - AC | \$68 |
| CCRO::CNTRL AVE::60 | CENTRAL AVENUE | 35TH STREET | 34TH STREET | 31,326 | L & T CR | 2.1% | Crack Sealing - AC | \$660 |
| CCRO::CNTRL AVE::60 | CENTRAL AVENUE | 35TH STREET | 34TH STREET | 31,326 | ALLIGATOR CR | 0.4% | Patching - AC Deep | \$1,699 |
| CCRO::CNTRL AVE::60 | CENTRAL AVENUE | 35TH STREET | 34TH STREET | 31,326 | RUTTING | 0.0% | Patching - AC Deep | \$68 |
| CCRO::CNTRL AVE::60 | CENTRAL AVENUE | 35TH STREET | 34TH STREET | 31,326 | RUTTING | 1.2% | Patching - AC Shallow | \$2,125 |
| CCRO::CNTRL AVE::70 | CENTRAL AVENUE | 34TH STREET | PARK AVENUE | 17,760 | L & T CR | 1.8% | Crack Sealing - AC | \$316 |
| CCRO::CNTRL AVE::70 | CENTRAL AVENUE | 34TH STREET | PARK AVENUE | 17,760 | ALLIGATOR CR | 0.1% | Crack Sealing - AC | \$14 |
| CCRO::CNTRL AVE::70 | CENTRAL AVENUE | 34TH STREET | PARK AVENUE | 17,760 | L & T CR | 2.2% | Crack Sealing - AC | \$385 |
| CCRO::CNTRL AVE::70 | CENTRAL AVENUE | 34TH STREET | PARK AVENUE | 17,760 | RUTTING | 0.0% | Patching - AC Shallow | \$28 |
| CCRO::CNTRL AVE::80 | CENTRAL AVENUE | PARK AVENUE | 32ND STREET | 37,990 | L & T CR | 0.1% | Crack Sealing - AC | \$54 |
| CCRO::CNTRL AVE::80 | CENTRAL AVENUE | PARK AVENUE | 32ND STREET | 37,990 | L & T CR | 0.5% | Crack Sealing - AC | \$194 |
| CCRO::CNTRL AVE::80 | CENTRAL AVENUE | PARK AVENUE | 32ND STREET | 37,990 | ALLIGATOR CR | 0.6% | Crack Sealing - AC | \$84 |
| CCRO::CNTRL AVE::80 | CENTRAL AVENUE | PARK AVENUE | 32ND STREET | 37,990 | RUTTING | 0.0% | Patching - AC Deep | \$24 |
| CCRO::CNTRL AVE::80 | CENTRAL AVENUE | PARK AVENUE | 32ND STREET | 37,990 | ALLIGATOR CR | 0.5% | Patching - AC Deep | \$2,826 |
| CCRO::CNTRL AVE::80 | CENTRAL AVENUE | PARK AVENUE | 32ND STREET | 37,990 | RUTTING | 0.1% | Patching - AC Shallow | \$243 |
| CCRO::CNTRL AVE::90 | CENTRAL AVENUE | 32ND STREET | 31ST STREET | 26,666 | L & T CR | 3.9% | Crack Sealing - AC | \$1,026 |
| CCRO::LMBRD AVE::130 | LOMBARD AVENUE | 28TH STREET | 29TH STREET | 21,892 | EDGE CR | 0.5% | Crack Sealing - AC | \$104 |
| CCRO::LMBRD AVE::130 | LOMBARD AVENUE | 28TH STREET | 29TH STREET | 21,892 | ALLIGATOR CR | 0.4% | Crack Sealing - AC | \$40 |
| CCRO::LMBRD AVE::130 | LOMBARD AVENUE | 28TH STREET | 29TH STREET | 21,892 | L & T CR | 1.0% | Crack Sealing - AC | \$225 |
| CCRO::LMBRD AVE::130 | LOMBARD AVENUE | 28TH STREET | 29TH STREET | 21,892 | L & T CR | 0.9% | Crack Sealing - AC | \$199 |
| CCRO::LMBRD AVE::130 | LOMBARD AVENUE | 28TH STREET | 29TH STREET | 21,892 | ALLIGATOR CR | 2.1% | Patching - AC Deep | \$6,121 |
| CCRO::LMBRD AVE::130 | LOMBARD AVENUE | 28TH STREET | 29TH STREET | 21,892 | RUTTING | 0.1% | Patching - AC Shallow | \$59 |
| CCRO::LMBRD AVE::130 | LOMBARD AVENUE | 28TH STREET | 29TH STREET | 21,892 | BLOCK CR | 1.8% | Patching - AC Shallow | \$2,107 |
| CCRO::LRM AVE::100 | LARAMIE AVENUE | 29TH PLACE | OGDEN AVENUE | 28,746 | L & T CR | 2.8% | Crack Sealing - AC | \$802 |
| CCRO::LRM AVE::100 | LARAMIE AVENUE | 29TH PLACE | OGDEN AVENUE | 28,746 | L & T CR | 1.1% | Crack Sealing - AC | \$328 |
| CCRO::LRM AVE::100 | LARAMIE AVENUE | 29TH PLACE | OGDEN AVENUE | 28,746 | EDGE CR | 0.3% | Crack Sealing - AC | \$97 |
| CCRO::LRM AVE::100 | LARAMIE AVENUE | 29TH PLACE | OGDEN AVENUE | 28,746 | ALLIGATOR CR | 0.2% | Crack Sealing - AC | \$31 |
| CCRO::LRM AVE::100 | LARAMIE AVENUE | 29TH PLACE | OGDEN AVENUE | 28,746 | ALLIGATOR CR | 2.5% | Patching - AC Deep | \$9,069 |
| CCRO::LRM AVE::100 | LARAMIE AVENUE | 29TH PLACE | OGDEN AVENUE | 28,746 | RUTTING | 0.0% | Patching - AC Shallow | \$57 |
| CCRO::LRM AVE::110 | LARAMIE AVENUE | OGDEN AVENUE | 26TH STREET | 54,563 | JT SEAL DMG | 100.0% | Joint Seal (Localized) | \$11,383 |
| CCRO::LRM AVE::180 | LARAMIE AVENUE | 22ND PLACE | 22ND STREET | 12,691 | L & T CR | 3.0% | Crack Sealing - AC | \$380 |
| CCRO::LRM AVE::180 | LARAMIE AVENUE | 22ND PLACE | 22ND STREET | 12,691 | L & T CR | 0.4% | Crack Sealing - AC | \$55 |

| Pavement ID | Road Name | From | To | Area | Distress Type | Density | Maint. Activity | Cost |
|--------------------|----------------|-------------|-------------|--------|---------------|---------|-----------------------|---------|
| CCRO::LRM AVE::180 | LARAMIE AVENUE | 22ND PLACE | 22ND STREET | 12,691 | ALLIGATOR CR | 0.7% | Patching - AC Deep | \$1,469 |
| CCRO::LRM AVE::180 | LARAMIE AVENUE | 22ND PLACE | 22ND STREET | 12,691 | RUTTING | 0.1% | Patching - AC Shallow | \$46 |
| CCRO::LRM AVE::200 | LARAMIE AVENUE | 21ST PLACE | 21ST STREET | 19,481 | ALLIGATOR CR | 1.5% | Crack Sealing - AC | \$110 |
| CCRO::LRM AVE::200 | LARAMIE AVENUE | 21ST PLACE | 21ST STREET | 19,481 | BLOCK CR | 27.9% | Crack Sealing - AC | \$1,656 |
| CCRO::LRM AVE::200 | LARAMIE AVENUE | 21ST PLACE | 21ST STREET | 19,481 | L & T CR | 1.9% | Crack Sealing - AC | \$372 |
| CCRO::LRM AVE::200 | LARAMIE AVENUE | 21ST PLACE | 21ST STREET | 19,481 | L & T CR | 3.7% | Crack Sealing - AC | \$718 |
| CCRO::LRM AVE::210 | LARAMIE AVENUE | 21ST STREET | 19TH STREET | 30,730 | BLOCK CR | 98.9% | Crack Sealing - AC | \$9,262 |
| CCRO::LRM AVE::220 | LARAMIE AVENUE | 19TH STREET | 18TH STREET | 31,478 | BLOCK CR | 98.1% | Crack Sealing - AC | \$9,412 |
| CCRO::LRM AVE::230 | LARAMIE AVENUE | 18TH STREET | 16TH STREET | 32,684 | BLOCK CR | 98.4% | Crack Sealing - AC | \$9,797 |
| CCRO::LRM AVE::230 | LARAMIE AVENUE | 18TH STREET | 16TH STREET | 32,684 | RUTTING | 0.0% | Patching - AC Shallow | \$80 |

APPENDIX F – PAVEMENT INVENTORY AND CONDITION TABULAR DATA

| Pavement ID | Road Name | From | To | Surface | Rank | Length (FT) | Width (FT) | Area (SF) | PCI | IRI |
|--------------------|-------------|------------------|------------------|---------|------|-------------|------------|-----------|-----|-----|
| CCRO::12TH PL::10 | 12TH PLACE | CICERO AVENUE | 47TH AVENUE | Asphalt | S | 671 | 26 | 17,435 | 24 | 540 |
| CCRO::12TH PL::20 | 12TH PLACE | 47TH AVENUE | END | Asphalt | S | 516 | 26 | 13,419 | 10 | 705 |
| CCRO::13TH ST::10 | 13TH STREET | LOMBARD AVENUE | 61ST COURT | Asphalt | S | 331 | 26 | 8,604 | 35 | 393 |
| CCRO::13TH ST::100 | 13TH STREET | 57TH COURT | 57TH AVENUE | Asphalt | S | 331 | 26 | 8,606 | 56 | 353 |
| CCRO::13TH ST::110 | 13TH STREET | 57TH AVENUE | 56TH COURT | Asphalt | S | 338 | 26 | 8,777 | 55 | 363 |
| CCRO::13TH ST::120 | 13TH STREET | 56TH COURT | CENTRAL AVENUE | Asphalt | S | 328 | 26 | 8,529 | 81 | 303 |
| CCRO::13TH ST::130 | 13TH STREET | LARAMIE AVENUE | 51ST COURT | Asphalt | S | 330 | 26 | 8,580 | 37 | 392 |
| CCRO::13TH ST::140 | 13TH STREET | 51ST COURT | 51ST AVENUE | Asphalt | S | 340 | 26 | 8,848 | 20 | 522 |
| CCRO::13TH ST::150 | 13TH STREET | 51ST AVENUE | 50TH COURT | Asphalt | S | 328 | 26 | 8,520 | 27 | 358 |
| CCRO::13TH ST::160 | 13TH STREET | 50TH COURT | 50TH AVENUE | Asphalt | S | 333 | 26 | 8,669 | 25 | 390 |
| CCRO::13TH ST::170 | 13TH STREET | 50TH AVENUE | 49TH COURT | Asphalt | S | 338 | 26 | 8,777 | 43 | 337 |
| CCRO::13TH ST::180 | 13TH STREET | 49TH COURT | 49TH AVENUE | Asphalt | S | 336 | 26 | 8,736 | 24 | 517 |
| CCRO::13TH ST::190 | 13TH STREET | 49TH AVENUE | 48TH COURT | Asphalt | S | 332 | 26 | 8,623 | 43 | 340 |
| CCRO::13TH ST::20 | 13TH STREET | 61ST COURT | 61ST AVENUE | Asphalt | S | 336 | 26 | 8,746 | 35 | 301 |
| CCRO::13TH ST::200 | 13TH STREET | 48TH COURT | CICERO AVENUE | Asphalt | S | 325 | 26 | 8,458 | 53 | 402 |
| CCRO::13TH ST::210 | 13TH STREET | CICERO AVENUE | 47TH AVENUE | Asphalt | S | 669 | 26 | 17,400 | 36 | 387 |
| CCRO::13TH ST::30 | 13TH STREET | 61ST AVENUE | 60TH COURT | Asphalt | S | 324 | 26 | 8,420 | 85 | 364 |
| CCRO::13TH ST::40 | 13TH STREET | 60TH COURT | AUSTIN BOULEVARD | Asphalt | S | 340 | 26 | 8,848 | 62 | 444 |
| CCRO::13TH ST::50 | 13TH STREET | AUSTIN BOULEVARD | 59TH COURT | Asphalt | S | 338 | 26 | 8,797 | 36 | 344 |
| CCRO::13TH ST::60 | 13TH STREET | 59TH COURT | 59TH AVENUE | Asphalt | S | 327 | 26 | 8,504 | 34 | 279 |
| CCRO::13TH ST::70 | 13TH STREET | 59TH AVENUE | 58TH COURT | Asphalt | S | 339 | 26 | 8,815 | 43 | 325 |
| CCRO::13TH ST::80 | 13TH STREET | 58TH COURT | 58TH AVENUE | Asphalt | S | 336 | 26 | 8,741 | 54 | 336 |
| CCRO::13TH ST::90 | 13TH STREET | 58TH AVENUE | 57TH COURT | Asphalt | S | 333 | 26 | 8,664 | 56 | 345 |
| CCRO::14TH ST::10 | 14TH STREET | LOMBARD AVENUE | 61ST COURT | Asphalt | S | 332 | 26 | 8,643 | 35 | 405 |
| CCRO::14TH ST::100 | 14TH STREET | 57TH COURT | 57TH AVENUE | Asphalt | S | 332 | 26 | 8,637 | 35 | 288 |
| CCRO::14TH ST::110 | 14TH STREET | 57TH AVENUE | 56TH COURT | Asphalt | S | 339 | 26 | 8,809 | 49 | 269 |
| CCRO::14TH ST::120 | 14TH STREET | 56TH COURT | CENTRAL AVENUE | Asphalt | S | 330 | 26 | 8,578 | 51 | 369 |
| CCRO::14TH ST::130 | 14TH STREET | CENTRAL AVENUE | 55TH COURT | Asphalt | S | 325 | 26 | 8,437 | 62 | 316 |
| CCRO::14TH ST::140 | 14TH STREET | LARAMIE AVENUE | 51ST COURT | Asphalt | S | 328 | 26 | 8,515 | 38 | 229 |
| CCRO::14TH ST::150 | 14TH STREET | 51ST COURT | 51ST AVENUE | Asphalt | S | 340 | 26 | 8,836 | 72 | 161 |
| CCRO::14TH ST::160 | 14TH STREET | 51ST AVENUE | 50TH COURT | Asphalt | S | 329 | 26 | 8,565 | 62 | 179 |
| CCRO::14TH ST::170 | 14TH STREET | 50TH COURT | 50TH AVENUE | Asphalt | S | 332 | 26 | 8,633 | 60 | 234 |
| CCRO::14TH ST::180 | 14TH STREET | 50TH AVENUE | 49TH COURT | Asphalt | S | 337 | 26 | 8,765 | 61 | 302 |
| CCRO::14TH ST::190 | 14TH STREET | 49TH COURT | 49TH AVENUE | Asphalt | S | 337 | 26 | 8,773 | 43 | 395 |
| CCRO::14TH ST::20 | 14TH STREET | 61ST COURT | 61ST AVENUE | Asphalt | S | 336 | 26 | 8,725 | 33 | 307 |
| CCRO::14TH ST::200 | 14TH STREET | 49TH AVENUE | 48TH COURT | Asphalt | S | 334 | 26 | 8,695 | 51 | 210 |
| CCRO::14TH ST::210 | 14TH STREET | 48TH COURT | CICERO AVENUE | Asphalt | S | 324 | 26 | 8,435 | 48 | 340 |
| CCRO::14TH ST::220 | 14TH STREET | CICERO AVENUE | 47TH AVENUE | Asphalt | S | 666 | 26 | 17,313 | 36 | 289 |
| CCRO::14TH ST::230 | 14TH STREET | 47TH AVENUE | END | Asphalt | S | 566 | 26 | 14,723 | 59 | 206 |
| CCRO::14TH ST::30 | 14TH STREET | 61ST AVENUE | 60TH COURT | Asphalt | S | 333 | 26 | 8,650 | 46 | 250 |
| CCRO::14TH ST::40 | 14TH STREET | 60TH COURT | AUSTIN BOULEVARD | Asphalt | S | 333 | 26 | 8,645 | 54 | 225 |
| CCRO::14TH ST::50 | 14TH STREET | AUSTIN BOULEVARD | 59TH COURT | Asphalt | S | 336 | 26 | 8,736 | 79 | 316 |
| CCRO::14TH ST::60 | 14TH STREET | 59TH COURT | 59TH AVENUE | Asphalt | S | 328 | 26 | 8,534 | 44 | 298 |

| Pavement ID | Road Name | From | To | Surface | Rank | Length (FT) | Width (FT) | Area (SF) | PCI | IRI |
|--------------------|-------------|------------------|------------------|----------|------|-------------|------------|-----------|-----|-----|
| CCRO::14TH ST::70 | 14TH STREET | 59TH AVENUE | 58TH COURT | Asphalt | S | 338 | 26 | 8,797 | 53 | 330 |
| CCRO::14TH ST::80 | 14TH STREET | 58TH COURT | 58TH AVENUE | Asphalt | S | 335 | 26 | 8,715 | 52 | 417 |
| CCRO::14TH ST::90 | 14TH STREET | 58TH AVENUE | 57TH COURT | Asphalt | S | 334 | 26 | 8,678 | 54 | 380 |
| CCRO::15TH PL::10 | 15TH PLACE | CENTRAL AVENUE | 55TH COURT | Concrete | S | 341 | 26 | 8,859 | 67 | 514 |
| CCRO::15TH PL::20 | 15TH PLACE | CICERO AVENUE | END | Asphalt | S | 479 | 26 | 12,463 | 52 | 296 |
| CCRO::15TH ST::10 | 15TH STREET | LOMBARD AVENUE | 61ST COURT | Asphalt | S | 334 | 26 | 8,681 | 47 | 322 |
| CCRO::15TH ST::100 | 15TH STREET | 57TH COURT | 57TH AVENUE | Asphalt | S | 333 | 26 | 8,664 | 61 | 352 |
| CCRO::15TH ST::110 | 15TH STREET | 57TH AVENUE | 56TH COURT | Asphalt | S | 340 | 26 | 8,838 | 48 | 366 |
| CCRO::15TH ST::120 | 15TH STREET | 56TH COURT | CENTRAL AVENUE | Asphalt | S | 332 | 26 | 8,630 | 37 | 267 |
| CCRO::15TH ST::130 | 15TH STREET | LARAMIE AVENUE | 51ST COURT | Asphalt | S | 329 | 26 | 8,547 | 68 | 204 |
| CCRO::15TH ST::140 | 15TH STREET | 51ST COURT | 51ST AVENUE | Asphalt | S | 338 | 26 | 8,795 | 48 | 257 |
| CCRO::15TH ST::150 | 15TH STREET | 51ST AVENUE | 50TH COURT | Asphalt | S | 331 | 26 | 8,615 | 44 | 243 |
| CCRO::15TH ST::160 | 15TH STREET | 50TH COURT | 50TH AVENUE | Asphalt | S | 331 | 26 | 8,599 | 53 | 343 |
| CCRO::15TH ST::170 | 15TH STREET | 50TH AVENUE | 49TH COURT | Asphalt | S | 337 | 26 | 8,756 | 45 | 263 |
| CCRO::15TH ST::180 | 15TH STREET | 49TH COURT | 49TH AVENUE | Asphalt | S | 339 | 26 | 8,808 | 53 | 267 |
| CCRO::15TH ST::190 | 15TH STREET | 49TH AVENUE | 48TH COURT | Asphalt | S | 337 | 26 | 8,767 | 50 | 228 |
| CCRO::15TH ST::20 | 15TH STREET | 61ST COURT | 61ST AVENUE | Asphalt | S | 335 | 26 | 8,713 | 29 | 386 |
| CCRO::15TH ST::200 | 15TH STREET | 48TH COURT | CICERO AVENUE | Asphalt | S | 323 | 26 | 8,411 | 51 | 239 |
| CCRO::15TH ST::30 | 15TH STREET | 61ST AVENUE | 60TH COURT | Asphalt | S | 333 | 26 | 8,659 | 46 | 301 |
| CCRO::15TH ST::40 | 15TH STREET | 60TH COURT | AUSTIN BOULEVARD | Asphalt | S | 325 | 26 | 8,453 | 30 | 437 |
| CCRO::15TH ST::50 | 15TH STREET | AUSTIN BOULEVARD | 59TH COURT | Asphalt | S | 342 | 26 | 8,889 | 41 | 408 |
| CCRO::15TH ST::60 | 15TH STREET | 59TH COURT | 59TH AVENUE | Asphalt | S | 329 | 26 | 8,549 | 33 | 356 |
| CCRO::15TH ST::70 | 15TH STREET | 59TH AVENUE | 58TH COURT | Asphalt | S | 337 | 26 | 8,775 | 45 | 379 |
| CCRO::15TH ST::80 | 15TH STREET | 58TH COURT | 58TH AVENUE | Asphalt | S | 334 | 26 | 8,692 | 34 | 357 |
| CCRO::15TH ST::90 | 15TH STREET | 58TH AVENUE | 57TH COURT | Asphalt | S | 334 | 26 | 8,689 | 52 | 378 |
| CCRO::16TH ST::10 | 16TH STREET | LOMBARD AVENUE | 61ST COURT | Asphalt | P | 335 | 40 | 13,415 | 53 | 232 |
| CCRO::16TH ST::100 | 16TH STREET | 57TH COURT | 57TH AVENUE | Asphalt | P | 329 | 40 | 13,173 | 88 | 127 |
| CCRO::16TH ST::110 | 16TH STREET | 57TH AVENUE | 56TH COURT | Asphalt | P | 340 | 40 | 13,605 | 78 | 192 |
| CCRO::16TH ST::120 | 16TH STREET | 56TH COURT | CENTRAL AVENUE | Asphalt | P | 327 | 40 | 13,074 | 69 | 372 |
| CCRO::16TH ST::130 | 16TH STREET | CENTRAL AVENUE | 55TH COURT | Asphalt | P | 343 | 40 | 13,713 | 52 | 309 |
| CCRO::16TH ST::140 | 16TH STREET | 55TH COURT | 55TH AVENUE | Asphalt | P | 326 | 40 | 13,039 | 45 | 289 |
| CCRO::16TH ST::150 | 16TH STREET | 55TH AVENUE | 54TH AVENUE | Asphalt | P | 677 | 40 | 27,060 | 53 | 350 |
| CCRO::16TH ST::160 | 16TH STREET | 54TH AVENUE | LARAMIE AVENUE | Asphalt | P | 1,329 | 40 | 53,153 | 45 | 341 |
| CCRO::16TH ST::170 | 16TH STREET | LARAMIE AVENUE | 51ST COURT | Asphalt | P | 329 | 40 | 13,172 | 24 | 391 |
| CCRO::16TH ST::180 | 16TH STREET | 51ST COURT | 51ST AVENUE | Asphalt | P | 337 | 40 | 13,473 | 48 | 165 |
| CCRO::16TH ST::190 | 16TH STREET | 51ST AVENUE | 50TH COURT | Asphalt | P | 333 | 40 | 13,316 | 50 | 179 |
| CCRO::16TH ST::20 | 16TH STREET | 61ST COURT | 61ST AVENUE | Asphalt | P | 335 | 40 | 13,384 | 70 | 214 |
| CCRO::16TH ST::200 | 16TH STREET | 50TH COURT | 50TH AVENUE | Asphalt | P | 329 | 40 | 13,174 | 50 | 296 |
| CCRO::16TH ST::210 | 16TH STREET | 50TH AVENUE | 49TH COURT | Asphalt | P | 336 | 40 | 13,457 | 41 | 203 |
| CCRO::16TH ST::220 | 16TH STREET | 49TH COURT | 49TH AVENUE | Asphalt | P | 340 | 40 | 13,604 | 36 | 233 |
| CCRO::16TH ST::230 | 16TH STREET | 49TH AVENUE | 48TH COURT | Asphalt | P | 340 | 40 | 13,599 | 33 | 219 |
| CCRO::16TH ST::240 | 16TH STREET | 48TH COURT | CICERO AVENUE | Asphalt | P | 322 | 40 | 12,881 | 54 | 519 |
| CCRO::16TH ST::250 | 16TH STREET | CICERO AVENUE | 47TH COURT | Asphalt | P | 340 | 40 | 13,620 | 58 | 251 |

| Pavement ID | Road Name | From | To | Surface | Rank | Length (FT) | Width (FT) | Area (SF) | PCI | IRI |
|--------------------|-------------|------------------|------------------|---------|------|-------------|------------|-----------|-----|-----|
| CCRO::16TH ST::260 | 16TH STREET | 47TH COURT | END | Asphalt | P | 437 | 40 | 17,488 | 58 | 222 |
| CCRO::16TH ST::30 | 16TH STREET | 61ST AVENUE | 60TH COURT | Asphalt | P | 329 | 40 | 13,175 | 78 | 170 |
| CCRO::16TH ST::40 | 16TH STREET | 60TH COURT | AUSTIN BOULEVARD | Asphalt | P | 337 | 40 | 13,483 | 80 | 145 |
| CCRO::16TH ST::50 | 16TH STREET | AUSTIN BOULEVARD | 59TH COURT | Asphalt | P | 333 | 40 | 13,302 | 88 | 143 |
| CCRO::16TH ST::60 | 16TH STREET | 59TH COURT | 59TH AVENUE | Asphalt | P | 329 | 40 | 13,180 | 81 | 171 |
| CCRO::16TH ST::70 | 16TH STREET | 59TH AVENUE | 58TH COURT | Asphalt | P | 337 | 40 | 13,463 | 88 | 201 |
| CCRO::16TH ST::80 | 16TH STREET | 58TH COURT | 58TH AVENUE | Asphalt | P | 333 | 40 | 13,328 | 86 | 118 |
| CCRO::16TH ST::90 | 16TH STREET | 58TH AVENUE | 57TH COURT | Asphalt | P | 341 | 40 | 13,625 | 80 | 111 |
| CCRO::18TH ST::10 | 18TH STREET | LOMBARD AVENUE | 61ST COURT | Asphalt | S | 331 | 26 | 8,612 | 59 | 227 |
| CCRO::18TH ST::100 | 18TH STREET | 57TH COURT | 57TH AVENUE | Asphalt | S | 332 | 26 | 8,621 | 64 | 323 |
| CCRO::18TH ST::110 | 18TH STREET | 57TH AVENUE | 56TH COURT | Asphalt | S | 335 | 26 | 8,719 | 48 | 330 |
| CCRO::18TH ST::120 | 18TH STREET | 56TH COURT | CENTRAL AVENUE | Asphalt | S | 335 | 26 | 8,703 | 51 | 364 |
| CCRO::18TH ST::130 | 18TH STREET | CENTRAL AVENUE | 55TH COURT | Asphalt | S | 335 | 26 | 8,721 | 27 | 339 |
| CCRO::18TH ST::140 | 18TH STREET | 55TH COURT | 55TH AVENUE | Asphalt | S | 329 | 26 | 8,547 | 25 | 338 |
| CCRO::18TH ST::150 | 18TH STREET | LARAMIE AVENUE | 51ST COURT | Asphalt | S | 330 | 26 | 8,575 | 64 | 256 |
| CCRO::18TH ST::160 | 18TH STREET | 51ST COURT | 51ST AVENUE | Asphalt | S | 337 | 26 | 8,768 | 56 | 231 |
| CCRO::18TH ST::170 | 18TH STREET | 51ST AVENUE | 50TH COURT | Asphalt | S | 332 | 26 | 8,631 | 67 | 302 |
| CCRO::18TH ST::180 | 18TH STREET | 50TH COURT | 50TH AVENUE | Asphalt | S | 338 | 26 | 8,793 | 48 | 368 |
| CCRO::18TH ST::190 | 18TH STREET | 50TH AVENUE | 49TH COURT | Asphalt | S | 335 | 26 | 8,719 | 68 | 296 |
| CCRO::18TH ST::200 | 18TH STREET | 61ST COURT | 61ST AVENUE | Asphalt | S | 338 | 26 | 8,797 | 70 | 323 |
| CCRO::18TH ST::210 | 18TH STREET | 49TH COURT | 49TH AVENUE | Asphalt | S | 329 | 26 | 8,557 | 64 | 334 |
| CCRO::18TH ST::220 | 18TH STREET | 49TH AVENUE | 48TH COURT | Asphalt | S | 342 | 26 | 8,902 | 48 | 300 |
| CCRO::18TH ST::230 | 18TH STREET | 48TH COURT | CICERO AVENUE | Asphalt | S | 325 | 26 | 8,444 | 56 | 377 |
| CCRO::18TH ST::240 | 18TH STREET | CICERO AVENUE | 47TH COURT | Asphalt | S | 341 | 26 | 8,876 | 88 | 335 |
| CCRO::18TH ST::30 | 18TH STREET | 47TH COURT | END | Asphalt | S | 147 | 26 | 3,819 | 69 | 325 |
| CCRO::18TH ST::40 | 18TH STREET | 61ST AVENUE | 60TH COURT | Asphalt | S | 333 | 26 | 8,660 | 59 | 312 |
| CCRO::18TH ST::50 | 18TH STREET | 60TH COURT | AUSTIN BOULEVARD | Asphalt | S | 329 | 26 | 8,567 | 52 | 245 |
| CCRO::18TH ST::60 | 18TH STREET | AUSTIN BOULEVARD | 59TH COURT | Asphalt | S | 339 | 26 | 8,805 | 88 | 340 |
| CCRO::18TH ST::70 | 18TH STREET | 59TH COURT | 59TH AVENUE | Asphalt | S | 330 | 26 | 8,568 | 51 | 307 |
| CCRO::18TH ST::80 | 18TH STREET | 59TH AVENUE | 58TH COURT | Asphalt | S | 335 | 26 | 8,704 | 53 | 359 |
| CCRO::18TH ST::90 | 18TH STREET | 58TH COURT | 58TH AVENUE | Asphalt | S | 336 | 26 | 8,732 | 58 | 427 |
| CCRO::18TH ST::100 | 18TH STREET | 58TH AVENUE | 57TH COURT | Asphalt | S | 339 | 26 | 8,802 | 58 | 394 |
| CCRO::19TH ST::10 | 19TH STREET | LOMBARD AVENUE | 61ST COURT | Asphalt | S | 327 | 15 | 4,898 | 22 | 759 |
| CCRO::19TH ST::110 | 19TH STREET | 59TH COURT | 59TH AVENUE | Asphalt | S | 330 | 26 | 8,569 | 43 | 381 |
| CCRO::19TH ST::120 | 19TH STREET | 59TH AVENUE | 58TH COURT | Asphalt | S | 333 | 26 | 8,659 | 44 | 316 |
| CCRO::19TH ST::130 | 19TH STREET | 58TH COURT | 58TH AVENUE | Asphalt | S | 339 | 26 | 8,806 | 39 | 419 |
| CCRO::19TH ST::140 | 19TH STREET | 58TH AVENUE | 57TH COURT | Asphalt | S | 336 | 26 | 8,743 | 65 | 301 |
| CCRO::19TH ST::150 | 19TH STREET | 57TH COURT | 57TH AVENUE | Asphalt | S | 334 | 26 | 8,673 | 45 | 246 |
| CCRO::19TH ST::160 | 19TH STREET | 57TH AVENUE | 56TH COURT | Asphalt | S | 331 | 26 | 8,596 | 53 | 293 |
| CCRO::19TH ST::170 | 19TH STREET | 56TH COURT | CENTRAL AVENUE | Asphalt | S | 335 | 26 | 8,720 | 63 | 280 |
| CCRO::19TH ST::180 | 19TH STREET | CENTRAL AVENUE | 55TH COURT | Asphalt | S | 337 | 26 | 8,752 | 65 | 310 |
| CCRO::19TH ST::190 | 19TH STREET | 55TH COURT | 55TH AVENUE | Asphalt | S | 332 | 26 | 8,634 | 67 | 255 |
| CCRO::19TH ST::200 | 19TH STREET | LARAMIE AVENUE | 51ST COURT | Asphalt | S | 329 | 26 | 8,564 | 47 | 424 |

| Pavement ID | Road Name | From | To | Surface | Rank | Length (FT) | Width (FT) | Area (SF) | PCI | IRI |
|--------------------|-------------|------------------|------------------|---------|------|-------------|------------|-----------|-----|-----|
| CCRO::19TH ST::20 | 19TH STREET | 61ST COURT | 61ST AVENUE | Asphalt | S | 343 | 15 | 5,140 | 26 | 787 |
| CCRO::19TH ST::200 | 19TH STREET | 51ST COURT | 51ST AVENUE | Asphalt | S | 338 | 26 | 8,778 | 51 | 325 |
| CCRO::19TH ST::210 | 19TH STREET | 51ST AVENUE | 50TH COURT | Asphalt | S | 331 | 26 | 8,605 | 23 | 427 |
| CCRO::19TH ST::220 | 19TH STREET | 50TH COURT | 50TH AVENUE | Asphalt | S | 342 | 26 | 8,883 | 33 | 405 |
| CCRO::19TH ST::230 | 19TH STREET | 50TH AVENUE | 49TH COURT | Asphalt | S | 334 | 26 | 8,678 | 32 | 259 |
| CCRO::19TH ST::240 | 19TH STREET | 49TH COURT | 49TH AVENUE | Asphalt | S | 326 | 26 | 8,482 | 51 | 252 |
| CCRO::19TH ST::250 | 19TH STREET | 49TH AVENUE | 48TH COURT | Asphalt | S | 343 | 26 | 8,905 | 36 | 344 |
| CCRO::19TH ST::260 | 19TH STREET | 48TH COURT | CICERO AVENUE | Asphalt | S | 328 | 26 | 8,534 | 55 | 296 |
| CCRO::19TH ST::270 | 19TH STREET | CICERO AVENUE | 47TH COURT | Asphalt | S | 342 | 26 | 8,880 | 26 | 434 |
| CCRO::19TH ST::280 | 19TH STREET | 47TH COURT | 47TH AVENUE | Asphalt | S | 316 | 26 | 8,224 | 45 | 299 |
| CCRO::19TH ST::290 | 19TH STREET | 47TH AVENUE | 46TH COURT | Asphalt | S | 345 | 26 | 8,970 | 25 | 400 |
| CCRO::19TH ST::30 | 19TH STREET | 61ST AVENUE | 60TH COURT | Asphalt | S | 338 | 15 | 5,065 | 52 | 383 |
| CCRO::19TH ST::300 | 19TH STREET | 46TH COURT | END | Asphalt | S | 177 | 26 | 4,612 | 40 | 312 |
| CCRO::19TH ST::40 | 19TH STREET | 60TH COURT | AUSTIN BOULEVARD | Asphalt | S | 331 | 15 | 4,960 | 62 | 341 |
| CCRO::19TH ST::50 | 19TH STREET | 60TH COURT | AUSTIN BOULEVARD | Asphalt | S | 328 | 15 | 4,924 | 66 | 422 |
| CCRO::19TH ST::60 | 19TH STREET | 61ST AVENUE | 60TH COURT | Asphalt | S | 337 | 15 | 5,048 | 64 | 422 |
| CCRO::19TH ST::70 | 19TH STREET | 61ST COURT | 61ST AVENUE | Asphalt | S | 342 | 15 | 5,128 | 22 | 802 |
| CCRO::19TH ST::80 | 19TH STREET | START | 61ST COURT | Asphalt | S | 273 | 15 | 4,091 | 26 | 539 |
| CCRO::19TH ST::90 | 19TH STREET | AUSTIN BOULEVARD | 59TH COURT | Asphalt | S | 336 | 26 | 8,748 | 61 | 298 |
| CCRO::20TH ST::10 | 20TH STREET | CICERO AVENUE | 47TH AVENUE | Asphalt | S | 669 | 26 | 17,396 | 46 | 454 |
| CCRO::20TH ST::20 | 20TH STREET | 47TH AVENUE | END | Asphalt | S | 484 | 26 | 12,580 | 52 | 391 |
| CCRO::21ST PL::10 | 21ST PLACE | CENTRAL AVENUE | 55TH COURT | Asphalt | S | 338 | 26 | 8,797 | 23 | 478 |
| CCRO::21ST PL::100 | 21ST PLACE | 47TH AVENUE | END | Asphalt | S | 468 | 26 | 12,172 | 55 | 205 |
| CCRO::21ST PL::20 | 21ST PLACE | LARAMIE AVENUE | 51ST COURT | Asphalt | S | 336 | 26 | 8,725 | 47 | 512 |
| CCRO::21ST PL::30 | 21ST PLACE | 51ST COURT | 51ST AVENUE | Asphalt | S | 338 | 26 | 8,794 | 57 | 226 |
| CCRO::21ST PL::40 | 21ST PLACE | 51ST AVENUE | 50TH COURT | Asphalt | S | 330 | 26 | 8,568 | 54 | 253 |
| CCRO::21ST PL::50 | 21ST PLACE | 50TH COURT | 50TH AVENUE | Asphalt | S | 346 | 26 | 8,988 | 46 | 411 |
| CCRO::21ST PL::60 | 21ST PLACE | 50TH AVENUE | 49TH COURT | Asphalt | S | 322 | 26 | 8,382 | 47 | 304 |
| CCRO::21ST PL::70 | 21ST PLACE | 49TH COURT | 49TH AVENUE | Asphalt | S | 337 | 26 | 8,768 | 86 | 374 |
| CCRO::21ST PL::80 | 21ST PLACE | 49TH AVENUE | 48TH COURT | Asphalt | S | 338 | 26 | 8,779 | 77 | 243 |
| CCRO::21ST PL::90 | 21ST PLACE | 48TH COURT | CICERO AVENUE | Asphalt | S | 330 | 26 | 8,570 | 87 | 321 |
| CCRO::21ST ST::10 | 21ST STREET | LOMBARD AVENUE | 61ST COURT | Asphalt | S | 323 | 26 | 8,396 | 52 | 225 |
| CCRO::21ST ST::100 | 21ST STREET | 57TH COURT | 57TH AVENUE | Asphalt | S | 336 | 26 | 8,727 | 100 | 196 |
| CCRO::21ST ST::110 | 21ST STREET | 57TH AVENUE | 56TH COURT | Asphalt | S | 326 | 26 | 8,471 | 100 | 195 |
| CCRO::21ST ST::120 | 21ST STREET | 56TH COURT | CENTRAL AVENUE | Asphalt | S | 336 | 26 | 8,740 | 100 | 239 |
| CCRO::21ST ST::130 | 21ST STREET | LARAMIE AVENUE | 51ST COURT | Asphalt | S | 329 | 26 | 8,549 | 26 | 484 |
| CCRO::21ST ST::140 | 21ST STREET | 51ST COURT | 51ST AVENUE | Asphalt | S | 338 | 26 | 8,788 | 23 | 423 |
| CCRO::21ST ST::150 | 21ST STREET | 51ST AVENUE | 50TH COURT | Asphalt | S | 330 | 26 | 8,582 | 21 | 455 |
| CCRO::21ST ST::160 | 21ST STREET | 50TH COURT | 50TH AVENUE | Asphalt | S | 344 | 26 | 8,953 | 25 | 699 |
| CCRO::21ST ST::170 | 21ST STREET | 50TH AVENUE | 49TH COURT | Asphalt | S | 327 | 26 | 8,489 | 52 | 422 |
| CCRO::21ST ST::180 | 21ST STREET | 49TH COURT | 49TH AVENUE | Asphalt | S | 333 | 26 | 8,663 | 63 | 405 |
| CCRO::21ST ST::190 | 21ST STREET | 49TH AVENUE | 48TH COURT | Asphalt | S | 339 | 26 | 8,825 | 52 | 283 |
| CCRO::21ST ST::20 | 21ST STREET | 61ST COURT | 61ST AVENUE | Asphalt | S | 346 | 26 | 8,993 | 53 | 299 |

| Pavement ID | Road Name | From | To | Surface | Rank | Length (FT) | Width (FT) | Area (SF) | PCI | IRI |
|--------------------|-------------|------------------|------------------|---------|------|-------------|------------|-----------|-----|-----|
| CCRO::21ST ST::200 | 21ST STREET | 48TH COURT | CICERO AVENUE | Asphalt | S | 329 | 26 | 8,543 | 45 | 363 |
| CCRO::21ST ST::210 | 21ST STREET | CICERO AVENUE | 47TH COURT | Asphalt | S | 337 | 26 | 8,774 | 100 | 332 |
| CCRO::21ST ST::220 | 21ST STREET | 47TH COURT | 47TH AVENUE | Asphalt | S | 333 | 26 | 8,654 | 100 | 397 |
| CCRO::21ST ST::230 | 21ST STREET | 47TH AVENUE | END | Asphalt | S | 474 | 26 | 12,319 | 50 | 438 |
| CCRO::21ST ST::30 | 21ST STREET | 61ST AVENUE | 60TH COURT | Asphalt | S | 340 | 26 | 8,834 | 57 | 328 |
| CCRO::21ST ST::40 | 21ST STREET | 60TH COURT | AUSTIN BOULEVARD | Asphalt | S | 327 | 26 | 8,507 | 58 | 279 |
| CCRO::21ST ST::50 | 21ST STREET | AUSTIN BOULEVARD | 59TH COURT | Asphalt | S | 337 | 26 | 8,761 | 100 | 324 |
| CCRO::21ST ST::60 | 21ST STREET | 59TH COURT | 59TH AVENUE | Asphalt | S | 330 | 26 | 8,570 | 100 | 291 |
| CCRO::21ST ST::70 | 21ST STREET | 59TH AVENUE | 58TH COURT | Asphalt | S | 331 | 26 | 8,614 | 100 | 341 |
| CCRO::21ST ST::80 | 21ST STREET | 58TH COURT | 58TH AVENUE | Asphalt | S | 341 | 26 | 8,872 | 100 | 296 |
| CCRO::21ST ST::90 | 21ST STREET | 58TH AVENUE | 57TH COURT | Asphalt | S | 334 | 26 | 8,686 | 100 | 333 |
| CCRO::22ND PL::10 | 22ND PLACE | 58TH AVENUE | 57TH AVENUE | Asphalt | S | 669 | 26 | 17,389 | 51 | 323 |
| CCRO::22ND PL::20 | 22ND PLACE | 57TH AVENUE | CENTRAL AVENUE | Asphalt | S | 669 | 26 | 17,383 | 51 | 412 |
| CCRO::22ND PL::30 | 22ND PLACE | CENTRAL AVENUE | 55TH AVENUE | Asphalt | S | 666 | 26 | 17,304 | 46 | 317 |
| CCRO::22ND PL::40 | 22ND PLACE | 55TH AVENUE | 54TH AVENUE | Asphalt | S | 673 | 26 | 17,492 | 57 | 271 |
| CCRO::22ND PL::50 | 22ND PLACE | 54TH AVENUE | 53RD AVENUE | Asphalt | S | 664 | 26 | 17,256 | 61 | 318 |
| CCRO::22ND PL::60 | 22ND PLACE | 53RD AVENUE | LARAMIE AVENUE | Asphalt | S | 669 | 26 | 17,385 | 46 | 421 |
| CCRO::22ND PL::70 | 22ND PLACE | LARAMIE AVENUE | 51ST AVENUE | Asphalt | S | 667 | 26 | 17,340 | 55 | 192 |
| CCRO::22ND PL::80 | 22ND PLACE | 51ST AVENUE | 50TH AVENUE | Asphalt | S | 672 | 26 | 17,460 | 54 | 321 |
| CCRO::22ND PL::90 | 22ND PLACE | 49TH AVENUE | CICERO AVENUE | Asphalt | S | 663 | 26 | 17,238 | 94 | 179 |
| CCRO::23RD PL::10 | 23RD PLACE | 58TH AVENUE | 57TH AVENUE | Asphalt | S | 667 | 26 | 17,343 | 23 | 751 |
| CCRO::23RD PL::100 | 23RD PLACE | 49TH AVENUE | END | Asphalt | S | 492 | 26 | 12,798 | 55 | 205 |
| CCRO::23RD PL::20 | 23RD PLACE | 57TH AVENUE | CENTRAL AVENUE | Asphalt | S | 669 | 26 | 17,386 | 29 | 358 |
| CCRO::23RD PL::30 | 23RD PLACE | CENTRAL AVENUE | 55TH AVENUE | Asphalt | S | 669 | 26 | 17,394 | 57 | 192 |
| CCRO::23RD PL::40 | 23RD PLACE | 55TH AVENUE | 54TH AVENUE | Asphalt | S | 672 | 26 | 17,472 | 46 | 268 |
| CCRO::23RD PL::50 | 23RD PLACE | 54TH AVENUE | 53RD AVENUE | Asphalt | S | 673 | 26 | 17,508 | 100 | 629 |
| CCRO::23RD PL::60 | 23RD PLACE | 53RD AVENUE | LARAMIE AVENUE | Asphalt | S | 659 | 26 | 17,135 | 37 | 488 |
| CCRO::23RD PL::70 | 23RD PLACE | LARAMIE AVENUE | 51ST AVENUE | Asphalt | S | 669 | 26 | 17,382 | 64 | 287 |
| CCRO::23RD PL::80 | 23RD PLACE | 51ST AVENUE | 50TH AVENUE | Asphalt | S | 673 | 26 | 17,511 | 56 | 285 |
| CCRO::23RD PL::90 | 23RD PLACE | 50TH AVENUE | 49TH AVENUE | Asphalt | S | 676 | 26 | 17,581 | 37 | 377 |
| CCRO::23RD ST::10 | 23RD STREET | LOMBARD AVENUE | 61ST COURT | Asphalt | S | 324 | 26 | 8,434 | 19 | 641 |
| CCRO::23RD ST::100 | 23RD STREET | 57TH AVENUE | CENTRAL AVENUE | Asphalt | S | 669 | 26 | 17,387 | 100 | 473 |
| CCRO::23RD ST::110 | 23RD STREET | CENTRAL AVENUE | 55TH AVENUE | Asphalt | S | 667 | 26 | 17,347 | 51 | 298 |
| CCRO::23RD ST::120 | 23RD STREET | 55TH AVENUE | 54TH AVENUE | Asphalt | S | 672 | 26 | 17,483 | 56 | 238 |
| CCRO::23RD ST::130 | 23RD STREET | 54TH AVENUE | 53RD AVENUE | Asphalt | S | 673 | 26 | 17,510 | 63 | 339 |
| CCRO::23RD ST::140 | 23RD STREET | 53RD AVENUE | LARAMIE AVENUE | Asphalt | S | 658 | 26 | 17,098 | 51 | 403 |
| CCRO::23RD ST::150 | 23RD STREET | LARAMIE AVENUE | 51ST AVENUE | Asphalt | S | 669 | 26 | 17,389 | 36 | 364 |
| CCRO::23RD ST::160 | 23RD STREET | 51ST AVENUE | 50TH AVENUE | Asphalt | S | 682 | 26 | 17,730 | 28 | 418 |
| CCRO::23RD ST::170 | 23RD STREET | 50TH AVENUE | 49TH AVENUE | Asphalt | S | 666 | 26 | 17,314 | 34 | 485 |
| CCRO::23RD ST::180 | 23RD STREET | 49TH AVENUE | CICERO AVENUE | Asphalt | S | 670 | 26 | 17,423 | 24 | 454 |
| CCRO::23RD ST::20 | 23RD STREET | 61ST COURT | 61ST AVENUE | Asphalt | S | 345 | 26 | 8,979 | 23 | 527 |
| CCRO::23RD ST::30 | 23RD STREET | 61ST AVENUE | 60TH COURT | Asphalt | S | 340 | 26 | 8,839 | 21 | 453 |
| CCRO::23RD ST::40 | 23RD STREET | 60TH COURT | AUSTIN BOULEVARD | Asphalt | S | 328 | 26 | 8,536 | 24 | 306 |

| Pavement ID | Road Name | From | To | Surface | Rank | Length (FT) | Width (FT) | Area (SF) | PCI | IRI |
|--------------------|-------------|------------------|------------------|---------|------|-------------|------------|-----------|-----|-----|
| CCRO::23RD ST::50 | 23RD STREET | AUSTIN BOULEVARD | 59TH COURT | Asphalt | S | 335 | 26 | 8,703 | 100 | 375 |
| CCRO::23RD ST::60 | 23RD STREET | 59TH COURT | 59TH AVENUE | Asphalt | S | 329 | 26 | 8,549 | 100 | 311 |
| CCRO::23RD ST::70 | 23RD STREET | 59TH AVENUE | 58TH COURT | Asphalt | S | 336 | 26 | 8,726 | 100 | 267 |
| CCRO::23RD ST::80 | 23RD STREET | 58TH COURT | 58TH AVENUE | Asphalt | S | 340 | 26 | 8,849 | 100 | 589 |
| CCRO::23RD ST::90 | 23RD STREET | 58TH AVENUE | 57TH AVENUE | Asphalt | S | 668 | 26 | 17,368 | 100 | 606 |
| CCRO::24TH PL::10 | 24TH PLACE | CENTRAL AVENUE | 55TH AVENUE | Asphalt | S | 675 | 26 | 17,555 | 83 | 217 |
| CCRO::24TH PL::20 | 24TH PLACE | 55TH AVENUE | 54TH AVENUE | Asphalt | S | 671 | 26 | 17,453 | 79 | 254 |
| CCRO::24TH PL::30 | 24TH PLACE | 54TH AVENUE | 53RD AVENUE | Asphalt | S | 673 | 26 | 17,507 | 87 | 169 |
| CCRO::24TH PL::40 | 24TH PLACE | 53RD AVENUE | LARAMIE AVENUE | Asphalt | S | 662 | 26 | 17,209 | 84 | 202 |
| CCRO::24TH PL::50 | 24TH PLACE | LARAMIE AVENUE | 51ST AVENUE | Asphalt | S | 670 | 26 | 17,429 | 73 | 320 |
| CCRO::24TH PL::60 | 24TH PLACE | 51ST AVENUE | 50TH AVENUE | Asphalt | S | 671 | 26 | 17,456 | 66 | 275 |
| CCRO::24TH PL::70 | 24TH PLACE | 50TH AVENUE | 49TH AVENUE | Asphalt | S | 679 | 26 | 17,667 | 49 | 364 |
| CCRO::24TH PL::80 | 24TH PLACE | 49TH AVENUE | CICERO AVENUE | Asphalt | S | 658 | 26 | 17,118 | 49 | 307 |
| CCRO::24TH ST::10 | 24TH STREET | LOMBARD AVENUE | 61ST COURT | Asphalt | S | 330 | 26 | 8,584 | 62 | 300 |
| CCRO::24TH ST::100 | 24TH STREET | 57TH COURT | 57TH AVENUE | Asphalt | S | 333 | 26 | 8,652 | 53 | 230 |
| CCRO::24TH ST::110 | 24TH STREET | 57TH AVENUE | 56TH COURT | Asphalt | S | 332 | 26 | 8,640 | 52 | 207 |
| CCRO::24TH ST::120 | 24TH STREET | 56TH COURT | CENTRAL AVENUE | Asphalt | S | 334 | 26 | 8,675 | 59 | 205 |
| CCRO::24TH ST::130 | 24TH STREET | CENTRAL AVENUE | 55TH AVENUE | Asphalt | S | 674 | 26 | 17,512 | 45 | 279 |
| CCRO::24TH ST::140 | 24TH STREET | 55TH AVENUE | 54TH AVENUE | Asphalt | S | 672 | 26 | 17,462 | 50 | 324 |
| CCRO::24TH ST::150 | 24TH STREET | 54TH AVENUE | 53RD AVENUE | Asphalt | S | 673 | 26 | 17,510 | 100 | 492 |
| CCRO::24TH ST::160 | 24TH STREET | 53RD AVENUE | LARAMIE AVENUE | Asphalt | S | 660 | 26 | 17,169 | 100 | 417 |
| CCRO::24TH ST::170 | 24TH STREET | LARAMIE AVENUE | 51ST AVENUE | Asphalt | S | 670 | 26 | 17,408 | 52 | 347 |
| CCRO::24TH ST::180 | 24TH STREET | 51ST AVENUE | 50TH AVENUE | Asphalt | S | 668 | 26 | 17,366 | 53 | 376 |
| CCRO::24TH ST::190 | 24TH STREET | 50TH AVENUE | 49TH AVENUE | Asphalt | S | 683 | 26 | 17,747 | 59 | 260 |
| CCRO::24TH ST::20 | 24TH STREET | 61ST COURT | 61ST AVENUE | Asphalt | S | 341 | 26 | 8,862 | 68 | 248 |
| CCRO::24TH ST::200 | 24TH STREET | 49TH AVENUE | CICERO AVENUE | Asphalt | S | 657 | 26 | 17,092 | 60 | 358 |
| CCRO::24TH ST::30 | 24TH STREET | 61ST AVENUE | 60TH COURT | Asphalt | S | 340 | 26 | 8,853 | 91 | 314 |
| CCRO::24TH ST::40 | 24TH STREET | 60TH COURT | AUSTIN BOULEVARD | Asphalt | S | 327 | 26 | 8,505 | 79 | 367 |
| CCRO::24TH ST::50 | 24TH STREET | AUSTIN BOULEVARD | 59TH COURT | Asphalt | S | 332 | 26 | 8,634 | 65 | 266 |
| CCRO::24TH ST::60 | 24TH STREET | 59TH COURT | 59TH AVENUE | Asphalt | S | 328 | 26 | 8,526 | 55 | 272 |
| CCRO::24TH ST::70 | 24TH STREET | 59TH AVENUE | 58TH COURT | Asphalt | S | 342 | 26 | 8,890 | 55 | 240 |
| CCRO::24TH ST::80 | 24TH STREET | 58TH COURT | 58TH AVENUE | Asphalt | S | 337 | 26 | 8,755 | 49 | 260 |
| CCRO::24TH ST::90 | 24TH STREET | 58TH AVENUE | 57TH COURT | Asphalt | S | 333 | 26 | 8,670 | 50 | 191 |
| CCRO::25TH PL::10 | 25TH PLACE | CENTRAL AVENUE | 55TH AVENUE | Asphalt | S | 679 | 26 | 17,645 | 19 | 450 |
| CCRO::25TH PL::20 | 25TH PLACE | 55TH AVENUE | 54TH AVENUE | Asphalt | S | 670 | 26 | 17,433 | 78 | 302 |
| CCRO::25TH PL::30 | 25TH PLACE | 54TH AVENUE | 53RD AVENUE | Asphalt | S | 673 | 26 | 17,507 | 38 | 369 |
| CCRO::25TH PL::40 | 25TH PLACE | 53RD AVENUE | LARAMIE AVENUE | Asphalt | S | 611 | 26 | 15,899 | 25 | 427 |
| CCRO::25TH PL::50 | 25TH PLACE | LARAMIE AVENUE | 51ST AVENUE | Asphalt | S | 629 | 26 | 16,360 | 38 | 306 |
| CCRO::25TH PL::60 | 25TH PLACE | 51ST AVENUE | 50TH AVENUE | Asphalt | S | 691 | 26 | 17,956 | 52 | 288 |
| CCRO::25TH ST::10 | 25TH STREET | LOMBARD AVENUE | 61ST COURT | Asphalt | S | 336 | 26 | 8,733 | 37 | 315 |
| CCRO::25TH ST::100 | 25TH STREET | 57TH COURT | 57TH AVENUE | Asphalt | S | 331 | 26 | 8,609 | 62 | 383 |
| CCRO::25TH ST::110 | 25TH STREET | 57TH AVENUE | 56TH COURT | Asphalt | S | 334 | 26 | 8,697 | 62 | 307 |
| CCRO::25TH ST::120 | 25TH STREET | 56TH COURT | CENTRAL AVENUE | Asphalt | S | 326 | 26 | 8,476 | 56 | 440 |

| Pavement ID | Road Name | From | To | Surface | Rank | Length (FT) | Width (FT) | Area (SF) | PCI | IRI |
|--------------------|-------------|------------------|------------------|----------|------|-------------|------------|-----------|-----|-----|
| CCRO::25TH ST::130 | 25TH STREET | CENTRAL AVENUE | 55TH AVENUE | Asphalt | S | 683 | 26 | 17,747 | 74 | 250 |
| CCRO::25TH ST::140 | 25TH STREET | 55TH AVENUE | 54TH AVENUE | Asphalt | S | 671 | 26 | 17,443 | 55 | 165 |
| CCRO::25TH ST::150 | 25TH STREET | 54TH AVENUE | 53RD AVENUE | Asphalt | S | 673 | 26 | 17,506 | 42 | 155 |
| CCRO::25TH ST::160 | 25TH STREET | 53RD AVENUE | LARAMIE AVENUE | Asphalt | S | 663 | 26 | 17,242 | 50 | 174 |
| CCRO::25TH ST::170 | 25TH STREET | LARAMIE AVENUE | 51ST AVENUE | Asphalt | S | 672 | 26 | 17,459 | 52 | 275 |
| CCRO::25TH ST::180 | 25TH STREET | 51ST AVENUE | 50TH AVENUE | Asphalt | S | 680 | 26 | 17,676 | 48 | 150 |
| CCRO::25TH ST::190 | 25TH STREET | 50TH AVENUE | 49TH AVENUE | Asphalt | S | 671 | 26 | 17,449 | 40 | 225 |
| CCRO::25TH ST::20 | 25TH STREET | 61ST COURT | 61ST AVENUE | Asphalt | S | 336 | 26 | 8,747 | 33 | 400 |
| CCRO::25TH ST::200 | 25TH STREET | 49TH AVENUE | CICERO AVENUE | Asphalt | S | 658 | 26 | 17,107 | 47 | 216 |
| CCRO::25TH ST::30 | 25TH STREET | 61ST AVENUE | 60TH COURT | Asphalt | S | 341 | 26 | 8,870 | 31 | 327 |
| CCRO::25TH ST::40 | 25TH STREET | 60TH COURT | AUSTIN BOULEVARD | Asphalt | S | 324 | 26 | 8,412 | 33 | 335 |
| CCRO::25TH ST::50 | 25TH STREET | AUSTIN BOULEVARD | 59TH COURT | Asphalt | S | 332 | 26 | 8,632 | 60 | 333 |
| CCRO::25TH ST::60 | 25TH STREET | 59TH COURT | 59TH AVENUE | Asphalt | S | 346 | 26 | 9,001 | 53 | 244 |
| CCRO::25TH ST::70 | 25TH STREET | 59TH AVENUE | 58TH COURT | Asphalt | S | 329 | 26 | 8,556 | 53 | 365 |
| CCRO::25TH ST::80 | 25TH STREET | 58TH COURT | 58TH AVENUE | Asphalt | S | 333 | 26 | 8,661 | 43 | 399 |
| CCRO::25TH ST::90 | 25TH STREET | 58TH AVENUE | 57TH COURT | Asphalt | S | 333 | 26 | 8,667 | 55 | 309 |
| CCRO::27TH ST::10 | 27TH STREET | LOMBARD AVENUE | 61ST COURT | Asphalt | S | 342 | 26 | 8,881 | 40 | 402 |
| CCRO::27TH ST::20 | 27TH STREET | 61ST COURT | 61ST AVENUE | Asphalt | S | 333 | 26 | 8,646 | 23 | 438 |
| CCRO::27TH ST::30 | 27TH STREET | 61ST AVENUE | 60TH COURT | Asphalt | S | 343 | 26 | 8,907 | 28 | 464 |
| CCRO::27TH ST::40 | 27TH STREET | 60TH COURT | AUSTIN BOULEVARD | Asphalt | S | 321 | 26 | 8,355 | 30 | 304 |
| CCRO::27TH ST::50 | 27TH STREET | AUSTIN BOULEVARD | 59TH COURT | Asphalt | S | 335 | 26 | 8,698 | 75 | 235 |
| CCRO::27TH ST::60 | 27TH STREET | 59TH COURT | 59TH AVENUE | Asphalt | S | 345 | 26 | 8,969 | 70 | 240 |
| CCRO::27TH ST::70 | 27TH STREET | 59TH AVENUE | 58TH COURT | Asphalt | S | 334 | 26 | 8,680 | 66 | 221 |
| CCRO::27TH ST::80 | 27TH STREET | 58TH COURT | 58TH AVENUE | Asphalt | S | 319 | 26 | 8,302 | 72 | 264 |
| CCRO::28TH PL::10 | 28TH PLACE | 50TH AVENUE | 49TH AVENUE | Asphalt | S | 456 | 26 | 11,869 | 86 | 285 |
| CCRO::28TH ST::10 | 28TH STREET | LOMBARD AVENUE | 61ST COURT | Concrete | S | 341 | 26 | 8,874 | 67 | 313 |
| CCRO::28TH ST::100 | 28TH STREET | 48TH COURT | CICERO AVENUE | Asphalt | S | 446 | 26 | 11,602 | 69 | 326 |
| CCRO::28TH ST::20 | 28TH STREET | 61ST COURT | 61ST AVENUE | Concrete | S | 334 | 26 | 8,678 | 74 | 360 |
| CCRO::28TH ST::30 | 28TH STREET | 61ST AVENUE | 60TH COURT | Asphalt | S | 343 | 26 | 8,909 | 94 | 352 |
| CCRO::28TH ST::40 | 28TH STREET | 60TH COURT | AUSTIN BOULEVARD | Asphalt | S | 323 | 26 | 8,390 | 73 | 441 |
| CCRO::28TH ST::50 | 28TH STREET | AUSTIN BOULEVARD | 59TH COURT | Asphalt | S | 337 | 26 | 8,767 | 30 | 499 |
| CCRO::28TH ST::60 | 28TH STREET | 59TH COURT | 59TH AVENUE | Asphalt | S | 343 | 26 | 8,908 | 24 | 599 |
| CCRO::28TH ST::70 | 28TH STREET | 59TH AVENUE | 58TH COURT | Asphalt | S | 325 | 26 | 8,446 | 22 | 397 |
| CCRO::28TH ST::80 | 28TH STREET | 50TH AVENUE | 49TH AVENUE | Asphalt | S | 454 | 26 | 11,807 | 60 | 414 |
| CCRO::28TH ST::90 | 28TH STREET | 49TH AVENUE | 48TH COURT | Asphalt | S | 448 | 26 | 11,642 | 89 | 279 |
| CCRO::29TH PL::10 | 29TH PLACE | 53RD AVENUE | LARAMIE AVENUE | Asphalt | S | 680 | 26 | 17,685 | 100 | 566 |
| CCRO::29TH PL::20 | 29TH PLACE | LARAMIE AVENUE | 51ST AVENUE | Asphalt | S | 432 | 26 | 11,231 | 30 | 737 |
| CCRO::29TH PL::30 | 29TH PLACE | 51ST AVENUE | 50TH COURT | Asphalt | S | 448 | 26 | 11,642 | 40 | 595 |
| CCRO::29TH PL::40 | 29TH PLACE | 50TH COURT | 50TH AVENUE | Asphalt | S | 455 | 26 | 11,818 | 22 | 621 |
| CCRO::29TH PL::50 | 29TH PLACE | 50TH AVENUE | 49TH AVENUE | Asphalt | S | 461 | 26 | 11,982 | 50 | 432 |
| CCRO::29TH ST::10 | 29TH STREET | 60TH COURT | AUSTIN BOULEVARD | Asphalt | S | 326 | 26 | 8,485 | 43 | 205 |
| CCRO::29TH ST::20 | 29TH STREET | LARAMIE AVENUE | END | Asphalt | S | 383 | 26 | 9,946 | 41 | 395 |
| CCRO::29TH ST::30 | 29TH STREET | 51ST AVENUE | 50TH COURT | Asphalt | S | 447 | 26 | 11,628 | 45 | 314 |

| Pavement ID | Road Name | From | To | Surface | Rank | Length (FT) | Width (FT) | Area (SF) | PCI | IRI |
|--------------------|-------------|------------------|----------------|---------|------|-------------|------------|-----------|-----|-----|
| CCRO::29TH ST::40 | 29TH STREET | 50TH COURT | 50TH AVENUE | Asphalt | S | 454 | 26 | 11,803 | 44 | 308 |
| CCRO::29TH ST::50 | 29TH STREET | 50TH AVENUE | 49TH AVENUE | Asphalt | S | 459 | 26 | 11,929 | 28 | 446 |
| CCRO::29TH ST::60 | 29TH STREET | 49TH AVENUE | 48TH COURT | Asphalt | S | 448 | 26 | 11,638 | 54 | 363 |
| CCRO::29TH ST::70 | 29TH STREET | 48TH COURT | CICERO AVENUE | Asphalt | S | 438 | 26 | 11,381 | 51 | 362 |
| CCRO::30TH PL::10 | 30TH PLACE | 55TH AVENUE | 54TH AVENUE | Asphalt | S | 673 | 26 | 17,486 | 24 | 318 |
| CCRO::30TH PL::20 | 30TH PLACE | 54TH AVENUE | 53RD AVENUE | Asphalt | S | 677 | 26 | 17,608 | 36 | 381 |
| CCRO::30TH PL::30 | 30TH PLACE | 53RD AVENUE | LARAMIE AVENUE | Asphalt | S | 677 | 26 | 17,592 | 37 | 364 |
| CCRO::30TH PL::40 | 30TH PLACE | LARAMIE AVENUE | 51ST AVENUE | Asphalt | S | 437 | 26 | 11,374 | 100 | 205 |
| CCRO::30TH PL::50 | 30TH PLACE | 51ST AVENUE | 50TH COURT | Asphalt | S | 446 | 26 | 11,587 | 100 | 302 |
| CCRO::30TH PL::60 | 30TH PLACE | 50TH COURT | 50TH AVENUE | Asphalt | S | 456 | 26 | 11,853 | 100 | 271 |
| CCRO::30TH PL::70 | 30TH PLACE | 50TH AVENUE | 49TH AVENUE | Asphalt | S | 457 | 26 | 11,870 | 100 | 300 |
| CCRO::30TH ST::10 | 30TH STREET | 55TH AVENUE | 54TH AVENUE | Asphalt | S | 674 | 26 | 17,516 | 52 | 344 |
| CCRO::30TH ST::20 | 30TH STREET | 54TH AVENUE | 53RD AVENUE | Asphalt | S | 678 | 26 | 17,618 | 52 | 311 |
| CCRO::30TH ST::30 | 30TH STREET | 53RD AVENUE | LARAMIE AVENUE | Asphalt | S | 676 | 26 | 17,563 | 57 | 339 |
| CCRO::30TH ST::40 | 30TH STREET | LARAMIE AVENUE | 51ST AVENUE | Asphalt | S | 436 | 26 | 11,342 | 51 | 344 |
| CCRO::30TH ST::50 | 30TH STREET | 51ST AVENUE | 50TH COURT | Asphalt | S | 448 | 26 | 11,646 | 58 | 473 |
| CCRO::30TH ST::60 | 30TH STREET | 50TH COURT | 50TH AVENUE | Asphalt | S | 455 | 26 | 11,834 | 52 | 399 |
| CCRO::30TH ST::70 | 30TH STREET | 50TH AVENUE | 49TH AVENUE | Asphalt | S | 459 | 26 | 11,928 | 60 | 380 |
| CCRO::30TH ST::80 | 30TH STREET | 49TH AVENUE | 48TH COURT | Asphalt | S | 452 | 26 | 11,752 | 65 | 352 |
| CCRO::30TH ST::90 | 30TH STREET | 48TH COURT | CICERO AVENUE | Asphalt | S | 437 | 26 | 11,369 | 50 | 442 |
| CCRO::31ST PL::10 | 31ST PLACE | LARAMIE AVENUE | 51ST AVENUE | Asphalt | S | 441 | 26 | 11,476 | 100 | 292 |
| CCRO::31ST PL::20 | 31ST PLACE | 51ST AVENUE | 50TH COURT | Asphalt | S | 444 | 26 | 11,553 | 100 | 330 |
| CCRO::31ST PL::30 | 31ST PLACE | 50TH COURT | 50TH AVENUE | Asphalt | S | 457 | 26 | 11,885 | 100 | 248 |
| CCRO::31ST PL::40 | 31ST PLACE | 50TH AVENUE | 49TH AVENUE | Asphalt | S | 453 | 26 | 11,786 | 100 | 252 |
| CCRO::31ST ST::10 | 31ST STREET | AUSTIN BOULEVARD | 59TH AVENUE | Asphalt | P | 666 | 40 | 26,634 | 23 | 338 |
| CCRO::31ST ST::100 | 31ST STREET | 53RD AVENUE | 52ND COURT | Asphalt | P | 340 | 40 | 13,607 | 46 | 135 |
| CCRO::31ST ST::110 | 31ST STREET | 52ND COURT | LARAMIE AVENUE | Asphalt | P | 332 | 40 | 13,275 | 61 | 136 |
| CCRO::31ST ST::120 | 31ST STREET | LARAMIE AVENUE | 51ST AVENUE | Asphalt | P | 444 | 40 | 17,772 | 53 | 276 |
| CCRO::31ST ST::130 | 31ST STREET | 51ST AVENUE | 50TH COURT | Asphalt | P | 443 | 40 | 17,735 | 46 | 257 |
| CCRO::31ST ST::140 | 31ST STREET | 50TH COURT | 50TH AVENUE | Asphalt | P | 457 | 40 | 18,265 | 48 | 237 |
| CCRO::31ST ST::150 | 31ST STREET | 50TH AVENUE | 49TH AVENUE | Asphalt | P | 454 | 40 | 18,173 | 55 | 140 |
| CCRO::31ST ST::160 | 31ST STREET | 49TH AVENUE | 48TH COURT | Asphalt | P | 461 | 40 | 18,426 | 55 | 119 |
| CCRO::31ST ST::170 | 31ST STREET | 48TH COURT | CICERO AVENUE | Asphalt | P | 445 | 40 | 17,792 | 60 | 283 |
| CCRO::31ST ST::20 | 31ST STREET | 59TH AVENUE | OGDEN AVENUE | Asphalt | P | 1,174 | 40 | 46,955 | 33 | 345 |
| CCRO::31ST ST::30 | 31ST STREET | OGDEN AVENUE | CENTRAL AVENUE | Asphalt | P | 820 | 40 | 32,814 | 75 | 182 |
| CCRO::31ST ST::40 | 31ST STREET | CENTRAL AVENUE | 55TH COURT | Asphalt | P | 337 | 40 | 13,486 | 45 | 157 |
| CCRO::31ST ST::50 | 31ST STREET | 55TH COURT | 55TH AVENUE | Asphalt | P | 336 | 40 | 13,454 | 54 | 236 |
| CCRO::31ST ST::60 | 31ST STREET | 55TH AVENUE | 54TH COURT | Asphalt | P | 337 | 40 | 13,474 | 58 | 114 |
| CCRO::31ST ST::70 | 31ST STREET | 54TH COURT | 54TH AVENUE | Asphalt | P | 337 | 40 | 13,462 | 51 | 240 |
| CCRO::31ST ST::80 | 31ST STREET | 54TH AVENUE | 53RD COURT | Asphalt | P | 333 | 40 | 13,336 | 41 | 230 |
| CCRO::31ST ST::90 | 31ST STREET | 53RD COURT | 53RD AVENUE | Asphalt | P | 343 | 40 | 13,736 | 39 | 123 |
| CCRO::32ND PL::10 | 32ND PLACE | LARAMIE AVENUE | 51ST AVENUE | Asphalt | S | 439 | 26 | 11,405 | 43 | 362 |
| CCRO::32ND PL::20 | 32ND PLACE | 51ST AVENUE | 50TH COURT | Asphalt | S | 446 | 26 | 11,605 | 34 | 480 |

| Pavement ID | Road Name | From | To | Surface | Rank | Length (FT) | Width (FT) | Area (SF) | PCI | IRI |
|--------------------|-------------|----------------|------------------|---------|------|-------------|------------|-----------|-----|-----|
| CCRO::32ND PL::30 | 32ND PLACE | 50TH COURT | 50TH AVENUE | Asphalt | S | 458 | 26 | 11,913 | 23 | 425 |
| CCRO::32ND PL::40 | 32ND PLACE | 50TH AVENUE | 49TH AVENUE | Asphalt | S | 451 | 26 | 11,728 | 100 | 240 |
| CCRO::32ND ST::10 | 32ND STREET | START | 61ST COURT | Asphalt | S | 269 | 28 | 7,543 | 100 | 210 |
| CCRO::32ND ST::100 | 32ND STREET | 54TH AVENUE | 53RD COURT | Asphalt | S | 331 | 28 | 9,258 | 100 | 418 |
| CCRO::32ND ST::110 | 32ND STREET | 53RD COURT | 53RD AVENUE | Asphalt | S | 339 | 28 | 9,497 | 100 | 454 |
| CCRO::32ND ST::120 | 32ND STREET | 53RD AVENUE | 52ND COURT | Asphalt | S | 339 | 28 | 9,496 | 100 | 546 |
| CCRO::32ND ST::130 | 32ND STREET | 52ND COURT | LARAMIE AVENUE | Asphalt | S | 336 | 28 | 9,404 | 100 | 402 |
| CCRO::32ND ST::140 | 32ND STREET | LARAMIE AVENUE | 51ST AVENUE | Asphalt | S | 442 | 28 | 12,362 | 32 | 451 |
| CCRO::32ND ST::150 | 32ND STREET | 51ST AVENUE | 50TH COURT | Asphalt | S | 445 | 28 | 12,469 | 43 | 295 |
| CCRO::32ND ST::160 | 32ND STREET | 50TH COURT | 50TH AVENUE | Asphalt | S | 458 | 28 | 12,814 | 32 | 352 |
| CCRO::32ND ST::170 | 32ND STREET | 50TH AVENUE | 49TH AVENUE | Asphalt | S | 452 | 28 | 12,661 | 32 | 365 |
| CCRO::32ND ST::180 | 32ND STREET | 49TH AVENUE | 48TH COURT | Asphalt | S | 455 | 28 | 12,737 | 19 | 320 |
| CCRO::32ND ST::190 | 32ND STREET | 48TH COURT | CICERO AVENUE | Asphalt | S | 450 | 28 | 12,597 | 100 | 415 |
| CCRO::32ND ST::20 | 32ND STREET | LOMBARD AVENUE | 61ST COURT | Asphalt | S | 332 | 40 | 13,293 | 95 | 147 |
| CCRO::32ND ST::30 | 32ND STREET | 61ST COURT | 61ST AVENUE | Asphalt | S | 339 | 40 | 13,546 | 87 | 214 |
| CCRO::32ND ST::40 | 32ND STREET | 61ST AVENUE | 60TH COURT | Asphalt | S | 345 | 40 | 13,785 | 89 | 230 |
| CCRO::32ND ST::50 | 32ND STREET | 60TH COURT | AUSTIN BOULEVARD | Asphalt | S | 324 | 40 | 12,948 | 86 | 178 |
| CCRO::32ND ST::60 | 32ND STREET | CENTRAL AVENUE | 55TH COURT | Asphalt | S | 341 | 28 | 9,546 | 100 | 308 |
| CCRO::32ND ST::70 | 32ND STREET | 55TH COURT | 55TH AVENUE | Asphalt | S | 335 | 28 | 9,391 | 100 | 424 |
| CCRO::32ND ST::80 | 32ND STREET | 55TH AVENUE | 54TH COURT | Asphalt | S | 335 | 28 | 9,370 | 100 | 389 |
| CCRO::32ND ST::90 | 32ND STREET | 54TH COURT | 54TH AVENUE | Asphalt | S | 337 | 28 | 9,446 | 100 | 421 |
| CCRO::33RD ST::10 | 33RD STREET | 61ST AVENUE | 60TH COURT | Asphalt | S | 329 | 30 | 9,856 | 25 | 426 |
| CCRO::33RD ST::100 | 33RD STREET | 49TH AVENUE | 48TH COURT | Asphalt | S | 449 | 26 | 11,679 | 23 | 367 |
| CCRO::33RD ST::20 | 33RD STREET | 60TH COURT | AUSTIN BOULEVARD | Asphalt | S | 342 | 30 | 10,247 | 25 | 407 |
| CCRO::33RD ST::30 | 33RD STREET | 54TH COURT | END | Asphalt | S | 189 | 28 | 5,290 | 24 | 419 |
| CCRO::33RD ST::40 | 33RD STREET | 54TH COURT | 54TH AVENUE | Asphalt | S | 332 | 28 | 9,284 | 22 | 465 |
| CCRO::33RD ST::50 | 33RD STREET | 54TH AVENUE | 53RD COURT | Asphalt | S | 335 | 28 | 9,370 | 22 | 369 |
| CCRO::33RD ST::60 | 33RD STREET | 53RD COURT | 53RD AVENUE | Asphalt | S | 335 | 28 | 9,387 | 23 | 368 |
| CCRO::33RD ST::70 | 33RD STREET | 53RD AVENUE | 52ND COURT | Asphalt | S | 338 | 28 | 9,471 | 22 | 432 |
| CCRO::33RD ST::80 | 33RD STREET | 52ND COURT | LARAMIE AVENUE | Asphalt | S | 332 | 28 | 9,291 | 25 | 817 |
| CCRO::33RD ST::90 | 33RD STREET | 50TH AVENUE | 49TH AVENUE | Asphalt | S | 450 | 26 | 11,701 | 22 | 366 |
| CCRO::34TH ST::10 | 34TH STREET | LOMBARD AVENUE | 61ST COURT | Asphalt | S | 329 | 26 | 8,565 | 26 | 397 |
| CCRO::34TH ST::100 | 34TH STREET | 57TH COURT | 57TH AVENUE | Asphalt | S | 332 | 26 | 8,633 | 23 | 285 |
| CCRO::34TH ST::110 | 34TH STREET | 57TH AVENUE | 56TH COURT | Asphalt | S | 342 | 26 | 8,882 | 45 | 293 |
| CCRO::34TH ST::120 | 34TH STREET | 56TH COURT | CENTRAL AVENUE | Asphalt | S | 328 | 26 | 8,530 | 23 | 340 |
| CCRO::34TH ST::130 | 34TH STREET | CENTRAL AVENUE | 55TH COURT | Asphalt | S | 339 | 26 | 8,820 | 59 | 369 |
| CCRO::34TH ST::140 | 34TH STREET | 55TH COURT | 55TH AVENUE | Asphalt | S | 329 | 26 | 8,549 | 69 | 173 |
| CCRO::34TH ST::150 | 34TH STREET | 55TH AVENUE | 54TH AVENUE | Asphalt | S | 674 | 26 | 17,528 | 63 | 187 |
| CCRO::34TH ST::160 | 34TH STREET | 54TH AVENUE | 53RD COURT | Asphalt | S | 340 | 26 | 8,848 | 23 | 213 |
| CCRO::34TH ST::170 | 34TH STREET | 53RD COURT | LARAMIE AVENUE | Asphalt | S | 1,000 | 26 | 25,994 | 23 | 256 |
| CCRO::34TH ST::20 | 34TH STREET | 61ST COURT | 61ST AVENUE | Asphalt | S | 333 | 26 | 8,653 | 23 | 315 |
| CCRO::34TH ST::30 | 34TH STREET | 61ST AVENUE | 60TH COURT | Asphalt | S | 327 | 26 | 8,506 | 26 | 352 |
| CCRO::34TH ST::40 | 34TH STREET | 60TH COURT | AUSTIN BOULEVARD | Asphalt | S | 339 | 26 | 8,820 | 24 | 338 |

| Pavement ID | Road Name | From | To | Surface | Rank | Length (FT) | Width (FT) | Area (SF) | PCI | IRI |
|--------------------|-------------|------------------|------------------|---------|------|-------------|------------|-----------|-----|-----|
| CCRO::34TH ST::50 | 34TH STREET | AUSTIN BOULEVARD | 59TH COURT | Asphalt | S | 335 | 26 | 8,714 | 53 | 306 |
| CCRO::34TH ST::60 | 34TH STREET | 59TH COURT | 59TH AVENUE | Asphalt | S | 335 | 26 | 8,712 | 51 | 300 |
| CCRO::34TH ST::70 | 34TH STREET | 59TH AVENUE | 58TH COURT | Asphalt | S | 335 | 26 | 8,719 | 30 | 328 |
| CCRO::34TH ST::80 | 34TH STREET | 58TH COURT | 58TH AVENUE | Asphalt | S | 332 | 26 | 8,642 | 20 | 295 |
| CCRO::34TH ST::90 | 34TH STREET | 58TH AVENUE | 57TH COURT | Asphalt | S | 332 | 26 | 8,632 | 20 | 277 |
| CCRO::35TH ST::10 | 35TH STREET | LOMBARD AVENUE | 61ST COURT | Asphalt | S | 329 | 26 | 8,548 | 30 | 323 |
| CCRO::35TH ST::100 | 35TH STREET | 57TH COURT | 57TH AVENUE | Asphalt | P | 332 | 26 | 8,629 | 60 | 121 |
| CCRO::35TH ST::110 | 35TH STREET | 57TH AVENUE | 56TH COURT | Asphalt | P | 343 | 26 | 8,910 | 68 | 116 |
| CCRO::35TH ST::120 | 35TH STREET | 56TH COURT | CENTRAL AVENUE | Asphalt | P | 317 | 26 | 8,231 | 57 | 239 |
| CCRO::35TH ST::130 | 35TH STREET | CENTRAL AVENUE | 55TH COURT | Asphalt | P | 348 | 26 | 9,048 | 48 | 150 |
| CCRO::35TH ST::140 | 35TH STREET | 55TH COURT | 55TH AVENUE | Asphalt | P | 338 | 26 | 8,776 | 48 | 124 |
| CCRO::35TH ST::150 | 35TH STREET | 55TH AVENUE | 54TH COURT | Asphalt | P | 338 | 26 | 8,776 | 47 | 130 |
| CCRO::35TH ST::160 | 35TH STREET | 54TH COURT | 54TH AVENUE | Asphalt | P | 327 | 26 | 8,500 | 59 | 156 |
| CCRO::35TH ST::170 | 35TH STREET | 54TH AVENUE | 53RD COURT | Asphalt | P | 343 | 26 | 8,914 | 45 | 222 |
| CCRO::35TH ST::180 | 35TH STREET | 53RD COURT | 53RD AVENUE | Asphalt | P | 321 | 26 | 8,352 | 47 | 139 |
| CCRO::35TH ST::190 | 35TH STREET | 53RD AVENUE | 53RD COURT | Asphalt | P | 351 | 26 | 9,134 | 52 | 159 |
| CCRO::35TH ST::20 | 35TH STREET | 61ST COURT | 61ST AVENUE | Asphalt | S | 334 | 26 | 8,679 | 29 | 447 |
| CCRO::35TH ST::200 | 35TH STREET | 52ND COURT | LARAMIE AVENUE | Asphalt | P | 317 | 26 | 8,243 | 85 | 303 |
| CCRO::35TH ST::30 | 35TH STREET | 61ST AVENUE | 60TH COURT | Asphalt | S | 325 | 26 | 8,459 | 26 | 446 |
| CCRO::35TH ST::40 | 35TH STREET | 60TH COURT | AUSTIN BOULEVARD | Asphalt | S | 345 | 26 | 8,958 | 42 | 364 |
| CCRO::35TH ST::50 | 35TH STREET | AUSTIN BOULEVARD | 59TH COURT | Asphalt | P | 332 | 26 | 8,633 | 52 | 282 |
| CCRO::35TH ST::60 | 35TH STREET | 59TH COURT | 59TH AVENUE | Asphalt | P | 338 | 26 | 8,779 | 65 | 188 |
| CCRO::35TH ST::70 | 35TH STREET | 59TH AVENUE | 58TH COURT | Asphalt | P | 334 | 26 | 8,697 | 70 | 178 |
| CCRO::35TH ST::80 | 35TH STREET | 58TH COURT | 58TH AVENUE | Asphalt | P | 330 | 26 | 8,576 | 66 | 157 |
| CCRO::35TH ST::90 | 35TH STREET | 58TH AVENUE | 57TH COURT | Asphalt | P | 333 | 26 | 8,646 | 68 | 177 |
| CCRO::36TH ST::10 | 36TH STREET | LOMBARD AVENUE | 61ST COURT | Asphalt | S | 328 | 26 | 8,531 | 25 | 430 |
| CCRO::36TH ST::100 | 36TH STREET | 57TH COURT | 57TH AVENUE | Asphalt | S | 335 | 26 | 8,720 | 100 | 531 |
| CCRO::36TH ST::110 | 36TH STREET | 57TH AVENUE | 56TH COURT | Asphalt | S | 340 | 26 | 8,832 | 100 | 440 |
| CCRO::36TH ST::120 | 36TH STREET | 56TH COURT | CENTRAL AVENUE | Asphalt | S | 322 | 26 | 8,372 | 100 | 407 |
| CCRO::36TH ST::130 | 36TH STREET | CENTRAL AVENUE | 55TH COURT | Asphalt | S | 342 | 26 | 8,901 | 49 | 457 |
| CCRO::36TH ST::140 | 36TH STREET | 55TH COURT | 55TH AVENUE | Asphalt | S | 336 | 26 | 8,724 | 48 | 366 |
| CCRO::36TH ST::150 | 36TH STREET | 55TH AVENUE | 54TH COURT | Asphalt | S | 335 | 26 | 8,705 | 54 | 306 |
| CCRO::36TH ST::160 | 36TH STREET | 54TH COURT | 54TH AVENUE | Asphalt | S | 332 | 26 | 8,632 | 25 | 362 |
| CCRO::36TH ST::170 | 36TH STREET | 54TH AVENUE | 53RD COURT | Asphalt | S | 339 | 26 | 8,817 | 46 | 301 |
| CCRO::36TH ST::180 | 36TH STREET | 53RD COURT | 53RD AVENUE | Asphalt | S | 328 | 26 | 8,521 | 63 | 250 |
| CCRO::36TH ST::190 | 36TH STREET | 53RD AVENUE | 52ND COURT | Asphalt | S | 342 | 26 | 8,883 | 59 | 256 |
| CCRO::36TH ST::20 | 36TH STREET | 61ST COURT | 61ST AVENUE | Asphalt | S | 335 | 26 | 8,708 | 29 | 421 |
| CCRO::36TH ST::200 | 36TH STREET | 52ND COURT | LARAMIE AVENUE | Asphalt | S | 297 | 26 | 7,725 | 55 | 435 |
| CCRO::36TH ST::30 | 36TH STREET | 61ST AVENUE | 60TH COURT | Asphalt | S | 324 | 26 | 8,412 | 32 | 349 |
| CCRO::36TH ST::40 | 36TH STREET | 60TH COURT | AUSTIN BOULEVARD | Asphalt | S | 352 | 26 | 9,161 | 23 | 400 |
| CCRO::36TH ST::50 | 36TH STREET | AUSTIN BOULEVARD | 59TH COURT | Asphalt | S | 323 | 26 | 8,404 | 92 | 287 |
| CCRO::36TH ST::60 | 36TH STREET | 59TH COURT | 59TH AVENUE | Asphalt | S | 341 | 26 | 8,874 | 88 | 346 |
| CCRO::36TH ST::70 | 36TH STREET | 59TH AVENUE | 58TH COURT | Asphalt | S | 332 | 26 | 8,620 | 95 | 205 |

| Pavement ID | Road Name | From | To | Surface | Rank | Length (FT) | Width (FT) | Area (SF) | PCI | IRI |
|--------------------|-------------|------------------|------------------|---------|------|-------------|------------|-----------|-----|-----|
| CCRO::36TH ST::80 | 36TH STREET | 58TH COURT | 58TH AVENUE | Asphalt | S | 341 | 26 | 8,857 | 91 | 278 |
| CCRO::36TH ST::90 | 36TH STREET | 58TH AVENUE | 57TH COURT | Asphalt | S | 330 | 26 | 8,583 | 100 | 490 |
| CCRO::37TH ST::10 | 37TH STREET | LOMBARD AVENUE | 61ST COURT | Asphalt | S | 328 | 26 | 8,516 | 23 | 424 |
| CCRO::37TH ST::100 | 37TH STREET | 57TH COURT | 57TH AVENUE | Asphalt | S | 327 | 26 | 8,498 | 48 | 237 |
| CCRO::37TH ST::110 | 37TH STREET | 57TH AVENUE | 56TH COURT | Asphalt | S | 349 | 26 | 9,067 | 49 | 297 |
| CCRO::37TH ST::120 | 37TH STREET | 56TH COURT | CENTRAL AVENUE | Asphalt | S | 322 | 26 | 8,381 | 59 | 365 |
| CCRO::37TH ST::130 | 37TH STREET | CENTRAL AVENUE | 55TH COURT | Asphalt | S | 342 | 26 | 8,886 | 53 | 213 |
| CCRO::37TH ST::140 | 37TH STREET | 55TH COURT | 55TH AVENUE | Asphalt | S | 335 | 26 | 8,705 | 50 | 168 |
| CCRO::37TH ST::150 | 37TH STREET | 55TH AVENUE | 54TH COURT | Asphalt | S | 335 | 26 | 8,722 | 48 | 120 |
| CCRO::37TH ST::160 | 37TH STREET | 54TH COURT | 54TH AVENUE | Asphalt | S | 328 | 26 | 8,530 | 66 | 179 |
| CCRO::37TH ST::170 | 37TH STREET | 54TH AVENUE | 53RD COURT | Asphalt | S | 340 | 26 | 8,829 | 48 | 287 |
| CCRO::37TH ST::180 | 37TH STREET | 53RD COURT | 53RD AVENUE | Asphalt | S | 334 | 26 | 8,690 | 24 | 336 |
| CCRO::37TH ST::190 | 37TH STREET | 53RD AVENUE | 52ND COURT | Asphalt | S | 335 | 26 | 8,706 | 27 | 354 |
| CCRO::37TH ST::20 | 37TH STREET | 61ST COURT | 61ST AVENUE | Asphalt | S | 336 | 26 | 8,733 | 31 | 293 |
| CCRO::37TH ST::200 | 37TH STREET | 52ND COURT | LARAMIE AVENUE | Asphalt | S | 301 | 26 | 7,836 | 27 | 406 |
| CCRO::37TH ST::30 | 37TH STREET | 61ST AVENUE | 60TH COURT | Asphalt | S | 334 | 26 | 8,672 | 24 | 606 |
| CCRO::37TH ST::40 | 37TH STREET | 60TH COURT | AUSTIN BOULEVARD | Asphalt | S | 340 | 26 | 8,840 | 23 | 456 |
| CCRO::37TH ST::50 | 37TH STREET | AUSTIN BOULEVARD | 59TH COURT | Asphalt | S | 318 | 26 | 8,279 | 49 | 268 |
| CCRO::37TH ST::60 | 37TH STREET | 59TH COURT | 59TH AVENUE | Asphalt | S | 345 | 26 | 8,977 | 55 | 275 |
| CCRO::37TH ST::70 | 37TH STREET | 59TH AVENUE | 58TH COURT | Asphalt | S | 334 | 26 | 8,683 | 54 | 293 |
| CCRO::37TH ST::80 | 37TH STREET | 58TH COURT | 58TH AVENUE | Asphalt | S | 334 | 26 | 8,672 | 49 | 343 |
| CCRO::37TH ST::90 | 37TH STREET | 58TH AVENUE | 57TH COURT | Asphalt | S | 326 | 26 | 8,487 | 68 | 305 |
| CCRO::38TH ST::10 | 38TH STREET | LOMBARD AVENUE | 61ST COURT | Asphalt | S | 327 | 26 | 8,502 | 85 | 242 |
| CCRO::38TH ST::100 | 38TH STREET | 57TH COURT | 57TH AVENUE | Asphalt | S | 336 | 26 | 8,748 | 95 | 205 |
| CCRO::38TH ST::110 | 38TH STREET | 57TH AVENUE | 56TH COURT | Asphalt | S | 340 | 26 | 8,831 | 94 | 272 |
| CCRO::38TH ST::120 | 38TH STREET | 56TH COURT | CENTRAL AVENUE | Asphalt | S | 323 | 26 | 8,390 | 95 | 215 |
| CCRO::38TH ST::130 | 38TH STREET | 54TH AVENUE | 53RD COURT | Asphalt | S | 314 | 26 | 8,163 | 20 | 384 |
| CCRO::38TH ST::140 | 38TH STREET | 53RD COURT | 53RD AVENUE | Asphalt | S | 341 | 26 | 8,859 | 22 | 666 |
| CCRO::38TH ST::150 | 38TH STREET | 53RD AVENUE | 52ND COURT | Asphalt | S | 334 | 26 | 8,697 | 23 | 480 |
| CCRO::38TH ST::160 | 38TH STREET | 52ND COURT | LARAMIE AVENUE | Asphalt | S | 302 | 26 | 7,850 | 25 | 282 |
| CCRO::38TH ST::20 | 38TH STREET | 61ST COURT | 61ST AVENUE | Asphalt | S | 337 | 26 | 8,757 | 95 | 266 |
| CCRO::38TH ST::30 | 38TH STREET | 61ST AVENUE | 60TH COURT | Asphalt | S | 334 | 26 | 8,683 | 93 | 230 |
| CCRO::38TH ST::40 | 38TH STREET | 60TH COURT | AUSTIN BOULEVARD | Asphalt | S | 338 | 26 | 8,779 | 86 | 278 |
| CCRO::38TH ST::50 | 38TH STREET | AUSTIN BOULEVARD | 59TH COURT | Asphalt | S | 318 | 26 | 8,260 | 83 | 326 |
| CCRO::38TH ST::60 | 38TH STREET | 59TH COURT | 59TH AVENUE | Asphalt | S | 347 | 26 | 9,018 | 75 | 347 |
| CCRO::38TH ST::70 | 38TH STREET | 59TH AVENUE | 58TH COURT | Asphalt | S | 339 | 26 | 8,805 | 95 | 246 |
| CCRO::38TH ST::80 | 38TH STREET | 58TH COURT | 58TH AVENUE | Asphalt | S | 328 | 26 | 8,533 | 94 | 284 |
| CCRO::38TH ST::90 | 38TH STREET | 58TH AVENUE | 57TH COURT | Asphalt | S | 325 | 26 | 8,462 | 95 | 273 |
| CCRO::46TH CT::10 | 46TH COURT | 19TH STREET | 16TH STREET | Asphalt | S | 1,327 | 26 | 34,496 | 23 | 377 |
| CCRO::47TH AVE::10 | 47TH AVENUE | 22ND STREET | 21ST PLACE | Asphalt | S | 373 | 26 | 9,706 | 28 | 577 |
| CCRO::47TH AVE::20 | 47TH AVENUE | 21ST PLACE | 21ST STREET | Asphalt | S | 308 | 26 | 8,002 | 29 | 371 |
| CCRO::47TH AVE::30 | 47TH AVENUE | 21ST STREET | 20TH STREET | Asphalt | S | 333 | 26 | 8,645 | 52 | 334 |
| CCRO::47TH AVE::40 | 47TH AVENUE | 20TH STREET | 19TH STREET | Asphalt | S | 335 | 26 | 8,710 | 27 | 368 |

| Pavement ID | Road Name | From | To | Surface | Rank | Length (FT) | Width (FT) | Area (SF) | PCI | IRI |
|---------------------|-------------|-------------|----------------|---------|------|-------------|------------|-----------|-----|-----|
| CCRO::47TH AVE::50 | 47TH AVENUE | 14TH STREET | 13TH STREET | Asphalt | S | 659 | 26 | 17,126 | 43 | 212 |
| CCRO::47TH AVE::60 | 47TH AVENUE | 13TH STREET | 12TH PLACE | Asphalt | S | 338 | 26 | 8,797 | 24 | 239 |
| CCRO::47TH AVE::70 | 47TH AVENUE | 12TH PLACE | ROOSEVELT ROAD | Asphalt | S | 338 | 26 | 8,787 | 40 | 233 |
| CCRO::47TH CT::10 | 47TH COURT | 22ND STREET | 21ST STREET | Asphalt | S | 637 | 26 | 16,574 | 66 | 294 |
| CCRO::47TH CT::20 | 47TH COURT | 19TH STREET | 18TH STREET | Asphalt | S | 664 | 26 | 17,260 | 100 | 390 |
| CCRO::47TH CT::30 | 47TH COURT | 18TH STREET | 16TH STREET | Asphalt | S | 670 | 26 | 17,425 | 100 | 487 |
| CCRO::48TH CT::10 | 48TH COURT | 33RD STREET | 32ND STREET | Asphalt | S | 656 | 26 | 17,046 | 53 | 236 |
| CCRO::48TH CT::100 | 48TH COURT | 18TH STREET | 16TH STREET | Asphalt | S | 675 | 26 | 17,548 | 49 | 355 |
| CCRO::48TH CT::110 | 48TH COURT | 16TH STREET | 15TH STREET | Asphalt | S | 654 | 26 | 17,015 | 25 | 563 |
| CCRO::48TH CT::120 | 48TH COURT | 15TH STREET | 14TH STREET | Asphalt | S | 664 | 26 | 17,260 | 45 | 309 |
| CCRO::48TH CT::130 | 48TH COURT | 14TH STREET | 13TH STREET | Asphalt | S | 662 | 26 | 17,214 | 40 | 393 |
| CCRO::48TH CT::140 | 48TH COURT | 13TH STREET | ROOSEVELT ROAD | Asphalt | S | 673 | 26 | 17,500 | 47 | 275 |
| CCRO::48TH CT::20 | 48TH COURT | 32ND STREET | 31ST STREET | Asphalt | S | 664 | 26 | 17,256 | 69 | 208 |
| CCRO::48TH CT::30 | 48TH COURT | 31ST STREET | 30TH STREET | Asphalt | S | 666 | 26 | 17,306 | 53 | 337 |
| CCRO::48TH CT::40 | 48TH COURT | 30TH STREET | 29TH STREET | Asphalt | S | 661 | 26 | 17,184 | 55 | 379 |
| CCRO::48TH CT::50 | 48TH COURT | 29TH STREET | 28TH STREET | Asphalt | S | 670 | 26 | 17,427 | 56 | 330 |
| CCRO::48TH CT::60 | 48TH COURT | 28TH STREET | END | Asphalt | S | 313 | 26 | 8,147 | 45 | 334 |
| CCRO::48TH CT::70 | 48TH COURT | 21ST PLACE | 21ST STREET | Asphalt | S | 374 | 26 | 9,727 | 22 | 472 |
| CCRO::48TH CT::80 | 48TH COURT | 21ST STREET | 19TH STREET | Asphalt | S | 665 | 26 | 17,295 | 16 | 417 |
| CCRO::48TH CT::90 | 48TH COURT | 19TH STREET | 18TH STREET | Asphalt | S | 664 | 26 | 17,253 | 20 | 440 |
| CCRO::49TH AVE::10 | 49TH AVENUE | 32ND PLACE | 33RD STREET | Asphalt | S | 312 | 26 | 8,109 | 41 | 278 |
| CCRO::49TH AVE::100 | 49TH AVENUE | 28TH PLACE | 28TH STREET | Asphalt | S | 334 | 26 | 8,688 | 44 | 295 |
| CCRO::49TH AVE::110 | 49TH AVENUE | 28TH STREET | OGDEN AVENUE | Asphalt | S | 385 | 26 | 9,998 | 51 | 266 |
| CCRO::49TH AVE::120 | 49TH AVENUE | 25TH PLACE | 25TH STREET | Asphalt | S | 332 | 26 | 8,640 | 71 | 331 |
| CCRO::49TH AVE::130 | 49TH AVENUE | 25TH STREET | 24TH PLACE | Asphalt | S | 328 | 26 | 8,533 | 44 | 344 |
| CCRO::49TH AVE::140 | 49TH AVENUE | 24TH PLACE | 24TH STREET | Asphalt | S | 336 | 26 | 8,728 | 51 | 383 |
| CCRO::49TH AVE::150 | 49TH AVENUE | 24TH STREET | 23RD PLACE | Asphalt | S | 339 | 26 | 8,805 | 50 | 412 |
| CCRO::49TH AVE::160 | 49TH AVENUE | 23RD PLACE | 23RD STREET | Asphalt | S | 331 | 26 | 8,619 | 25 | 425 |
| CCRO::49TH AVE::170 | 49TH AVENUE | 23RD STREET | 22ND PLACE | Asphalt | S | 341 | 26 | 8,876 | 23 | 308 |
| CCRO::49TH AVE::180 | 49TH AVENUE | 22ND PLACE | 22ND STREET | Asphalt | S | 300 | 26 | 7,801 | 52 | 340 |
| CCRO::49TH AVE::190 | 49TH AVENUE | 22ND STREET | 21ST PLACE | Asphalt | S | 313 | 26 | 8,143 | 59 | 756 |
| CCRO::49TH AVE::20 | 49TH AVENUE | 32ND PLACE | 32ND STREET | Asphalt | S | 334 | 26 | 8,684 | 31 | 452 |
| CCRO::49TH AVE::200 | 49TH AVENUE | 21ST PLACE | 21ST STREET | Asphalt | S | 374 | 26 | 9,726 | 50 | 402 |
| CCRO::49TH AVE::210 | 49TH AVENUE | 21ST STREET | 19TH STREET | Asphalt | S | 668 | 26 | 17,366 | 50 | 430 |
| CCRO::49TH AVE::220 | 49TH AVENUE | 19TH STREET | 18TH STREET | Asphalt | S | 659 | 26 | 17,147 | 50 | 303 |
| CCRO::49TH AVE::230 | 49TH AVENUE | 18TH STREET | 16TH STREET | Asphalt | S | 684 | 26 | 17,775 | 52 | 407 |
| CCRO::49TH AVE::240 | 49TH AVENUE | 16TH STREET | 15TH STREET | Asphalt | S | 650 | 26 | 16,887 | 73 | 280 |
| CCRO::49TH AVE::250 | 49TH AVENUE | 15TH STREET | 14TH STREET | Asphalt | S | 665 | 26 | 17,284 | 79 | 271 |
| CCRO::49TH AVE::260 | 49TH AVENUE | 14TH STREET | 13TH STREET | Asphalt | S | 666 | 26 | 17,305 | 23 | 608 |
| CCRO::49TH AVE::270 | 49TH AVENUE | 13TH STREET | ROOSEVELT ROAD | Asphalt | S | 669 | 26 | 17,404 | 22 | 592 |
| CCRO::49TH AVE::30 | 49TH AVENUE | 32ND STREET | 31ST PLACE | Asphalt | S | 340 | 26 | 8,837 | 41 | 269 |
| CCRO::49TH AVE::40 | 49TH AVENUE | 31ST PLACE | 31ST STREET | Asphalt | S | 324 | 26 | 8,429 | 37 | 392 |
| CCRO::49TH AVE::50 | 49TH AVENUE | 31ST STREET | 30TH PLACE | Asphalt | S | 336 | 26 | 8,747 | 28 | 348 |

| Pavement ID | Road Name | From | To | Surface | Rank | Length (FT) | Width (FT) | Area (SF) | PCI | IRI |
|---------------------|-------------|-------------|----------------|----------|------|-------------|------------|-----------|-----|-----|
| CCRO::49TH AVE::60 | 49TH AVENUE | 30TH PLACE | 30TH STREET | Asphalt | S | 334 | 26 | 8,684 | 37 | 427 |
| CCRO::49TH AVE::70 | 49TH AVENUE | 30TH STREET | 29TH PLACE | Asphalt | S | 327 | 26 | 8,499 | 47 | 242 |
| CCRO::49TH AVE::80 | 49TH AVENUE | 29TH PLACE | 29TH STREET | Asphalt | S | 340 | 26 | 8,838 | 39 | 357 |
| CCRO::49TH AVE::90 | 49TH AVENUE | 29TH STREET | 28TH PLACE | Asphalt | S | 328 | 26 | 8,539 | 43 | 387 |
| CCRO::49TH CT::10 | 49TH COURT | 21ST PLACE | 21ST STREET | Asphalt | S | 381 | 26 | 9,899 | 100 | 515 |
| CCRO::49TH CT::20 | 49TH COURT | 21ST STREET | 19TH STREET | Asphalt | S | 664 | 26 | 17,267 | 100 | 546 |
| CCRO::49TH CT::30 | 49TH COURT | 19TH STREET | 18TH STREET | Asphalt | S | 663 | 26 | 17,243 | 100 | 450 |
| CCRO::49TH CT::40 | 49TH COURT | 18TH STREET | 16TH STREET | Asphalt | S | 688 | 26 | 17,884 | 100 | 392 |
| CCRO::49TH CT::50 | 49TH COURT | 16TH STREET | 15TH STREET | Asphalt | S | 643 | 26 | 16,710 | 52 | 683 |
| CCRO::49TH CT::60 | 49TH COURT | 15TH STREET | 14TH STREET | Asphalt | S | 674 | 26 | 17,525 | 41 | 462 |
| CCRO::49TH CT::70 | 49TH COURT | 14TH STREET | 13TH STREET | Asphalt | S | 659 | 26 | 17,134 | 34 | 560 |
| CCRO::49TH CT::80 | 49TH COURT | 13TH STREET | ROOSEVELT ROAD | Asphalt | S | 669 | 26 | 17,403 | 23 | 438 |
| CCRO::50TH AVE::10 | 50TH AVENUE | 32ND PLACE | 33RD STREET | Asphalt | S | 314 | 26 | 8,176 | 95 | 229 |
| CCRO::50TH AVE::100 | 50TH AVENUE | 28TH PLACE | 28TH STREET | Asphalt | S | 331 | 26 | 8,610 | 25 | 315 |
| CCRO::50TH AVE::110 | 50TH AVENUE | 28TH STREET | OGDEN AVENUE | Asphalt | S | 178 | 26 | 4,636 | 40 | 549 |
| CCRO::50TH AVE::120 | 50TH AVENUE | 26TH STREET | END | Concrete | S | 245 | 26 | 6,374 | 78 | 514 |
| CCRO::50TH AVE::130 | 50TH AVENUE | 26TH STREET | 25TH PLACE | Asphalt | S | 106 | 26 | 2,754 | 54 | 552 |
| CCRO::50TH AVE::140 | 50TH AVENUE | 25TH PLACE | 25TH STREET | Asphalt | S | 291 | 73 | 21,273 | 44 | 234 |
| CCRO::50TH AVE::150 | 50TH AVENUE | 25TH STREET | 24TH PLACE | Asphalt | S | 323 | 40 | 12,926 | 53 | 430 |
| CCRO::50TH AVE::160 | 50TH AVENUE | 24TH PLACE | 24TH STREET | Asphalt | S | 336 | 40 | 13,441 | 49 | 303 |
| CCRO::50TH AVE::170 | 50TH AVENUE | 24TH STREET | 23RD PLACE | Asphalt | S | 340 | 40 | 13,599 | 51 | 292 |
| CCRO::50TH AVE::180 | 50TH AVENUE | 23RD PLACE | 23RD STREET | Asphalt | S | 331 | 40 | 13,241 | 50 | 283 |
| CCRO::50TH AVE::190 | 50TH AVENUE | 23RD STREET | 22ND PLACE | Asphalt | S | 319 | 40 | 12,750 | 23 | 306 |
| CCRO::50TH AVE::20 | 50TH AVENUE | 32ND PLACE | 32ND STREET | Asphalt | S | 335 | 26 | 8,704 | 100 | 274 |
| CCRO::50TH AVE::200 | 50TH AVENUE | 22ND PLACE | 22ND STREET | Asphalt | S | 319 | 40 | 12,773 | 23 | 278 |
| CCRO::50TH AVE::210 | 50TH AVENUE | 22ND STREET | 21ST PLACE | Asphalt | S | 312 | 40 | 12,486 | 78 | 455 |
| CCRO::50TH AVE::220 | 50TH AVENUE | 21ST PLACE | 21ST STREET | Asphalt | S | 375 | 40 | 15,007 | 23 | 291 |
| CCRO::50TH AVE::230 | 50TH AVENUE | 21ST STREET | 19TH STREET | Asphalt | S | 667 | 40 | 26,695 | 45 | 219 |
| CCRO::50TH AVE::240 | 50TH AVENUE | 19TH STREET | 18TH STREET | Asphalt | S | 670 | 40 | 26,783 | 51 | 227 |
| CCRO::50TH AVE::250 | 50TH AVENUE | 18TH STREET | 16TH STREET | Asphalt | S | 684 | 40 | 27,375 | 53 | 282 |
| CCRO::50TH AVE::260 | 50TH AVENUE | 16TH STREET | 15TH STREET | Asphalt | S | 644 | 40 | 25,744 | 45 | 265 |
| CCRO::50TH AVE::270 | 50TH AVENUE | 15TH STREET | 14TH STREET | Asphalt | S | 672 | 40 | 26,861 | 52 | 220 |
| CCRO::50TH AVE::280 | 50TH AVENUE | 14TH STREET | 13TH STREET | Asphalt | S | 665 | 40 | 26,588 | 39 | 305 |
| CCRO::50TH AVE::290 | 50TH AVENUE | 13TH STREET | ROOSEVELT ROAD | Asphalt | S | 661 | 40 | 26,443 | 52 | 321 |
| CCRO::50TH AVE::30 | 50TH AVENUE | 32ND STREET | 31ST PLACE | Asphalt | S | 331 | 26 | 8,614 | 37 | 491 |
| CCRO::50TH AVE::40 | 50TH AVENUE | 31ST PLACE | 31ST STREET | Asphalt | S | 327 | 26 | 8,502 | 47 | 383 |
| CCRO::50TH AVE::50 | 50TH AVENUE | 31ST STREET | 30TH PLACE | Asphalt | S | 337 | 26 | 8,769 | 31 | 315 |
| CCRO::50TH AVE::60 | 50TH AVENUE | 30TH PLACE | 30TH STREET | Asphalt | S | 333 | 26 | 8,661 | 23 | 253 |
| CCRO::50TH AVE::70 | 50TH AVENUE | 30TH STREET | 29TH PLACE | Asphalt | S | 331 | 26 | 8,618 | 86 | 280 |
| CCRO::50TH AVE::80 | 50TH AVENUE | 29TH PLACE | 29TH STREET | Asphalt | S | 338 | 26 | 8,781 | 65 | 272 |
| CCRO::50TH AVE::90 | 50TH AVENUE | 29TH STREET | 28TH PLACE | Asphalt | S | 331 | 26 | 8,614 | 23 | 339 |
| CCRO::50TH CT::10 | 50TH COURT | 32ND PLACE | END | Asphalt | S | 312 | 26 | 8,121 | 53 | 251 |
| CCRO::50TH CT::100 | 50TH COURT | 21ST PLACE | 21ST STREET | Asphalt | S | 385 | 26 | 10,019 | 24 | 503 |

| Pavement ID | Road Name | From | To | Surface | Rank | Length (FT) | Width (FT) | Area (SF) | PCI | IRI |
|---------------------|-------------|-------------|----------------|---------|------|-------------|------------|-----------|-----|-----|
| CCRO::50TH CT::110 | 50TH COURT | 21ST STREET | 19TH STREET | Asphalt | S | 658 | 26 | 17,116 | 41 | 482 |
| CCRO::50TH CT::120 | 50TH COURT | 19TH STREET | 18TH STREET | Asphalt | S | 672 | 26 | 17,472 | 25 | 407 |
| CCRO::50TH CT::130 | 50TH COURT | 18TH STREET | 16TH STREET | Asphalt | S | 687 | 26 | 17,850 | 24 | 323 |
| CCRO::50TH CT::140 | 50TH COURT | 16TH STREET | 15TH STREET | Asphalt | S | 633 | 26 | 16,457 | 100 | 275 |
| CCRO::50TH CT::150 | 50TH COURT | 15TH STREET | 14TH STREET | Asphalt | S | 680 | 26 | 17,683 | 100 | 220 |
| CCRO::50TH CT::160 | 50TH COURT | 14TH STREET | 13TH STREET | Asphalt | S | 660 | 26 | 17,168 | 100 | 223 |
| CCRO::50TH CT::170 | 50TH COURT | 13TH STREET | ROOSEVELT ROAD | Asphalt | S | 663 | 26 | 17,250 | 100 | 137 |
| CCRO::50TH CT::20 | 50TH COURT | 32ND PLACE | 32ND STREET | Asphalt | S | 333 | 26 | 8,662 | 47 | 322 |
| CCRO::50TH CT::30 | 50TH COURT | 32ND STREET | 31ST PLACE | Asphalt | S | 333 | 26 | 8,655 | 52 | 338 |
| CCRO::50TH CT::40 | 50TH COURT | 31ST PLACE | 31ST STREET | Asphalt | S | 333 | 26 | 8,667 | 42 | 385 |
| CCRO::50TH CT::50 | 50TH COURT | 31ST STREET | 30TH PLACE | Asphalt | S | 329 | 26 | 8,547 | 68 | 470 |
| CCRO::50TH CT::60 | 50TH COURT | 30TH PLACE | 30TH STREET | Asphalt | S | 334 | 26 | 8,694 | 74 | 249 |
| CCRO::50TH CT::70 | 50TH COURT | 30TH STREET | 29TH PLACE | Asphalt | S | 333 | 26 | 8,651 | 49 | 316 |
| CCRO::50TH CT::80 | 50TH COURT | 29TH PLACE | 29TH STREET | Asphalt | S | 333 | 26 | 8,666 | 44 | 296 |
| CCRO::50TH CT::90 | 50TH COURT | 29TH STREET | OGDEN AVENUE | Asphalt | S | 639 | 26 | 16,621 | 49 | 368 |
| CCRO::51ST AVE::10 | 51ST AVENUE | 32ND PLACE | END | Asphalt | S | 307 | 26 | 7,979 | 47 | 363 |
| CCRO::51ST AVE::100 | 51ST AVENUE | 26TH STREET | 25TH PLACE | Asphalt | S | 335 | 26 | 8,702 | 53 | 251 |
| CCRO::51ST AVE::110 | 51ST AVENUE | 25TH PLACE | 25TH STREET | Asphalt | S | 318 | 26 | 8,273 | 79 | 314 |
| CCRO::51ST AVE::120 | 51ST AVENUE | 25TH STREET | 24TH PLACE | Asphalt | S | 341 | 26 | 8,879 | 61 | 374 |
| CCRO::51ST AVE::130 | 51ST AVENUE | 24TH PLACE | 24TH STREET | Asphalt | S | 333 | 26 | 8,654 | 57 | 343 |
| CCRO::51ST AVE::140 | 51ST AVENUE | 24TH STREET | 23RD PLACE | Asphalt | S | 332 | 26 | 8,635 | 74 | 399 |
| CCRO::51ST AVE::150 | 51ST AVENUE | 23RD PLACE | 23RD STREET | Asphalt | S | 330 | 26 | 8,580 | 63 | 275 |
| CCRO::51ST AVE::160 | 51ST AVENUE | 23RD STREET | 22ND PLACE | Asphalt | S | 333 | 26 | 8,658 | 52 | 266 |
| CCRO::51ST AVE::170 | 51ST AVENUE | 22ND PLACE | 22ND STREET | Asphalt | S | 317 | 26 | 8,238 | 53 | 366 |
| CCRO::51ST AVE::180 | 51ST AVENUE | 22ND STREET | 22ND STREET | Asphalt | S | 36 | 26 | 948 | 95 | 205 |
| CCRO::51ST AVE::190 | 51ST AVENUE | 21ST PLACE | 21ST STREET | Asphalt | S | 385 | 26 | 10,003 | 72 | 330 |
| CCRO::51ST AVE::20 | 51ST AVENUE | 32ND PLACE | 32ND STREET | Asphalt | S | 333 | 26 | 8,655 | 45 | 294 |
| CCRO::51ST AVE::200 | 51ST AVENUE | 21ST STREET | 19TH STREET | Asphalt | S | 657 | 26 | 17,073 | 33 | 470 |
| CCRO::51ST AVE::210 | 51ST AVENUE | 19TH STREET | 18TH STREET | Asphalt | S | 661 | 26 | 17,183 | 43 | 536 |
| CCRO::51ST AVE::220 | 51ST AVENUE | 18TH STREET | 16TH STREET | Asphalt | S | 698 | 26 | 18,156 | 80 | 244 |
| CCRO::51ST AVE::230 | 51ST AVENUE | 16TH STREET | 15TH STREET | Asphalt | S | 639 | 26 | 16,620 | 55 | 326 |
| CCRO::51ST AVE::240 | 51ST AVENUE | 15TH STREET | 14TH STREET | Asphalt | S | 670 | 26 | 17,416 | 54 | 203 |
| CCRO::51ST AVE::250 | 51ST AVENUE | 14TH STREET | 13TH STREET | Asphalt | S | 658 | 26 | 17,108 | 100 | 330 |
| CCRO::51ST AVE::260 | 51ST AVENUE | 13TH STREET | ROOSEVELT ROAD | Asphalt | S | 666 | 26 | 17,308 | 100 | 354 |
| CCRO::51ST AVE::30 | 51ST AVENUE | 32ND STREET | 31ST PLACE | Asphalt | S | 338 | 26 | 8,800 | 48 | 302 |
| CCRO::51ST AVE::40 | 51ST AVENUE | 31ST PLACE | 31ST STREET | Asphalt | S | 320 | 26 | 8,318 | 56 | 328 |
| CCRO::51ST AVE::50 | 51ST AVENUE | 31ST STREET | 30TH PLACE | Asphalt | S | 337 | 26 | 8,755 | 55 | 242 |
| CCRO::51ST AVE::60 | 51ST AVENUE | 30TH PLACE | 30TH STREET | Asphalt | S | 336 | 26 | 8,733 | 36 | 258 |
| CCRO::51ST AVE::70 | 51ST AVENUE | 30TH STREET | 29TH PLACE | Asphalt | S | 332 | 26 | 8,623 | 40 | 276 |
| CCRO::51ST AVE::80 | 51ST AVENUE | 29TH PLACE | 29TH STREET | Asphalt | S | 336 | 26 | 8,726 | 62 | 336 |
| CCRO::51ST AVE::90 | 51ST AVENUE | 29TH STREET | OGDEN AVENUE | Asphalt | S | 434 | 26 | 11,276 | 58 | 310 |
| CCRO::51ST CT::10 | 51ST COURT | 21ST PLACE | 21ST STREET | Asphalt | S | 382 | 26 | 9,940 | 52 | 250 |
| CCRO::51ST CT::20 | 51ST COURT | 21ST STREET | 19TH STREET | Asphalt | S | 656 | 26 | 17,065 | 61 | 404 |

| Pavement ID | Road Name | From | To | Surface | Rank | Length (FT) | Width (FT) | Area (SF) | PCI | IRI |
|---------------------|-------------|--------------|----------------|---------|------|-------------|------------|-----------|--------|--------|
| CCRO::51ST CT::30 | 51ST COURT | 19TH STREET | 18TH STREET | Asphalt | S | 662 | 26 | 17,218 | 28 | 549 |
| CCRO::51ST CT::40 | 51ST COURT | 18TH STREET | 16TH STREET | Asphalt | S | 698 | 26 | 18,142 | 32 | 510 |
| CCRO::51ST CT::50 | 51ST COURT | 16TH STREET | 15TH STREET | Asphalt | S | 636 | 26 | 16,530 | 62 | 359 |
| CCRO::51ST CT::60 | 51ST COURT | 15TH STREET | 14TH STREET | Asphalt | S | 667 | 26 | 17,349 | 64 | 283 |
| CCRO::51ST CT::70 | 51ST COURT | 14TH STREET | 13TH STREET | Asphalt | S | 663 | 26 | 17,246 | 33 | 331 |
| CCRO::51ST CT::80 | 51ST COURT | 13TH STREET | ROOSEVELT ROAD | Asphalt | S | 662 | 26 | 17,219 | 64 | 317 |
| CCRO::52ND CT::10 | 52ND COURT | 38TH STREET | 37TH STREET | Asphalt | S | 664 | 26 | 17,261 | 64 | 377 |
| CCRO::52ND CT::20 | 52ND COURT | 37TH STREET | 36TH STREET | Asphalt | S | 663 | 26 | 17,250 | 72 | 442 |
| CCRO::52ND CT::30 | 52ND COURT | 36TH STREET | 35TH STREET | Asphalt | S | 671 | 26 | 17,451 | 71 | 300 |
| CCRO::52ND CT::40 | 52ND COURT | 33RD STREET | 32ND STREET | Asphalt | S | 652 | 26 | 16,950 | 48 | 299 |
| CCRO::52ND CT::50 | 52ND COURT | 32ND STREET | 31ST STREET | Asphalt | S | 658 | 26 | 17,104 | 69 | 241 |
| CCRO::53RD AVE::10 | 53RD AVENUE | 38TH STREET | 37TH STREET | Asphalt | S | 669 | 26 | 17,405 | 91 | 381 |
| CCRO::53RD AVE::100 | 53RD AVENUE | 29TH PLACE | OGDEN AVENUE | Asphalt | S | 259 | 26 | 6,733 | 68 | 376 |
| CCRO::53RD AVE::110 | 53RD AVENUE | 26TH STREET | 25TH PLACE | Asphalt | S | 328 | 26 | 8,518 | 51 | 354 |
| CCRO::53RD AVE::120 | 53RD AVENUE | 25TH PLACE | 25TH STREET | Asphalt | S | 340 | 26 | 8,838 | 19 | 433 |
| CCRO::53RD AVE::130 | 53RD AVENUE | 25TH STREET | 24TH PLACE | Asphalt | S | 331 | 26 | 8,593 | 28 | 595 |
| CCRO::53RD AVE::140 | 53RD AVENUE | 24TH PLACE | 24TH STREET | Asphalt | S | 326 | 26 | 8,489 | 53 | 289 |
| CCRO::53RD AVE::150 | 53RD AVENUE | 24TH STREET | 23RD PLACE | Asphalt | S | 341 | 26 | 8,878 | 40 | 409 |
| CCRO::53RD AVE::160 | 53RD AVENUE | 23RD PLACE | 23RD STREET | Asphalt | S | 332 | 26 | 8,643 | 25 | 430 |
| CCRO::53RD AVE::170 | 53RD AVENUE | 23RD STREET | 22ND PLACE | Asphalt | S | 331 | 26 | 8,614 | 53 | 301 |
| CCRO::53RD AVE::180 | 53RD AVENUE | 22ND PLACE | 22ND STREET | Asphalt | S | 312 | 26 | 8,110 | 53 | 270 |
| CCRO::53RD AVE::190 | 53RD AVENUE | 22ND STREET | 22ND STREET | Asphalt | S | 33 | 26 | 870 | 95 | 205 |
| CCRO::53RD AVE::20 | 53RD AVENUE | 37TH STREET | 36TH STREET | Asphalt | S | 665 | 26 | 17,282 | 43 | 354 |
| CCRO::53RD AVE::30 | 53RD AVENUE | 36TH STREET | 35TH STREET | Asphalt | S | 665 | 26 | 17,297 | 48 | 398 |
| CCRO::53RD AVE::40 | 53RD AVENUE | 35TH STREET | END | Asphalt | S | 328 | 26 | 8,528 | 37 | 848 |
| CCRO::53RD AVE::50 | 53RD AVENUE | 33RD STREET | 32ND STREET | Asphalt | S | 659 | 26 | 17,123 | 62 | 217 |
| CCRO::53RD AVE::60 | 53RD AVENUE | 32ND STREET | 31ST STREET | Asphalt | S | 662 | 26 | 17,200 | 54 | 257 |
| CCRO::53RD AVE::70 | 53RD AVENUE | 31ST STREET | 30TH PLACE | Asphalt | S | 349 | 26 | 9,069 | 62 | 350 |
| CCRO::53RD AVE::80 | 53RD AVENUE | 30TH PLACE | 30TH STREET | Asphalt | S | 328 | 26 | 8,534 | 67 | 322 |
| CCRO::53RD AVE::90 | 53RD AVENUE | 30TH STREET | 29TH PLACE | Asphalt | S | 343 | 26 | 8,907 | 73 | 287 |
| CCRO::53RD CT::10 | 53RD COURT | 38TH STREET | 37TH STREET | Asphalt | S | 667 | 26 | 17,349 | 75 | 314 |
| CCRO::53RD CT::20 | 53RD COURT | 37TH STREET | 36TH STREET | Asphalt | S | 666 | 26 | 17,326 | 100 | 198 |
| CCRO::53RD CT::30 | 53RD COURT | 36TH STREET | 35TH STREET | Asphalt | S | 667 | 26 | 17,331 | 100 | 196 |
| CCRO::53RD CT::40 | 53RD COURT | 35TH STREET | 34TH STREET | Asphalt | S | 663 | 26 | 17,247 | 73 | 282 |
| CCRO::53RD CT::50 | 53RD COURT | 33RD STREET | 32ND STREET | Asphalt | S | 666 | 26 | 17,306 | 59 | 323 |
| CCRO::53RD CT::60 | 53RD COURT | 32ND STREET | 31ST STREET | Asphalt | S | 659 | 26 | 17,138 | 40 | 370 |
| CCRO::53RD CT::70 | 53RD COURT | OGDEN AVENUE | END | Gravel | T | 272 | 26 | 7,083 | Gravel | Gravel |
| CCRO::54TH AVE::10 | 54TH AVENUE | 38TH STREET | 37TH STREET | Asphalt | S | 664 | 26 | 17,271 | 46 | 335 |
| CCRO::54TH AVE::100 | 54TH AVENUE | 26TH STREET | 25TH PLACE | Asphalt | S | 332 | 26 | 8,627 | 100 | 316 |
| CCRO::54TH AVE::110 | 54TH AVENUE | 25TH PLACE | 25TH STREET | Asphalt | S | 333 | 26 | 8,670 | 100 | 401 |
| CCRO::54TH AVE::120 | 54TH AVENUE | 25TH STREET | 24TH PLACE | Asphalt | S | 335 | 26 | 8,704 | 100 | 429 |
| CCRO::54TH AVE::130 | 54TH AVENUE | 24TH PLACE | 24TH STREET | Asphalt | S | 332 | 26 | 8,625 | 100 | 391 |
| CCRO::54TH AVE::140 | 54TH AVENUE | 24TH STREET | 23RD PLACE | Asphalt | S | 329 | 26 | 8,553 | 100 | 421 |